

**AGENDA  
CITY OF ST. CHARLES  
PLAN COMMISSION  
CHAIR PETER VARGULICH**

**TUESDAY, AUGUST 19, 2025 - 7:00 P.M.  
COUNCIL CHAMBERS  
2 E. MAIN ST., ST. CHARLES, IL 60174**

- 1. Call to order**
- 2. Roll Call -**

Chair Peter Vargulich	Jeffrey Funke	Dave Rosenberg
Colleen Wiese	Gary Gruber	Gina Lawson
Zachary Ewoldt	John Fitzgerald	

Auditory Members    -    Holly Cabel, St. Charles Park District  
                                     -    Paul Gordon, School District #303
- 3. Pledge of Allegiance**
- 4. Presentation of minutes of the July 8, 2025 meeting of the Plan Commission**
- 5. Election of Officers**
- 6. River 504 Rowhomes, Brownstone PUD (Julie Salyers, J&B Builders, Inc.)**

Application for Special Use (PUD Amendment)  
Application for PUD Preliminary Plan

  - a. Public Hearing
  - b. Discussion/Recommendation
- 7. McGrath Kia Parking Expansion, McGrath Business Center PUD (Chris McGrath)**

Application for PUD Preliminary Plan  
Application for Final Plat of Subdivision
- 8. Public Comment**
- 9. Additional Business from Plan Commission Members or Staff**
- 10. Weekly Development Report**
- 11. Meeting Announcements**
  - a. Plan Commission

Wednesday, September 3, 2025 at 7:00pm Council Chambers  
Tuesday, September 16, 2025 at 7:00pm Council Chambers

Tuesday, October 7, 2025 at 7:00pm Council Chambers

b. Planning & Development Committee

Monday, September 8, 2025 at 7:00pm Council Chambers

Monday, October 13, 2025 at 7:00pm Council Chambers

## **12. Adjournment**

### ***ADA Compliance***

Any individual with a disability requesting a reasonable accommodation in order to participate in a public meeting should contact the ADA Coordinator, Jennifer McMahon, at least 48 hours in advance of the scheduled meeting. The ADA Coordinator can be reached in person at 2 East Main Street, St. Charles, IL, via telephone at (630) 377 4446 or 800 526 0844 (TTY), or via e-mail at [jmcMahon@stcharlesil.gov](mailto:jmcMahon@stcharlesil.gov). Every effort will be made to allow for meeting participation. Notices of this meeting were posted consistent with the requirements of 5 ILCS 120/1 et seq. (Open Meetings Act).



Staff Report  
Plan Commission Meeting – August 19, 2025

<b>Applicant:</b>	Julie Salyers, J&B Builders, Inc.
<b>Property Owner:</b>	River 504 LLC
<b>Location:</b>	NE corner of S. 1 <sup>st</sup> & Prairie Streets
<b>Purpose:</b>	PUD approval to develop 8 residential units
<b>Applications:</b>	Special Use (PUD Amendment) & PUD Preliminary Plan
<b>Public Hearing:</b>	Yes, required
<b>Zoning:</b>	CBD-1 / PUD
<b>Current Land Use:</b>	Vacant
<b>Comprehensive Plan:</b>	Mixed Use

**River 504 Rowhomes**



<b>Summary of Proposal:</b>	<p>Julie Salyers, on behalf of J&amp;B Builders, Inc. is proposing to develop 8 “rowhome” units on the last remaining vacant lot in the Brownstone PUD at the NE corner of S 1<sup>st</sup> &amp; Prairie Streets. Details:</p> <ul style="list-style-type: none"> <li>• 8 units in 4 buildings; 2 units in each building</li> <li>• Units facing S 1<sup>st</sup> Street, with attached garages accessed at the rear</li> <li>• 16 on-street parking spaces on S 1<sup>st</sup> Street</li> <li>• Streetscape improvements along S 1<sup>st</sup> Street</li> <li>• Connection of Prairie Street sidewalk to riverwalk</li> </ul> <p>The proposal is similar to a Concept Plan reviewed for this property last fall.</p>
<b>Info / Procedure on Application:</b>	<p><b>Special Use (PUD Amendment):</b></p> <ul style="list-style-type: none"> <li>• Approval of development project with specific deviations from the Zoning Ordinance and/or existing PUD Ordinance standards. (Establishes an ordinance amending the existing PUD, with unique zoning or subdivision standards that apply to a single development site)</li> <li>• Public hearing is required, with a mailed notice to surrounding property owners.</li> <li>• Single finding – Is the PUD Amendment in the public interest? Criteria are considered in reaching a decision. Responses to the criteria need not be in the affirmative to recommend approval of a PUD or PUD Amendment.</li> <li>• The Plan Commission may recommend conditions and restrictions upon the establishment, location, design, layout, height, density, construction, maintenance, aesthetics, operation and other elements of the PUD as deemed necessary to secure compliance with the standards specified in the Zoning Ordinance.</li> </ul>

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	<ul style="list-style-type: none"> <li>• The Plan Commission may recommend exceptions and deviations from the requirements of the Zoning and Subdivision Codes requested by the applicant, to the extent that it finds such exceptions and deviations are supportive of the standards and purposes for PUDs.</li> </ul> <p><b>PUD Preliminary Plan:</b></p> <ul style="list-style-type: none"> <li>• Approval of plans for development of property within a PUD- includes building elevations and site, landscape, and engineering plans.</li> <li>• Recommendation is based on compliance with the previously (or concurrently) approved Special Use for PUD standards and other City Code requirements.</li> </ul>
<b>Suggested Action:</b>	<p>Conduct the public hearing on the Special Use (PUD Amendment) and close if all testimony has been taken.</p> <p>The Plan Commission may vote on this item should the Commission feel that they have enough information to make a recommendation.</p> <p>Staff recommends that any recommendation include a condition requiring resolution of staff comments prior to City Council action.</p>
<b>Staff Contact:</b>	Ellen Johnson, Planner II

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## I. PROPERTY INFORMATION

### A. History / Context

The subject property constitutes three parcels at the northeast corner of S. 1<sup>st</sup> and Prairie Streets on the west side of downtown. The parcels are known as Lot 14, Lot 15, and Parcel 9 in the Brownstone Subdivision, recorded in 2001. The subject property is the final undeveloped portion of the Brownstone PUD, which includes the Brownstone townhomes along the riverfront and the mixed-use Milestone Row building fronting S 1<sup>st</sup> at Indiana Street.

The Brownstone PUD was approved under Ordinance 2000-Z-23 to enable redevelopment of the former Piano Factory site for residential townhomes. The townhomes were constructed by 2005. Plans also included two townhome buildings on the subject property (2 buildings with 5 townhome units each).

In 2005, the City approved an amendment to the Brownstone PUD under Ordinance 2005-Z-23, which changed the intended use of the subject property from townhomes to a mixed-use building, along with the property to the north, and created development standards for the two buildings, known as Milestone Row. The northern Milestone Row building was constructed in 2006 and contains first floor office space and 22 condominium units on floors 2-4, with underground parking for residents. The same building was approved for the subject property but was never constructed.

In 2022, a Concept Plan was submitted by J&B Builders (same as current applicant) to construct a mixed-use building on the subject property. The proposed building was 50 ft. in height, with first floor commercial space/parking and residential condo units on floors 2-4.

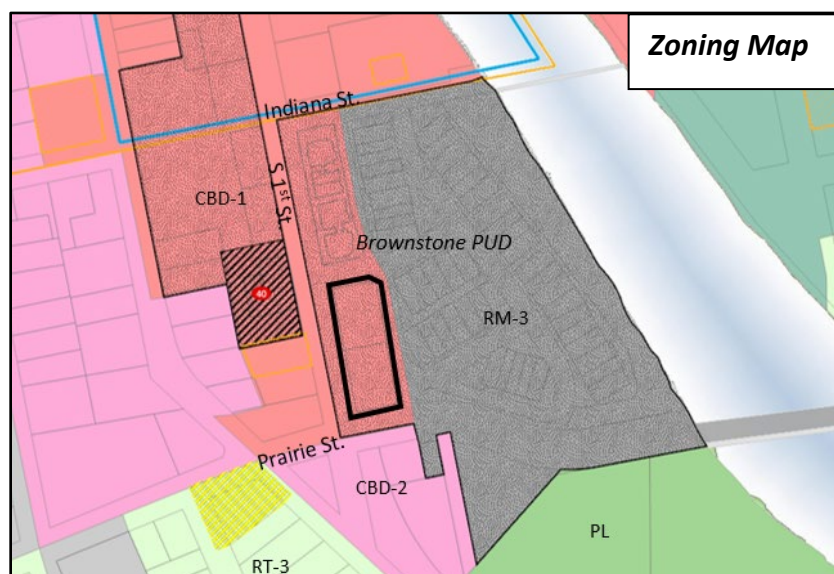
In 2024, a new Concept Plan was submitted by J&B Builders proposing 8 rowhome units on the subject property, with no commercial use. Feedback from Plan Commission and Planning & Development Committee was generally favorable.



## B. Zoning

The subject property is the southernmost property in the CBD-1 Central Business District, with CBD-2 zoning across Prairie Street and multi-family residential adjacent to the east.

	Zoning	Land Use
<b>Subject Property</b>	CBD-1 Central Business District / Brownstone PUD	Vacant
<b>North</b>	CBD-1 Central Business District / Brownstone PUD	Mixed-use building (Milestone Row)
<b>East</b>	RM-3 General Residential District / Brownstone PUD	Townhomes (Brownstones)
<b>South</b>	CBD-2 Mixed Use Business District	Commercial- Sammy's Bikes
<b>West</b>	CBD-1 Central Business District / 1 <sup>st</sup> Street Redevelopment PUD	Mixed-use building; mixed commercial uses



## C. Comprehensive Plan

The Land Use Plan adopted as part of the 2013 Comprehensive Plan identifies the subject property as “Mixed Use”, as is the rest of the downtown core.



The Mixed Use land use is described as follows:

*The Land Use Plan designates Downtown St. Charles as Mixed Use, characterized by land uses and development patterns that provide a vibrant, safe, attractive, and “walkable” pedestrian environment.*

*Mixed use areas should have strong pedestrian orientation and seek to create a more interesting and engaging pedestrian experience, accommodating pedestrian generating uses on the ground floor, and other uses above. Mixed use areas should provide a balance of uses unique to each site based on its location. Retail, entertainment, and dining uses are ideally suited for the ground floor with residential, educational, medical and/or office uses located on the upper floors. The primary objective is to provide an appropriate and compact mix of uses to foster an active and interesting district. Parking in the mixed use areas should be provided on-street, or in subtly located parking garages or parking lots.*

*Built form is a critical consideration within Mixed Use areas. Within mixed use areas, buildings should be located at, or near, the front property lines fronting the street to create a “street wall” – a continuous row of buildings and storefronts that encourages walkability and helps to establish a safe and attractive pedestrian environment.*

The subject property is also located within the Downtown Subarea. The Downtown Subarea Plan includes recommendations aimed at preserving and enhancing the downtown area (Ch. 8).

The plan identifies 1<sup>st</sup> Street, along which the subject property is located, as a Gateway Corridor, along with other streets that offer primary entry into Downtown. The following recommendations are made for Gateway Frontage properties:

- **Building Massing & Placement.**

*Buildings should be generally located on the front lot line, although small setbacks could accommodate gateway landscaping. To the extent possible, buildings should be built to the side lot lines to create a continuous streetwall.*



Gateway Frontage

- **Building Façade Orientation & Design.**

*Facades should have strong orientation to the public sidewalk, or angled toward key gateway intersections, with welcoming entrances. Attractive and safe rear entrances from rear parking areas or public walks should also be provided where appropriate.*

- **Architectural Style & Design.** *Buildings should use traditional building materials and design elements, and generally align with surrounding buildings in terms of horizontal elements and vertical rhythm. However, more flexibility and creativity should be encouraged within this general framework.*

- **Vehicular Access & Parking.** *Parking should be located to the rear of the lot, and minimal curb cuts should be provided from the public street. Development should share curb cuts and provide access from side streets instead of gateway streets wherever possible.*
- **Bicycle Access & Pedestrian Mobility.** *All buildings should provide an attractive and discernable public entry from the sidewalk, and to the extent possible, bicycle parking should be provided at the rear or sides of buildings, near parking areas or other pedestrian accessible areas.*
- **Land Use.** *Uses should be mixed, comprised of traditional downtown mixed use activities such as retail, restaurant, and local services, as well as secondary uses including offices and services with less customer visitation. Multi-story mixed use buildings should also be encouraged. Multi-family may also be appropriate on the fringe areas of Downtown.*

The subject property is also located within Catalyst Site “M”, along with other properties near the intersections of 1<sup>st</sup>/2<sup>nd</sup> and Prairie Streets. Catalyst sites are those parcels where redevelopment could have a catalytic impact on the surrounding area. Site M is described as follows:



*This area represents the southern gateway to Downtown, and existing uses and character around the intersection contrast with the community’s vision for Downtown.*

*Comprehensive redevelopment of this key intersection should transform the gateway into an attractive announcement of arrival into Downtown St. Charles. 1<sup>st</sup> Street and 2<sup>nd</sup> Street frontage could include mixed use or multi-family development, though viewsheds to important buildings and features, such as the historic Victorian house at 411 S. 2<sup>nd</sup> St. or Mount St. Mark Park, should be preserved. This opportunity may warrant further study to ensure that several urban systems are adequately planned for, such as open space linkages throughout Downtown and to the Fox River, bicycle pathways, traffic engineering, and signage and wayfinding.*

## II. PROPOSAL

Julie Salyers of J&B Builders, Inc. is requesting approval to develop residential “rowhomes” on the subject property. Details of the proposal are as follows:

- 8 residential units in 4 buildings
  - 2 units in each building; no “interior units”.
  - Units facing S 1<sup>st</sup> Street, with attached garages accessed at the rear.
  - Brick as the predominant façade material.
  - 4-story buildings
    - Approx. 5,200 sf of interior finished space per unit
    - 2-car garage & flex space on the ground level
    - Living space on the 2<sup>nd</sup> and 3<sup>rd</sup> levels
    - Partial 4<sup>th</sup> floor, with flex space & rooftop terrace
    - Exterior courtyard along the side of each unit
- 16 on-street public parking spaces on S 1<sup>st</sup> Street.

- Public sidewalk along S 1<sup>st</sup> Street & Cobblestone Drive, with streetscape design elements along S 1<sup>st</sup> Street.
- Off-site connection of Prairie Street sidewalk to riverwalk

Two development applications have been filed:

1. **Application for Special Use** requesting to amend the Brownstone PUD to allow for certain deviations from the existing PUD Ordinance and the Zoning Ordinance. Requested deviations are in regards to the change in land use from mixed-use to residential, building height, and setback from 1<sup>st</sup> Street.
2. **Application for PUD Preliminary Plan** to approve the site layout, landscaping, building elevations, photometric, and preliminary engineering plans.

### III. PLANNING ANALYSIS

Staff has analyzed the proposed development to determine compliance with the applicable standards of the Brownstone PUD and the Zoning Ordinance. Plans were reviewed against the following code sections and documents:

- Ch. 17.06 Design Review Standards & Guidelines
- Ch. 17.14 Business & Mixed Use Districts
- Ch. 17.24 Off-Street Parking, Loading & Access
- Ch. 17.26 Landscaping & Screening
- PUD Ordinances: 2000-Z-23; 2005-Z-9

#### A. Proposed Use

The proposed development includes 8 residential units, constructed in 4 buildings of 2 units each. The developer refers to the units as “rowhomes”. Under the Zoning Ordinance, the proposed units are considered “Two-Family Dwellings”, defined as:

***Dwelling, Two-Family:*** A building containing 2 dwelling units attached either vertically or horizontally.

The proposed use is not permitted under the existing PUD. The 2005 PUD Ordinance allowed only commercial uses on the first floor of the mixed-use building planned for the subject property. Residential units were permitted on the upper floors.

Two-Family Dwellings (and Townhomes) are not permitted in the CBD-1 Central Business District, which is the underlying zoning of the subject property. The only types of residential uses allowed in the CBD-1 District are artist live/work space, upper level dwellings above commercial use, and multi-family. This reflects the purpose of the CBD-1 District which is to provide for a mixed-use, compact district of retail, service, office and higher density residential uses in the central area of the City.

The subject property is located at the southern end of the CBD-1 District and is adjacent to CBD-2 zoning to the south. Two-Family Dwellings and Townhomes are permitted in the CBD-2 District.

The change in land use to allow for Two-Family Dwellings on the subject property requires approval as part of the PUD Amendment.

It is worth noting that the proposed development reverts back to the original Brownstone PUD from 2000 in terms of land use, which planned for townhomes on the subject property (2

buildings with 5 units each), with garage access from the rear. This is similar to the proposed land use and site layout.

## B. Bulk Standards / Parking

The table below compares the proposed development with the bulk standards applicable to the property per the 2005 PUD Ordinance and the underlying CBD-1 District. A deviation from the front yard (1<sup>st</sup> Street) setback has been requested as part of the PUD Amendment. A building height deviation has also been requested. All other bulk requirements are met.

Category	2005 PUD Ordinance	CBD-1 District	Concept Plan
<b>Type of Residential Unit</b>	Upper level multi-family	Upper level dwelling, multi-family	<b><i>Two-Family dwelling</i></b>
<b>Number of Residential Units</b>	22 units	29 units max. (based on 1,000 sf of lot area per unit)	8 units
<b>Max. Gross Floor Area per Building</b>	Approved building is approx. 60,000 sf (PUD ordinance does not specifically limit GFA; underlying CBD-1 District max. GFA is 40,000 sf)	40,000 sf	Approx. 5,200 sf GFA per unit; 10,400 sf GFA per building
<b>Max. Building Height</b>	49 ft.	50 ft.	<b><i>57 ft.</i></b>
<b>Front Yard</b>	5 ft. from 1 <sup>st</sup> St. ROW (architectural features can encroach up to 2 ft.)	None required; max. 5 ft.	<b><i>3.4 ft. from 1<sup>st</sup> St.</i></b>
<b>Exterior Side Yard</b>	4.7 ft. from north lot line (Cobblestone Dr) 5 ft. from south lot line (Prairie St)	None required; max. 5 ft.	7 ft. from north lot line 10 ft. from south lot line
<b>Rear Yard</b>	0 ft.	None	19 ft.
<b>Parking</b>	1 per residential unit (20 required) 33 spaces on 1 <sup>st</sup> St. for both Milestone Row bldgs.	1 per residential unit	2-car driveway/3 car garage per unit; 16 public spaces on 1 <sup>st</sup> St.

### ***Requested Deviations:***

1. Type of Residential Unit – Change in land use is proposed from mixed use with upper level dwelling to Two-Family Dwelling, as discussed previously.
2. Building Height - Maximum building height in the underlying CBD-1 District is 50 ft., however the allowable building height was reduced to 49 ft. under the 2005 PUD Ordinance. Proposed height is 57 ft., measured from grade to the top of ridge of the gables on the front elevation. A rooftop deck covers approximately half of the 4<sup>th</sup> story, at a height of 42'6". An exhibit has been submitted which compares the proposed building with neighboring buildings:
  - Milestone Row (to the north on 1<sup>st</sup> Street) – Height is comparable to the proposed building. Gable height is approx. 58'
  - Brownstone townhome (to the east) – Height is approx. 39', comparable to the rooftop deck of the proposed building but lower than the front half.



3. Front Setback – The PUD Ordinance requires a 2 ft. right-of-way (ROW) dedication for 1<sup>st</sup> Street along the subject property. Proposed is a 3.6 ft. ROW dedication so that the entire 1<sup>st</sup> Street sidewalk is within the ROW. The buildings were also pushed forward slightly to accommodate vehicle parking in the rear driveways. The resulting proposed setback is 3.4 ft.; 5 ft. is required under the PUD Ordinance. The underlying CBD-1 District does not have a minimum setback requirement; buildings are permitted up to the lot line.

### **C. Site Access / Connectivity**

Vehicles will access the proposed development off 1<sup>st</sup> Street via existing Cobblestone Drive. The proposed units have attached garages accessed from the rear (east) via driveways off Limestone Drive. The developer will be required to repave the south side of Cobblestone Drive and all of Limestone Drive along the site. This work is reflected on the engineering plans.

Sidewalk is proposed as part of the streetscape improvements along the 1<sup>st</sup> Street frontage. Sidewalk is also proposed along Cobblestone Drive (north side), as a “carriage walk”, meaning there is no parkway between the street and sidewalk. This is due to site grading, building placement, and the City’s preference to have the entire sidewalk within the ROW and accessible without the use of stairs. Sidewalk is not proposed on the east side where the driveways are located.

Existing sidewalk along Prairie Street will remain. An off-site sidewalk connecting the Prairie Street sidewalk to the Fox River trail, south of the Brownstones, has also been required in connection with this development and is reflected on the plans.

### **D. Landscaping / Streetscape**

There is no minimum landscaping requirement in the CBD-1 District. Building foundation landscaping is required only within a setback of 5 ft. or more from the building wall to the property line. A landscape plan has been submitted which includes planting areas along the front, rear, and sides of the building.

The Cobblestone Drive median will be replanted. Per Public Works, no trees are proposed to avoid site obstructions. Trees will be added along Cobblestone on the subject property.

The City approved a Streetscape and Landscape Plan for Milestone Row under Ord. 2007-Z-12 (attached). Plans depicted landscaping around the intended mixed-use building and along 1<sup>st</sup> Street. Streetscaping per the approved plan was installed along the existing Milestone Row building to the north when it was constructed.

Based on feedback from Plan Commission, Planning & Development Committee, and staff, the developer is proposing a sidewalk design of 7’ in width including a 5’ concrete sidewalk and 1’ brick border on each side. Brick pavers crossing Cobblestone Drive are continued to the end of the northern First Street planter bed, similar to the opposite side of the intersection. Landscape bump-outs bordered with pavers book-end the 1<sup>st</sup> Street parking stalls. Street lamps consistent with the rest of 1<sup>st</sup> Street will be installed. The proposed design reflects a more simplified streetscape compared to Milestone Row, which is consistent with the design approach for the more recent buildings on 1<sup>st</sup> Street north of Illinois Street. Staff believes this approach is reasonable at this location, given the residential use and location at the south end of 1<sup>st</sup> Street.

**E. Building Design**

Buildings in the CBD-1 District are subject to the Design Review standards and guidelines contained in Ch. 17.06. The Design Review standards and guidelines applicable to multi-family zoning districts were also reviewed against this proposal.

Building elevations have been provided. The front elevation has gabled roofs with cornice returns. Roof material is metal. The roof is flat at the rear for the rooftop deck. Brick is the primary façade material, accented with herringbone pattern and soldier courses. Limestone headers, sills, and coping are proposed, along with a wide limestone band between the 2<sup>nd</sup> and 3<sup>rd</sup> floors. Design is consistent around all four sides of the building.

The proposed design meets the applicable design standards and guidelines.

Note the property is outside of the Central Historic District and is therefore not subject to review by the Historic Preservation Commission.

**F. Site Lighting**

A photometric plan has been submitted indicating proposed lighting on the subject property. Wall-mounted fixtures are proposed. Lighting levels at property lines are indicated. The plan complies with Section 17.22.040 “Site Lighting”. Streetlights will also be incorporated into the streetscape design along 1<sup>st</sup> Street but are not included on the photometric plan.

**G. Plat of Subdivision**

The subject property was subdivided as part of the Brownstone Subdivision, recorded in 2001. The property was platted as Lot 14, Lot 15, and Parcel 9. There is a blanket public utility easement over the property.

A Preliminary Plat of Subdivision has been submitted. Proposed is resubdivision of the property as “River 504 Rowhomes Subdivision”. A total of 8 lots are proposed, one for each unit. Right-of-Way is dedicated to the City along S 1<sup>st</sup> Street (3.6’) and Cobblestone Drive (5’). This will result in public sidewalks along these streets being completely within City ROW. A blanket public utility and drainage easement covers the entire property except for the building footprints.

A Final Plat of Subdivision will be required after Preliminary approval.

**IV. DEPARTMENTAL REVIEWS****A. Engineering Review**

Preliminary engineering plans have been provided, revised, and approved by staff. Stormwater detention is not required for this development as detention provided for the Brownstone development included this lot. Final level comments have been provided to the applicant which will need to be addressed alongside the Final Plat of Subdivision.

**B. Fire Dept. Review**



The Fire Dept. has reviewed the plans. Site access, water supply, and hydrant locations are adequate. Fire sprinklers will be required due to the size of the buildings, as well as a 1-hour separation wall between attached units.

### C. Public Works

Public Works has reviewed the plans and all comments have been addressed.

## V. DEVELOPER CONTRIBUTIONS

### A. Inclusionary Housing

This development is subject to the Inclusionary Housing Ordinance, Title 19 of the City Code. The affordable unit requirement is 0.4 units (10% of the total number of units). A fee worksheet has been submitted indicating the applicant's intent to pay a fee in-lieu of providing 1 affordable unit. Based on a fee in-lieu amount of \$36,718 per required affordable two-family unit, a total fee in-lieu amount of \$14,687 will be due at the time of building permit.

### B. School & Park Districts

The developer of the Brownstone townhomes previously paid for 15 attached single-family units on the subject property. As such, credit granted will be granted for up to 15 units. This exceeds the 8 proposed units; therefore, no School or Park fees will be due.

## VI. OPTIONS FOR PLAN COMMISSION ACTION

### 1. Public Hearing – Close or Continue

If the Plan Commission feels they have adequate information the public hearing may be closed. The public hearing may be continued if additional information is deemed necessary to provide a recommendation.

***If Public Hearing is closed-***

### 2. Make a Recommendation to Planning & Development Committee

There is a single finding required for the City to approve a PUD application: ***Is the PUD in the public interest?*** There are five Criteria to be considered to reach a decision. These 5 Criteria are:

1. *The proposed PUD advances one or more of the purposes of the Planned Unit Development procedure stated in Section 17.04.400.A:*
  - i. *To promote a creative approach to site improvements and building design that results in a distinctive, attractive development that has a strong sense of place, yet becomes an integral part of the community.*
  - ii. *To create places oriented to the pedestrian that promote physical activity and social interaction, including but not limited to walkable neighborhoods, usable open space and recreational facilities for the enjoyment of all.*
  - iii. *To encourage a harmonious mix of land uses and a variety of housing types and prices.*

- iv. *To preserve native vegetation, topographic and geological features, and environmentally sensitive areas.*
  - v. *To promote the economical development and efficient use of land, utilities, street improvements, drainage facilities, structures and other facilities.*
  - vi. *To encourage redevelopment of sites containing obsolete or inappropriate buildings or uses.*
  - vii. *To encourage a collaborative process among developers, neighboring property owners and residents, governmental bodies, and the community.*
2. *The proposed PUD and PUD Preliminary Plans conform to the requirements of the underlying zoning district or districts in which the PUD is located and to the applicable Design Review Standards contained in Chapter 17.06, except where:*
    - A. *Conforming to the requirements would inhibit creative design that serves community goals, or*
    - B. *Conforming to the requirements would be impractical and the proposed PUD will provide benefits that outweigh those that would have been realized by conforming to the applicable requirements.*
  3. *The proposed PUD conforms with the standards applicable to Special Uses (Section 17.04.330.C.2):*
    - a. *Public convenience: The Special Use will serve the public convenience at the proposed location;*
    - b. *Sufficient infrastructure: That adequate utilities, access roads, drainage and/or necessary facilities have been, or are being, provided;*
    - c. *Effect on nearby property: That the Special Use will not be injurious to the use and enjoyment of other property in the immediate vicinity for the purposes already permitted, nor substantially diminish or impair property values within the neighborhood*
    - d. *Effect on development of surrounding property: That the establishment of the Special Use will not impede the normal and orderly development and improvement of the surrounding property for uses permitted in the district.*
    - e. *Effect on general welfare: That the establishment, maintenance or operation of the Special Use will not be detrimental to or endanger the public health, safety, comfort or general welfare.*
    - f. *Conformance with codes: That the proposed Special Use conforms to all applicable provisions of the St. Charles Municipal Code and meets or exceeds all applicable provisions of this title, except as may be varied pursuant to a Special Use for Planned Unit Development.*
  4. *The proposed PUD will be beneficial to the physical development, diversity, tax base and economic well-being of the City; and*
  5. *The proposed PUD conforms to the purposes and intent of the Comprehensive Plan.*

Responses to all criteria need not be in the affirmative to recommend approval of the PUD Amendment. The applicant has provided responses to the Criteria for PUDs as part of the application materials.

There are no findings of fact pertaining to PUD Preliminary Plans. Approval is subject to meeting the applicable standards of the Zoning Ordinance and PUD Ordinance, accounting for the requested PUD deviations.

**Recommendation Options:**

- a. **Recommend approval of the application for Special Use (PUD Amendment) and PUD Preliminary Plan** – Plan Commission may add additional conditions if deemed necessary by the Plan Commission to meet the PUD finding. A recommendation for approval of the PUD Preliminary Plan should be conditional upon resolution of staff comments prior to City Council action.

**OR**

- b. **Recommend denial of the application for Special Use (PUD Amendment) and PUD Preliminary Plan** – Plan Commission must substantiate how the PUD finding is not being met in order to recommend denial.

**VII. ATTACHMENTS**

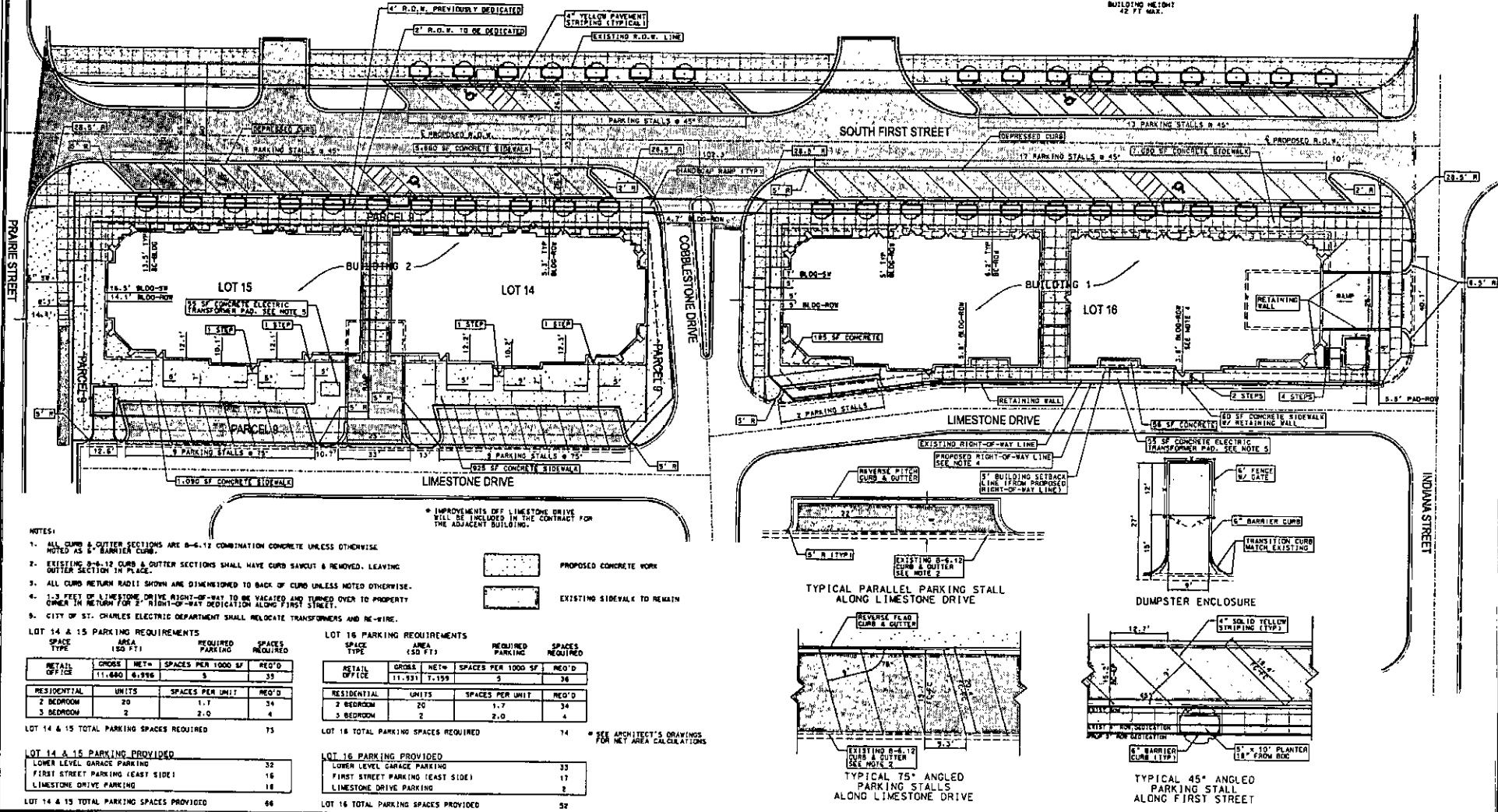
- Approved Site Plan from 2005 PUD Ordinance
- Approved Streetscape Plan from 2007
- Applications for Special Use (PUD Amendment) & PUD Preliminary Plan; received 3/13/25
- Plans & Plat

ROADWAY, LANDSCAPING AND STREETSCAPE ELEMENTS FROM WEST OF BUILDING LINE TO THE WEST RIGHT OF WAY LINE OF FIRST STREET SHALL BE INCLUDED AS A PART OF THE FUTURE FIRST STREET PROJECT AND ARE SUBJECT TO FURTHER REVIEW BY THE CITY OF ST. CHARLES. THESE ELEMENTS ARE NOT PART OF THIS CONTRACT.

**ZONING CLASSIFICATIONS**  
 EXISTING: R-2 W/ ADJACENT LANE FOR P.U.D.  
 PROPOSED: R-2 W/ ADJACENT LANE FOR P.U.D.  
 SURROUNDING ZONING  
 NORTH: B-2C  
 EAST: R-2 P.U.D.  
 SOUTH: M-1  
 WEST: M-1

**DEVELOPMENT STANDARDS**  
 MINIMUM LOT AREA:  
 FOR DWELLING UNIT:  
 LOT 14 & 15: 2,100 SF  
 LOT 16: 180 SF  
 NONRESIDENTIAL: NO MINIMUM  
 LOT WIDTH:  
 50 FT MIN  
 BUILDING HEIGHT:  
 42 FT MAX.

**BUILDING SETBACKS**  
 FRONT YARD (MIN):  
 FROM LOT LINE: 5 FT  
 GARAGE DOOR TO CURB: 18 FT  
 SIDE YARD (MIN):  
 INTERIOR: 5 FT  
 FACING STREET: 5 FT



- NOTES:**
1. ALL CURB & GUTTER SECTIONS ARE 8-6.12 COMBINATION CONCRETE UNLESS OTHERWISE NOTED AS 5" BARRIER CURB.
  2. EXISTING 8-6.12 CURB & GUTTER SECTIONS SHALL HAVE CURB SAWCUT & REMOVED, LEAVING GUTTER SECTION IN PLACE.
  3. ALL CURB RETURN RADII SHOWN ARE DIMENSIONED TO BACK OF CURB UNLESS NOTED OTHERWISE.
  4. 1-3 FEET OF LIMESTONE DRIVE RIGHT-OF-WAY TO BE VACATED & TURNED OVER TO PROPERTY OWNER IN RETURN FOR 2' RIGHT-OF-WAY DEDICATION ALONG FIRST STREET.
  5. CITY OF ST. CHARLES ELECTRIC DEPARTMENT SHALL RELOCATE TRANSFORMERS AND RE-WIRE.

**LOT 14 & 15 PARKING REQUIREMENTS**

SPACE TYPE	ORIG. AREA (SQ FT)	NET AREA (SQ FT)	SPACES PER 1000 SF	REQ'D SPACES
RETAIL OFFICE	11,680	8,916	5	33
RESIDENTIAL UNITS			SPACES PER UNIT	REQ'D
2 BEDROOM	20		1.7	34
3 BEDROOM	2		2.0	4

**LOT 14 & 15 TOTAL PARKING SPACES REQUIRED**

LOT 14 & 15 TOTAL PARKING SPACES REQUIRED	73
LOT 14 & 15 PARKING PROVIDED	
LOWER LEVEL GARAGE PARKING	32
FIRST STREET PARKING (EAST SIDE)	16
LIMESTONE DRIVE PARKING	18
LOT 14 & 15 TOTAL PARKING SPACES PROVIDED	66

**LOT 16 PARKING REQUIREMENTS**

SPACE TYPE	ORIG. AREA (SQ FT)	NET AREA (SQ FT)	SPACES PER 1000 SF	REQ'D SPACES
RETAIL OFFICE	11,931	7,155	5	36
RESIDENTIAL UNITS			SPACES PER UNIT	REQ'D
2 BEDROOM	20		1.7	34
3 BEDROOM	2		2.0	4

**LOT 16 TOTAL PARKING SPACES REQUIRED**

LOT 16 TOTAL PARKING SPACES REQUIRED	74
LOT 16 PARKING PROVIDED	
LOWER LEVEL GARAGE PARKING	33
FIRST STREET PARKING (EAST SIDE)	17
LIMESTONE DRIVE PARKING	2
LOT 16 TOTAL PARKING SPACES PROVIDED	52

\* SEE ARCHITECT'S DRAWINGS FOR NET AREA CALCULATIONS

**K+ K-Plus Engineering, Ltd.**  
 1979 N. MI. ST., SUITE 100  
 ST. CHARLES, MO 63301  
 (636) 261-1700  
 www.k-plus.com

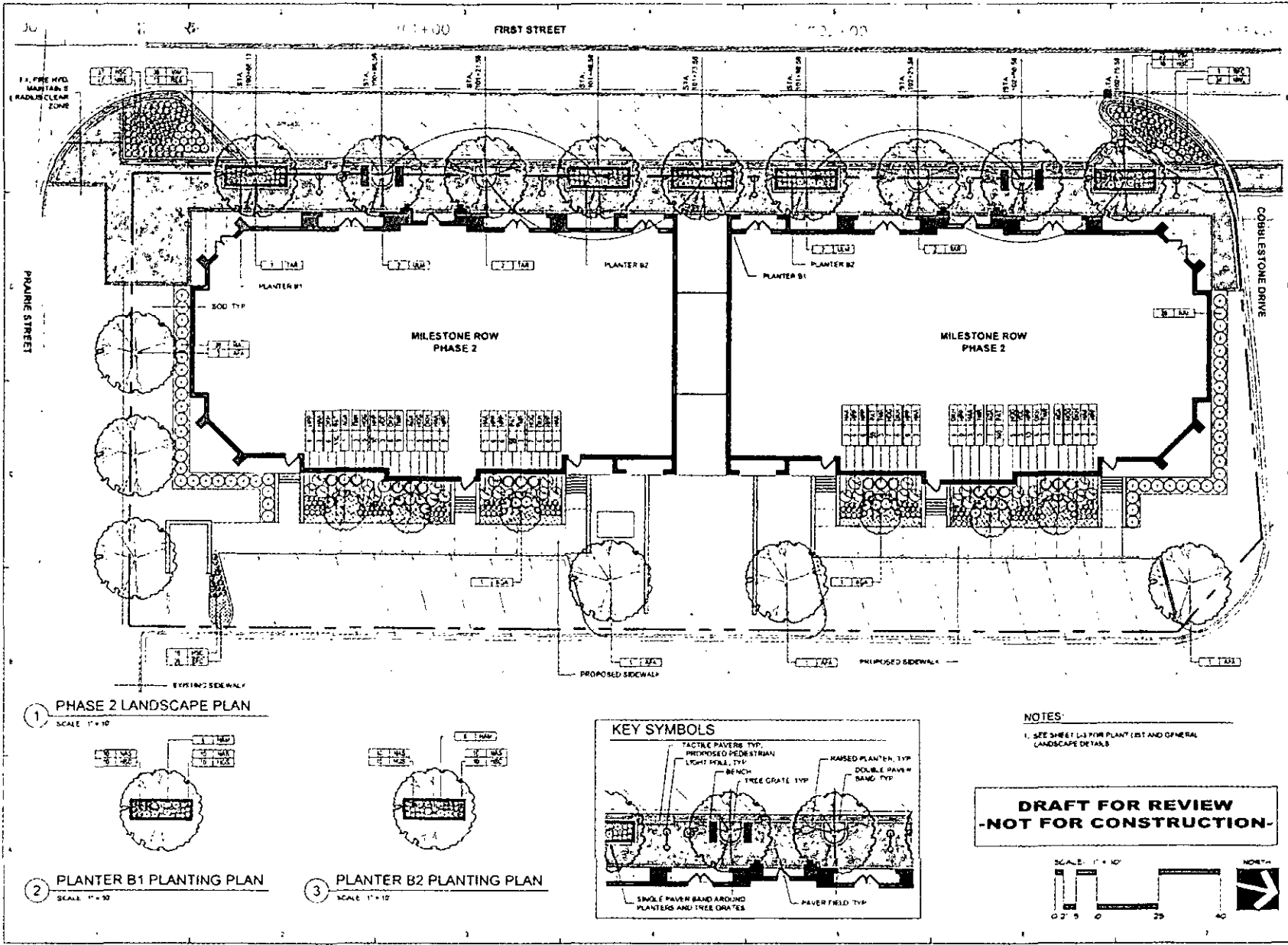
**STC DEVELOPMENT**  
 P.O. Box 3072  
 St. Charles, MO 63301  
 (636) 261-1700

DATE: MAY 8, 2008	CHECKED BY: KM
-------------------	----------------

FILE NAME: 05119	DATE: MAY 8, 2008
------------------	-------------------

**MILESTONE ROW**  
 ST. CHARLES KAMP COUNTY ILLINOIS

**SITE PLAN**



**DLK**  
Civic Design  
The Fine Arts Building  
410 South Dearborn Avenue  
Chicago, Illinois 60604  
Tel: 312 322-8011  
Fax: 312 322-8334  
www.dlkcivildesign.com



CALL J.U.L.I.P.  
1-800-862-0123

[illegible]

2	6/16/2007	Reimbursed by Revenue
2	7/11/2007	Reimbursed by Revenue
No.	Date	Description

Milestone  
Row  
St. Charles, IL

LANDSCAPE  
PLAN

Date	21/1/2001
Project	201045
In Charge	RON
Supplied by	RON
Checked by	LVS
Designed by	BAJ/ASH
Drawn by	BAJ/ASH

L-2

City of St. Charles  
Community Development Division  
2 E. Main Street  
St. Charles, IL 60174



Phone: (630) 377-4443  
Email: [cd@stcharlesil.gov](mailto:cd@stcharlesil.gov)

## SPECIAL USE APPLICATION

(To request a Special Use or Amendment, or a Special Use for PUD or Amendment)

### For City Use

Project Name: River 504 Rowhomes  
Project Number: 2022 -PR- 010  
Cityview Project Number: PLSU 202500083

Received Date  
**RECEIVED**

**MAR 13 2025**

City of St. Charles  
Community Development

- File this application to request a Special Use for a property, or to request to amend an existing Special Use Ordinance for a property
- Complete the application and submit with all required attachments to the Community Development Division.
- The information you provide must be complete and accurate. If you have any questions please contact the Community Development Division.
- City staff will review the submittal for completeness and for compliance with applicable requirements prior to establishing a public hearing date.

<b>1. Property Information:</b>	Location: NE Corner of S. 1st Street & Prairie Street	
	Parcel Number (s): 09-34-136-003, 09-34-136-002, 09-34-136-001	
	Proposed Name: River 504 Rowhomes	
<b>2. Applicant Information:</b>	Name: Julie Salyers, J&B Builders, Inc.	Phone: 630-587-9900
	Address 525 S Tyler Road, Unit P St. Charles, IL 60174	Email: <a href="mailto:julie@jbbuilders.com">julie@jbbuilders.com</a>
<b>3. Record Owner Information:</b>	Name: River 504 LLC	Phone: 630-587-9900
	Address: 525 S Tyler Road, Unit P St. Charles, IL 60174	Email: <a href="mailto:brian@jbbuilders.com">brian@jbbuilders.com</a>



**4. Identify the Type of Application:**

- ☐ **Special Use for Planned Unit Development - PUD Name:** \_\_\_\_\_
- ☐ New PUD
- ☒ Amendment to existing PUD- Ordinance #: 2005-2-9
- ☐ PUD Preliminary Plan filed concurrently
- ☐ **Other Special Use (from list in the Zoning Ordinance):** \_\_\_\_\_
- ☐ Newly established Special Use
- ☐ Amendment to an existing Special Use Ordinance #: \_\_\_\_\_

**5. Information Regarding Special Use:**

Comprehensive Plan designation of the property: Two Family Dwelling/Townhomes

Is the property a designated Landmark or in a Historic District? No

What is the property's current zoning? CBD-1

What is the property currently used for? Vacant property

If the proposed Special Use is approved, what improvements or construction are planned?

Construction of four free standings buildings that contain two residential units per building.

**6. For Special Use Amendments only:**

Why is the proposed change necessary?

Market could not support the cost to construct a mixed used condominium building.

What are the proposed amendments? (Attach proposed language if necessary)

Amend the 2025 PUD Ordinance to accomodate the change in use to townhomes/two family dwelling units.

**Note for existing buildings:** If your project involves using an existing building, whether you plan to alter it or not, please contact the St. Charles Fire Department (630-377-4458) and the Building and Code Enforcement Division (630-377-4406) for information on building, life safety and other code requirements. Depending on the proposed use, size of structure and type of construction, these requirements can result in substantial costs.



## 7. **Required Attachments:**

*If multiple zoning or subdivision applications will be submitted concurrently, do not submit duplicate checklist items or plans. Fee must be paid for each application.*

☒ **APPLICATION FEE:** Special Use for PUD: \$1,000

All other Special Use requests: \$750

☒ **REIMBURSEMENT OF FEES AGREEMENT:** An original, executed Reimbursement of Fees Agreement and deposit of funds in escrow with the City, as provided by Appendix B of the Zoning Ordinance.

☒ **REIMBURSEMENT OF FEES INITIAL DEPOSIT:** Deposit of funds in escrow with the City. Required deposit is based on review items (number of applications filed) and the size of the subject property:

Number of Review Items	Under 5 Acres	5-15 Acres	16-75 Acres	Over 75 Acres
1	\$2,000	\$3,000	\$4,000	\$5,000
2 or 3	<u>\$3,000</u>	\$5,000	\$6,000	\$8,000
4 or more	\$4,000	\$6,000	\$8,000	\$11,000

☒ **PROOF OF OWNERSHIP:** a) A current title policy report; or  
b) A deed and a current title search

*NOTE: Private covenants and deed restrictions can limit private property rights with respect to the use of land even though the City's Zoning Ordinance may authorize the use or a less restrictive use. We strongly advise that you perform a title search on the property to determine if there any private covenants containing use restrictions or other deed restrictions. As those private covenants and deed restrictions may conflict with the City's Zoning Ordinance, it is further recommended that you consult with an attorney to obtain an opinion with respect to whether your intended use is compatible with those restrictions.*

☒ **OWNERSHIP DISCLOSURE:** Use the appropriate disclosure form (attached), if the owner or applicant is a Partnership, Corporation, Trust, or LLC.

☐ **LETTER OF AUTHORIZATION:** If the property owner is not the applicant, an original letter of authorization from the property owner permitting the applicant to file the zoning application with the City of St. Charles for the subject property.

☒ **LEGAL DESCRIPTION:** For entire subject property, on 8 1/2 x 11 inch paper and Microsoft Word file.

☒ **PLAT OF SURVEY:** A current plat of survey for the subject property showing all existing improvements on the property, prepared by a registered Illinois Professional Land Surveyor.

☒ **FINDINGS OF FACT:** Fill out the attached "Criteria for Planned Unit Developments (PUDs)" form for any PUD application and the "Findings of Fact – Special Use" form for all other Special Use applications.

☒ **LIST OF PROPERTY OWNERS WITHIN 250 FT.:** Fill out the attached form or submit on a separate sheet. The form or the list must be signed and notarized. Property ownership information may be obtained using Kane County's interactive GIS mapping tool: [http://gistech.countyofkane.org/gisims/kanemap/kanegis4\\_AGOx.html](http://gistech.countyofkane.org/gisims/kanemap/kanegis4_AGOx.html)

☒ **SOIL AND WATER CONSERVATION DISTRICT NATURAL RESOURCES INVENTORY APPLICATION:** As required by State law, submit a Natural Resources Inventory (NRI) application and required fee directly to the Kane-DuPage Soil and Water Conservation District. Provide a copy of completed NRI application to the City. The NRI application can be found on the Kane-DuPage SWCD website: <https://kanedupageswcd.org/kd/natural-resource-inventory>

- ✓ **ENDANGERED SPECIES REPORT:** As required by State law, file an Endangered Species Consultation Agency Action with the Illinois Department of Natural Resources. Provide a copy of the report to the City. The online Ecological Compliance Assessment Tool (EcoCAT) should be utilized: <https://dnr2.illinois.gov/EcoPublic/>
- ✓ **KANE COUNTY TRANSPORTATION IMPACT FEE:** The Kane County Road Improvement Impact Fee Ordinance applies to new residential and non-residential development within Kane County. The impact fee is determined by Kane County upon submittal of an application to Kane County Department of Transportation at the time of building permit. At this stage, the Impact Fee Estimator Tool can be used to estimate the cost of the fee: <http://kdot.countyofkane.org/Pages/Impact-Fees.aspx>
- ✓ **TRAFFIC STUDY:** If applicable. Staff will advise you whether a traffic study is recommended based on the project. Regardless, the Plan Commission or City Council may request a traffic study as a part of the review process.
- ✍ **PLANS:** All required plans shall be drawn on sheets no larger than 24" x 36", unless the Director of Community Development permits a larger size when necessary to show a more comprehensive view of the project. All required plans shall show north arrow and scale, and shall be drawn at the same scale (except that a different scale may be used to show details or specific features). All plans shall include the name of the project, developer or owner of site, person or firm preparing the plan, and the date of plan preparation and all revisions.

**Copies:** Ten (10) full size copies and PDF electronic file emailed to: [cd@stcharlesil.gov](mailto:cd@stcharlesil.gov)

**Site Plan or plans shall show the following information:**

1. Accurate boundary lines with dimensions
2. Streets on and adjacent to the tract: Name and right-of-way width
3. Location, size, shape, height, and use of existing and proposed structures
4. Location and description of streets, sidewalks, and fences
5. Surrounding land uses
6. Date, north point, and scale
7. Ground elevation contour lines
8. Building/use setback lines
9. Location of any significant natural features
10. Location of any 100-year recurrence interval floodplain and floodway boundaries
11. Location and classification of wetland areas as delineated in the National Wetlands Inventory
12. Existing zoning classification of property
13. Existing and proposed land use
14. Area of property in square feet and acres
15. Proposed off-street parking and loading areas
16. Number of parking spaces provided, and number required by ordinance
17. Angle of parking spaces
18. Parking space dimensions and aisle widths
19. Driveway radii at the street curb line
20. Width of driveways at sidewalk and street curb line
21. Provision of handicapped parking spaces
22. Dimensions of handicapped parking spaces
23. Depressed ramps available to handicapped parking spaces
24. Location, dimensions and elevations of freestanding signs
25. Location and elevations of trash enclosures
26. Provision for required screening, if applicable
27. Exterior lighting plans showing:
  - a. Location, height, intensity and fixture type of all proposed exterior lighting
  - b. Photometric information pertaining to locations of proposed lighting fixtures

~~\*~~ (Note- For a Special Use for PUD, submit PUD Preliminary Plan Application In lieu of Site Plan)

(we) certify that this application and the documents submitted with it are true and correct to the best of my (our) knowledge and belief.

B. P. Benz MGR 3/13/25  
Record Owner Date

[Signature] 3/13/2025  
Applicant or Authorized Agent Date

**OWNERSHIP DISCLOSURE FORM  
LIMITED LIABILITY COMPANY (L.L.C.)**

STATE OF ILLINOIS       )  
                                      ) SS.  
KANE COUNTY            )

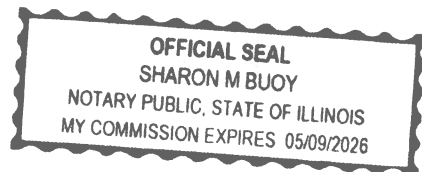
I, Brian Buoy, being first duly sworn on oath depose and say that I am  
Manager of River 504, LLC, an Illinois Limited Liability  
Company (L.L.C.), and that the following persons are all of the members of the said L.L.C.:

_____	_____
_____	_____
_____	_____
_____	_____
_____	_____
_____	_____
_____	_____

By: Brian Buoy, Manager

Subscribed and Sworn before me this 17<sup>th</sup> day of  
September, 2024.

Sharon M Buoy  
Notary Public



## CRITERIA FOR PLANNED UNIT DEVELOPMENTS (PUDs)

### **\*Use this form for PUD or PUD Amendment applications\***

*The St. Charles Zoning Ordinance requires the Plan Commission to consider the criteria listed below in making a recommendation to the City Council on whether the proposed Planned Unit Development is in the public interest. As the applicant, the "burden of proof" is on you to provide information that addresses the criteria below in order to demonstrate that the project is in the public interest.*

**PUD Name:** River 504 Rowhomes

#### **From the St. Charles Zoning Ordinance, Section 17.04.410.3:**

The Plan Commission shall not favorably recommend, and the City Council shall not approve, a Special Use for a PUD or an amendment to a Special Use for a PUD unless they each make findings of fact based on the application and the evidence presented at the public hearing that the PUD is in the public interest, based on the following criteria:

- i. The proposed PUD advances one or more of the purposes of the Planned Unit Development procedure stated in Section 17.04.400.A:**
  1. To promote a creative approach to site improvements and building design that results in a distinctive, attractive development that has a strong sense of place, yet becomes an integral part of the community.
  2. To create places oriented to the pedestrian that promote physical activity and social interaction, including but not limited to walkable neighborhoods, usable open space and recreational facilities for the enjoyment of all.
  3. To encourage a harmonious mix of land uses and a variety of housing types and prices.
  4. To preserve native vegetation, topographic and geological features, and environmentally sensitive areas.
  5. To promote the economical development and efficient use of land, utilities, street improvements, drainage facilities, structures and other facilities.
  6. To encourage redevelopment of sites containing obsolete or inappropriate buildings or uses.
  7. To encourage a collaborative process among developers, neighboring property owners and residents, governmental bodies and the community

The modifications to the existing approved PUD from 2005 maintain the purpose of the requirements established in Section 17.04.400 of the Zoning Ordinance. Currently vacant, undeveloped open space, the proposed development will serve as part of the southern gateway to downtown by providing residential housing, extension of the streetscape and on street public parking. The architectural style and orientation of each building facade and the building silhouettes of the proposed development blend the existing Milestone Row PUD with the previously approved Brownstone PUD, resulting in four free standing buildings rising four stories in height. The building height is comparable to the Milestone 1 building located north of the property while the free-standing buildings reflect the Brownstone residential units located east of the property. In addition, the free-standing buildings decrease the mass of the structure and allow addition green space and extensive landscaping between the buildings. Finally, the architectural design includes a "four sided" building with a timeless urban style that will compliment the existing surrounding buildings.

ii. **The proposed PUD and PUD Preliminary Plans conform to the requirements of the underlying zoning district or districts in which the PUD is located and to the applicable Design Review Standards contained in Chapter 17.06, except where:**

- A. Conforming to the requirements would inhibit creative design that serves community goals, or**
- B. Conforming to the requirements would be impractical and the proposed PUD will provide benefits that outweigh those that would have been realized by conforming to the applicable requirements.**

**Factors listed in Section 17.04.400.B shall be used to justify the relief from requirements:**

1. The PUD will provide community amenities beyond those required by ordinance, such as recreational facilities, public plazas, gardens, public art, pedestrian and transit facilities.
2. The PUD will preserve open space, natural beauty and critical environmental areas in excess of what is required by ordinance or other regulation.
3. The PUD will provide superior landscaping, buffering or screening.
4. The buildings within the PUD offer high quality architectural design.
5. The PUD provides for energy efficient building and site design.
6. The PUD provides for the use of innovative stormwater management techniques.
7. The PUD provides accessible dwelling units in numbers or with features beyond what is required by the Americans with Disabilities Act (ADA) or other applicable codes.
8. The PUD provides affordable dwelling units in conformance with, or in excess of, City policies and ordinances.
9. The PUD preserves historic buildings, sites or neighborhoods.

Conforming to the requirements of the existing PUD would be impractical in current market conditions. Prior to this proposal, a mixed-use condominium building was designed for the site. A cost analysis deemed the development was not feasible. The four, stand-alone two-family buildings we are proposing with up to 4,500 sf of interior finished space per unit, will have a more residential style of construction with wood framing, more simplistic mechanical, electrical and plumbing systems and no common space to build. The proposed development is very similar to the previously approved Brownstone PUD. The building height has been increased so that it is comparable to the Milestone 1 building located north of the development to provide a continuance of the streetscape along S 1st Street. With the increase in building height, additional interior finished space can be offered for each unit, allowing a reduction in the number of units on the site as compared to the previously approved ten units, providing less bulk and more green space while maintaining the feasibility of the development.

1. Proposed site plan promotes and extends pedestrian traffic with the extension of the existing sidewalk with paver accents from the property located to the north. The proposed offsite sidewalk will also connect the existing sidewalk along Prairie Street to the bike path along the river.

2. The stand-alone buildings have less bulk and will allow green space with landscaping between the buildings.

3. The proposed landscape plan provides ample landscaping with landscaping around the entire perimeter of the property and at the Cobblestone Drive median as well as extensive landscaping between the buildings.

4. The proposed architectural style reflects a timeless urban design that contains transitional components to blend with the existing adjacent buildings.

5. The energy efficient components (windows, MEP equipment, electric vehicle charging stations) will be incorporated into the building construction and the landscape design includes native, low maintenance plants.

6. Not applicable, no on-site stormwater management is required.

7. A private elevator is being offered as an option for each unit, unit entries can be adapted for accessibility and handicap parking has been provided on S 1st Street.

8. No.

9. The proposed building will include quality materials comparable to the previously approved design.

iii. **The proposed PUD conforms with the standards applicable to Special Uses (Section 17.04.330.C.2):**

**A. Public Convenience: The Special Use will serve the public convenience at the proposed location.**

The amendment to the special use will serve public convenience since the property currently sits undeveloped and vacant since its approval in 2005. The proposed development is similar in configuration, massing and land use as what was previously approved in the Brownstone PUD in 2000. Changes have been made to reduce the number of units from 10 to 8 and to eliminate mass by providing 4 stand alone buildings with green space between the buildings. In addition, each unit has the option for a 2 or 3 car garage as well as driveway space that will allow parking for unit guests. The ample unit parking will contribute to public convenience as the 16 parking spaces constructed along S. 1st Street will primarily be available for public use.

**B. Sufficient Infrastructure: That adequate utilities, access roads, drainage and/or necessary utilities have been, or are being, provided.**

Because the proposed development is part of the previously approved Milestone Row PUD, existing utilities, access roads and necessary facilities are either present or will be upgraded to meet the project needs. The existing water, electrical, storm and sanitary lines are directly adjacent to the property and can be extended or modified. Access to the unit garages is being proposed from Limestone Drive. A traffic study was completed and concluded the development will generate a low volume of peak hour traffic.

**C. Effect on Nearby Property: That the Special Use will not be injurious to the use and enjoyment of other property in the immediate vicinity for the purposes already permitted, nor substantially diminish or impair property values within the neighborhood.**

The amendment to the special use will not be injurious to the use and enjoyment of other properties in the immediate vicinity since the proposed architectural design reflects a "four sided" building that will be aesthetically pleasing to the adjacent properties on Limestone drive. In addition, by eliminating the commercial space, decreasing the number of residential units and providing ample private unit parking, residents in adjacent properties will encounter less traffic and congestion traffic at the site. The development will also provide 16 public parking spaces that can be used and enjoyed by other adjacent properties.

**D. Effect on Development of Surrounding Property: That the establishment of the Special Use will not impede the normal and orderly development and improvement of the surrounding property for uses permitted in the district.**

The amendemnt to the Special Use will compliment the normal and orderly development and improvement of the surrounding properties by extending the streetscape along S 1st Street. The proposed height, silhouette and architectural components of the proposed proposed stand alone buildings are consistent with the Milestone 1 building located north of the development while still relatively similar and complimentary to the existing residential units located east of the property that were also part of the Brownstone PUD that was previously approved for this site.



**E. Effect on General Welfare: That the establishment, maintenance or operation of the Special Use will not be detrimental to or endanger the public health, safety, comfort or general welfare.**

The amendment to the PUD will not be detrimental to or endanger the public health, safety, comfort or general welfare. The proposed development is very similar to the previously approved Brownstone PUD, fits within the neighborhood, provides public parking while extending the public streetscape along S 1st Street.

**F. Conformance with Codes: That the proposed Special Use conforms to all applicable provisions of the St. Charles Municipal Code and meets or exceeds all applicable provisions of this Title, except as may be varied pursuant to a Special Use for Planned Unit Development.**

The proposed development will be constructed in accordance with the St. Charles Municipal Code except for variances requested as part of this amendment to the existing Special Use.

**iv. The proposed PUD will be beneficial to the physical development, diversity, tax base and economic well-being of the City.**

The amendment to the previously approved PUD will increase the tax base and economic well-being of the City with the construction of 8 residential units on existing vacant land.

**v. The proposed PUD conforms to the purposes and intent of the Comprehensive Plan.**

The proposed PUD conforms to the purposes and intent of the Comprehensive Plan. The configuration of the proposed buildings, building face orientation and the building silhouettes blend the currently approved PUD with the previously approved Brownstone PUD. The vehicular access, parking and land use is also in line with Comprehensive Plan

## **Project Narrative**

Development Name: River 504 Rowhomes

Location: NE Corner of S 1<sup>st</sup> & Prairie Streets

### Proposed Land Use:

The development will consist of four freestanding buildings with two units in each building. The buildings will be oriented on the site to provide a front elevation along S 1<sup>st</sup> Street. A two-car garage (third car tandem) with a private driveway will be located at the rear of each unit that can be accessed from Limestone Drive. The green space located between the buildings will provide an additional exterior space for six of the units. Public parking spaces will be provided along S 1<sup>st</sup> Street. Improved landscaping will be provided along First Street and around the perimeter of the site. In addition, landscaping will be focused at the entrances on the side elevations of the northern and southernmost units to provide screening from Prairie Street and Cobblestone Drive.

The four story units will provide a flex space and a two-car garage with driveway on the ground level, living space on the second and third floor levels and additional flex space with a rooftop terrace on the fourth level. The building exteriors will be designed to complement the adjacent Brownstone Development and Milestone 1 properties. The total footprint for all four buildings totals 49,808 sf.

City of St. Charles  
Community Development Division  
2 E. Main Street  
St. Charles, IL 60174



Phone: (630) 377-4443  
Email: [cd@stcharlesil.gov](mailto:cd@stcharlesil.gov)

## PUD PRELIMINARY PLAN APPLICATION

### For City Use

Project Name: River 504 Rowhomes  
Project Number: 2022 -PR- 010  
Cityview Project Number: PLP4D202500082

Received Date  
**RECEIVED**

**MAR 13 2025**

City of St. Charles  
Community Development

- File this application to request approval of a PUD Preliminary Plan. Normally, this application is reviewed concurrently with an application for Special Use for PUD, unless a Special Use for PUD has previously been granted and no amendment is necessary.
- Complete the application and submit with all required attachments to the Community Development Division.
- The information you provide must be complete and accurate. If you have any questions please contact the Community Development Division.
- City staff will review the submittal for completeness and for compliance with applicable requirements. Staff will distribute the plans to other City departments for review when the application is complete.
- The PUD Preliminary Plan will be scheduled for Plan Commission review when staff has determined the plans are ready.

<b>1. Property Information:</b>	Location: NE corner S 1st & Prairie Streets	
	Parcel Number (s): 09-34-136-003, 09-34-136-002, 09-34-136-001	
	Proposed PUD Name: River 504 Rowhomes	
<b>2. Applicant Information:</b>	Name: Julie Salyers, J&B Builders, Inc.	Phone: 630-587-9900
	Address 525 S Tyler Road, Unit P St. Charles, IL 60174	Email: <a href="mailto:julie@jbbuilders.com">julie@jbbuilders.com</a>
<b>3. Record Owner Information:</b>	Name: River 504 LLC	Phone: 630-587-9900
	Address: 525 S Tyler Road, Unit P St. Charles, IL 60174	Email: <a href="mailto:brian@jbbuilders.com">brian@jbbuilders.com</a>

#### 4. Identify the Type of Application:

- ☐ New proposed Planned Unit Development (PUD) – *Special Use Application to be filed concurrently*
- ☒ Existing Planned Unit Development (PUD)
  - ☒ PUD Amendment required for proposed plan – *Special Use Application to be filed concurrently*

##### **Subdivision:**

- ☐ Proposed lot has already been platted and a new subdivision is not required.
- ☒ New subdivision of property is required:
  - ☐ Final Plat of Subdivision Application filed concurrently
  - ☒ Final Plat of Subdivision Application to be filed later

#### 5. Required Attachments:

*If multiple zoning or subdivision applications will be submitted concurrently, do not submit duplicate checklist items or plans. Fee must be paid for each application.*

- ☒ **APPLICATION FEE:** \$500
- ☐ **REIMBURSEMENT OF FEES AGREEMENT:** An original, executed Reimbursement of Fees Agreement and deposit of funds in escrow with the City, as provided by Appendix B of the Zoning Ordinance.
- ☐ **REIMBURSEMENT OF FEES INITIAL DEPOSIT:** Deposit of funds in escrow with the City. Required deposit is based on review items (number of applications filed) and the size of the subject property:

Number of Review Items	Under 5 Acres	5-15 Acres	16-75 Acres	Over 75 Acres
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- ☐ **PROOF OF OWNERSHIP:** a) A current title policy report; or  
b) A deed and a current title search

*NOTE: Private covenants and deed restrictions can limit private property rights with respect to the use of land even though the City's Zoning Ordinance may authorize the use or a less restrictive use. We strongly advise that you perform a title search on the property to determine if there any private covenants containing use restrictions or other deed restrictions. As those private covenants and deed restrictions may conflict with the City's Zoning Ordinance, it is further recommended that you consult with an attorney to obtain an opinion with respect to whether your intended use is compatible with those restrictions.*

- ☐ **OWNERSHIP DISCLOSURE:** Use the appropriate disclosure form (attached), if the owner or applicant is a Partnership, Corporation, Trust, or LLC.
- ☐ **LETTER OF AUTHORIZATION:** If the property owner is not the applicant, an original letter of authorization from the property owner permitting the applicant to file the zoning application with the City of St. Charles for the subject property.
- ☐ **LEGAL DESCRIPTION:** For entire subject property, on 8 1/2 x 11 inch paper and Microsoft Word file.
- ☐ **PLAT OF SURVEY:** A current plat of survey for the subject property showing all existing improvements on the property, prepared by a registered Illinois Professional Land Surveyor.

- ☐ **SOIL AND WATER CONSERVATION DISTRICT NATURAL RESOURCES INVENTORY APPLICATION:** As required by State law, submit a Natural Resources Inventory (NRI) application and required fee directly to the Kane-DuPage Soil and Water Conservation District. Provide a copy of completed NRI application to the City. The NRI application can be found on the Kane-DuPage SWCD website: <https://kanedupageswcd.org/kd/natural-resource-inventory>
- ☐ **ENDANGERED SPECIES REPORT:** As required by State law, file an Endangered Species Consultation Agency Action with the Illinois Department of Natural Resources. Provide a copy of the report to the City. The online Ecological Compliance Assessment Tool (EcoCAT) should be utilized: <https://dnr2.illinois.gov/EcoPublic/>
- ✓ ☒ **PRELIMINARY STORMWATER MANAGEMENT REPORT:** Provide stormwater management information as listed in item #7 of the Preliminary Engineering Checklist (attached).
- ✓ ☒ **BUILDING UTILITY DATA SUMMARY:** Use the attached worksheet to provide preliminary estimates regarding utility needs for the development.
- ✓ ☒ **PUBLIC BENEFITS, DEPARTURES FROM CODE:** A written statement describing how the PUD meets the purposes and requirements established in Section 17.04.400 of the Zoning Ordinance. List any requests for deviations (departures) from the requirements of Title 16 "Subdivisions and Land Improvement" and Title 17 "Zoning" and provide a rationale for each requested deviation.
- ✓ ☒ **CONSTRUCTION SCHEDULE:** Indicate the following:
  - Phases in which the project will be built with emphasis on area, density, use and public facilities, such as open space, to be developed with each phase. Overall design of each phase shall be shown on the Plat of Subdivision and through supporting material.
  - Approximate dates for beginning and completion of each phase.
- ☐ **PARK AND SCHOOL LAND/CASH WORKSHEETS:** *For residential developments only.* Use the attached worksheet to calculate the estimated population and student yields and resulting land/cash contributions in accordance with Title 16 of the St. Charles Municipal Code.
- ✓ ☒ **INCLUSIONARY HOUSING WORKSHEET:** *For residential developments only.* Use the attached worksheet to calculate the affordable unit requirement and indicate how the development will comply with Title 19 of the St. Charles Municipal Code.
- ☐ **KANE COUNTY TRANSPORTATION IMPACT FEE:** The Kane County Road Improvement Impact Fee Ordinance applies to new residential and non-residential development within Kane County. The impact fee is determined by Kane County upon submittal of an application to Kane County Department of Transportation at the time of building permit. At this stage, the Impact Fee Estimator Tool can be used to estimate the cost of the fee: <http://kdot.countyofkane.org/Pages/Impact-Fees.aspx>

**PLANS:** All required plans shall be drawn on sheets no larger than 24" x 36", unless the Director of Community Development permits a larger size when necessary to show a more comprehensive view of the project. All required plans shall show north arrow and scale, and shall be drawn at the same scale (except that a different scale may be used to show details or specific features). All plans shall include the name of the project, developer or owner of site, person or firm preparing the plan, and the date of plan preparation and all revisions.

**Copies:** Ten (10) full size copies and PDF electronic file emailed to: [cd@stcharlesil.gov](mailto:cd@stcharlesil.gov)

- ✓ ☒ **COMMENT RESPONSE LETTER:** Provide a written response to all outstanding City review comments from the Concept Plan review (if applicable).

- ✓ **PRELIMINARY PLAT OF SUBDIVISION:** If the PUD Preliminary Plan involves the subdivision of land, submit a Preliminary Plat of Subdivision that includes the information listed on the Subdivision Plat Drawing Requirements Checklist. Also submit a completed Subdivision Plat Drawing Requirements Checklist (attached).
- ✓ **PRELIMINARY ENGINEERING PLANS:** Plans shall include the information listed on the Preliminary Engineering Checklist (checklist).
- ✓ **PHOTOMETRIC PLAN:** Site lighting plan demonstrating compliance with Section 17.22.040 "Site Lighting". Depict proposed lighting levels within the site and up to all property lines. Identify locations and fixture specifications of all site lighting, including pole and building-mounted lighting.
- ✓ **ARCHITECTURAL PLANS:** Architectural plans and data for all principal buildings with sufficient detail to permit an understanding of the exterior appearance and architectural style of the proposed buildings, the number, size and type of dwelling units, the proposed uses of nonresidential and mixed-use buildings, total floor area and total building coverage of each building.
- ✓ **LANDSCAPE PLAN:** Landscape Plan demonstrating compliance with Ch. 17.26 "Landscaping & Screening" and which includes following information and:
  - Delineation of the buildings, structures, and paved surfaces situated on the site and/or contemplated to be built thereon
  - Delineation of all areas to be graded and limits of land disturbance, including proposed contours as shown on the Site/Engineering Plan
  - Accurate property boundary lines
  - Accurate location of proposed structures and other improvements, including paved areas, berms, lights, retention and detention areas, and landscaping
  - Site area proposed to be landscaped in square feet and as a percentage of the total site area
  - Percent of landscaped area provided as per code requirement
  - Dimensions of landscape islands
  - Setbacks of proposed impervious surfaces from property lines, street rights-of-way, and private drives
  - Location and identification of all planting beds and plant materials
  - Planting list including species of all plants, installation size (caliper, height, or spread as appropriate) and quantity of plants by species
  - Landscaping of ground signs and screening of dumpsters and other equipment
- **TREE PRESERVATION PLAN:** When required, in accordance with Ch. 8.30 of the St. Charles Municipal Code (see attached "Tree Preservation Requirements for Preliminary Plans". This information may be included as part of the Landscape Plan set.
- ✓ **ELECTRIC UTILITY PLAN & ELECTRIC SERVICE APPLICATION:** Provide electric utility information as listed in item #5 of the Preliminary Engineering Checklist. Also fill out and submit the Electric Service Application form (attached). Please note: The St. Charles Municipal Electric Utility is a "zero standard" utility, which means that the developer pays 100% of the costs related to electrical infrastructure.
- ✓ **TRUCK TURNING EXHIBIT(S):** Provide a WB-50 template showing turning movements through the site to verify adequate access for Fire vehicles. Provide a Vactor truck template if the project will require City Vactor truck access for sanitary sewer maintenance.
- **SKETCH PLAN FOR LATER PHASES OF PUD:** For phased PUDs only, submit a plan that shows, at minimum, the following:
  - General location of arterial and collector streets

- Location of any required landscape buffers
- Location of proposed access to the site from public streets
- Maximum number of square feet of floor area for nonresidential development
- Maximum number of dwelling units for residential development
- Open space and storm water management land

I (we) certify that this application and the documents submitted with it are true and correct to the best of my (our) knowledge and belief.

Ben P. Bay Mar 3/13/25  
Record Owner Date

[Signature] 3/13/2025  
Applicant or Authorized Agent Date



## PUBLIC BENEFITS, DEPARTURES FROM CODE

Development Name: River 504 Rowhomes

Location: NE corner of S. 1<sup>st</sup> & Prairie Streets

The modifications will revert the use more closely back to the previously approved Brownstone PUD in 2000 while maintaining the purpose and requirements established in Section 17.04.400 of the Zoning Ordinance. Currently vacant undeveloped open space, the proposed development will serve as part of the southern gateway to downtown by providing residential housing and on-street parking. The proposed facades are oriented to S 1<sup>st</sup> Street and promote walkability to downtown by harmoniously extending the existing streetscape from the properties located to the north. While the Brownstone PUD in 2000 included two buildings with 5 units per building, the proposed development includes four buildings, four stories high with 2 units in each building. The gross square footage of each unit is 6,226 sf. The architectural design includes a “four sided” building with a traditional feel that will provide architectural interest to the neighborhood incorporating the height and silhouette components of the Milestone 1 Building to the north while maintaining a residential feel to compliment the Brownstone development to the east.

We are requesting the following deviations from the 2005 PUD Ordinance:

1. Front Elevation Setback: Current setback requirement is 5.0'. With dedication of full sidewalk width to the city, we are requesting a 3.4' setback.
2. Building Height: Maximum building height of 57'. The maximum building height is calculated per the St. Charles zoning ordinance and reflects the distance from the finished grade to the roof ridge at the S 1<sup>st</sup> Street elevation. The proposed ridge height for River 504 is slightly lower than the ridge height of the Milestone 1 property and approximately 3 feet lower than the height of the towers located at both ends of the two Milestone 1 buildings.

## INCLUSIONARY HOUSING WORKSHEET

Name of Development: River 504 Rowhomes

Date Submitted:

Use this worksheet to determine the affordable unit requirement for the proposed development and to propose how the development will meet the Inclusionary Housing requirements of Title 19.

### 1) Calculate the number of affordable units required:

Unit Count Range	# of Units Proposed in Development		% of Affordable Units Required		# of Affordable Units Required
1 to 15 Units	8	X	5%	=	0.4
More than 15 Units		X	10%	=	

### 2) How will the Inclusionary Housing requirement be met?

- ☐ Provide on-site affordable units
- ☒ Pay a fee in-lieu of providing affordable units (calculate fee in-lieu below)
- ☐ Provide a mixture of affordable units and fee in-lieu
- # of affordable units to be provided:
  - Amount of fee in-lieu to be paid (calculate below):

#### Fee In-Lieu Payment Calculation

##### Multi-Family Development:

# of Affordable Units Required	# of Affordable Units Proposed to Pay the Fee-In-Lieu		Fee-In-Lieu Amount Per Affordable Unit		Total Fee-In-Lieu Amount
		X	\$52,454	=	

##### Single-Family Attached (Townhouse) / Two-Family Development:

# of Affordable Units Required	# of Affordable Units Proposed to Pay the Fee-In-Lieu		Fee-In-Lieu Amount Per Affordable Unit		Total Fee-In-Lieu Amount
0.4	0.4	X	\$36,718	=	14,687

##### Single-Family Development:

# of Affordable Units Required	# of Affordable Units Proposed to Pay the Fee-In-Lieu		Fee-In-Lieu Amount Per Affordable Unit		Total Fee-In-Lieu Amount
		X	\$20,982	=	

**402 - 542 SOUTH 1ST STREET  
ST. CHARLES, KANE COUNTY, ILLINOIS**

ALL ITEMS OF THIS PROJECT SHALL BE GOVERNED BY SPECIFICATIONS INCLUDED IN THE DOCUMENTS LISTED BELOW:

A. "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" PREPARED BY THE DEPARTMENT OF TRANSPORTATION OF THE STATE OF ILLINOIS AND ADOPTED BY SAID DEPARTMENT

B. "LATEST REVISIONS" OF THE SPECIFICATIONS HEREIN REFERRED TO AS THE "STANDARD SPECIFICATIONS"

C. "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION.

D. "ILLINOIS MANUAL" (LATEST REVISION) BY ASSOCIATION OF ILLINOIS SOIL & WATER CONSERVATION DISTRICTS.

E. "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS", BY ILLINOIS ENVIRONMENTAL PROTECTION AGENCY.

F. "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS & HIGHWAYS" BY ILLINOIS DEPARTMENT OF TRANSPORTATION.

G. "ILLINOIS ACCESSIBILITY CODE", (LATEST EDITION).

H. "LATEST REVISION IN EFFECT ON THE DATE OF THESE PLANS

2. PRIOR TO BID, THE CONTRACTOR SHALL VERIFY CONFORMANCE BETWEEN PLANS AND THE ABOVE REFERENCED CODES. THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF ANY DISCREPANCIES PRIOR TO BID.

3. IN THE EVENT OF A CONFLICT BETWEEN THESE VARIOUS STANDARDS, MUNICIPAL STANDARDS SHALL APPLY.

4. ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH SECTION 701 OF THE STANDARD SPECIFICATIONS.

5. ALL WORK PERFORMED SHALL COMPLY WITH ALL APPLICABLE RULES AND REGULATIONS OF OSHA.

6. THE CONTRACTOR IS RESPONSIBLE FOR NOTIFYING JULIE 1-800-892-0123 AT LEAST 48 HOURS IN ADVANCE OF CONSTRUCTION OPERATIONS. ALL UTILITIES MUST BE STAKED/LOCATED BY THE CONTRACTOR BEFORE CONSTRUCTION AND NOTIFY THE ENGINEER OF ANY DISCREPANCIES.

7. THE CONTRACTOR IS RESPONSIBLE FOR NOTIFYING THE MUNICIPAL PUBLIC WORKS DEPARTMENT 630-377-4405 A MINIMUM OF 48 HOURS BEFORE CONSTRUCTION STARTS. A 24-HOUR NOTICE MUST BE PROVIDED FOR INSPECTIONS AND TESTS. MUNICIPAL STAFF MUST PROTECT ALL WATERMAIN VALVES AND HYDRANTS ONLY.

8. THE CONTRACTOR SHALL OPERATE AND PRESERVE ALL SURVEYING MONUMENTS. UNLESS THE OWNER, THEIR AGENT OR A LICENSED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

9. THE CONTRACTOR SHALL BE AWARE OF POTENTIAL CONFLICTS WITH EXISTING UTILITIES AS MAY BE INDICATED ON THE PLANS. THESE AREAS SHALL BE EXCAVATED TO DETERMINE ELEVATIONS BEFORE BEGINNING CONSTRUCTION. CONTRACTOR SHALL NOTIFY THE ENGINEER OF ANY DISCREPANCIES.

10. SAVING OF REMOVAL ITEMS AS NOTED IN THE PLANS OR AS REQUIRED BY THE ENGINEER SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT OF THE ITEM BEING REMOVED AND NO EXTRA COMPENSATION WILL BE ALLOWED.

11. ALL ROAD SIGNS, STREET SIGNS AND TRAFFIC SIGNS WHICH NEED TO BE RELOCATED, OR MOVED DUE TO CONSTRUCTION SHALL BE TAKEN DOWN AND STORED BY THE CONTRACTOR AT HIS OWN EXPENSE EXCEPT THOSE WHICH THE PERMIT AGENCY SHALL REQUIRE TO BE TEMPORARILY RESET UNTIL COMPLETION OF CONSTRUCTION OPERATIONS. AFTER COMPLETION OF THE WORK, THE CONTRACTOR SHALL, AT HIS OWN EXPENSE, RESET ALL SIGN SIGNS AT THE LOCATIONS DESIGNATED BY THE PERMIT AGENCY ENGINEER.

12. THE CONTRACTOR SHALL PRESERVE ALL CONSTRUCTION STAKES UNTIL THEY ARE NO LONGER NEEDED. ANY STAKES DESTROYED BY THE CONTRACTOR PRIOR TO THEIR USE SHALL BE RESET BY THE OWNERS ENGINEER AT A COST TO BE BORNE BY THE CONTRACTOR THAT DESTROYED THE STAKES.

13. CONTRACTOR SHALL USE CARE NEAR ANY AND ALL EXISTING ITEMS WHICH ARE NOT INDICATED TO BE REMOVED OR MODIFIED. IF ANY DAMAGE TO THESE ITEMS OCCURS DURING CONSTRUCTION OPERATIONS SHALL BE REPAIRED AND/OR RESTORED BY THE CONTRACTOR AT THE CONTRACTORS OWN EXPENSE.

14. THE CONTRACTOR SHALL INDEMNIFY AND SAVE HARMLESS THE OWNER, THE PERMIT AGENCY AND ITS OFFICERS, EMPLOYEES, AND AGENTS, AND THE OWNERS ENGINEERS, FROM AND AGAINST ALL LOSSES, CLAIMS, DAMAGES, PAYMENTS, SUITS, AND THE JUDGMENT OF EVERY NATURE AND DESCRIPTION BROUGHT OR RECOVERED AGAINST THEM BY REASON OF ANY ACT OR OMISSION OF SAID CONTRACTOR, HIS AGENTS OR EMPLOYEES, IN THE EXECUTION OF THE WORK OR IN THE GUARDING OF IT.

15. BURNING ON THE SITE IS NOT PERMITTED.

16. ALL IMPROVEMENTS SHOWN ON THE PLANS ARE THE RESPONSIBILITY OF THE CONTRACTOR.



OVERALL SUBDIVISION:  
29,385 SF 0.675 AC

RIGHT-OF-WAY TO BE DEDICATED  
947 SF 0.022 AC

TOTAL LOTS:  
28,438 SF 0.653 AC


[illegible]

<u>SHEET NO.</u>	<u>SHEET NAME</u>
C1	TITLE SHEET
C2	EXISTING CONDITIONS PLAN
C3	DEMOLITION AND REMOVALS
C4	PROPOSED GEOMETRY
C5	PROPOSED UTILITY PLAN
C6	PROPOSED GRADING PLAN

<u>SHEET NO.</u>	<u>SHEET NAME</u>
C1	TITLE SHEET
C2	EXISTING CONDITIONS PLAN
C3	DEMOLITION AND REMOVALS
C4	PROPOSED GEOMETRY
C5	PROPOSED UTILITY PLAN
C6	PROPOSED GRADING PLAN



PROJECT TEAM		NOTE	BENCHMARKS	ABBREVIATIONS
OWNER	<b>RIVER 504, LLC</b> 505 S. RIVER RD. UNIT P ST. CHARLES, IL 60174 PH: (630)587.9900	1. DETAILS & NOTES WITHIN THESE PLANS, ALONG WITH NOTATIONS TO SUPPLEMENT SAME ARE CRUCIAL TO THE PROPER CONSTRUCTION OF THE DESIGN CONTAINED HEREIN.  2. CONTRACTOR SHALL VISIT THE SITE TO FAMILIARIZE HIMSELF WITH ALL CONDITIONS PRIOR TO ENGAGING IN A CONTRACT TO PERFORM WORK AS PROPOSED HEREIN.  3. PLANS ARE NOT VALID WITHOUT ORIGINAL SIGNATURE AND SEAL OF THE ENGINEER.	NAVD83, AS ESTABLISHED BY TRIMBLE VRS NETWORK AT TIME OF TOPGRAPHY SURVEY.  HORIZONTAL DATUM IS REFERENCING NAD'83	A ADJUSTED ALT ALTIMETER B BACK SIGHT C CLOSURE D DIRECTION E ELEVATION F FATHOMS G GROUND H HORIZONTAL I INTERSECTION J JUNCTION K KNOTS L LEVEL M MILE N NORTH O OCEAN P POINT Q QUANTITY R READING S SURFACE T TIDE U UTM V VERTICAL W WIND X X-AXIS Y Y-AXIS Z Z-AXIS
ARCHITECT	<b>ALLEN+PEPPA</b> 215 RULIFSON STREET GENEVA, IL 60134 PH: (630)578.1105		SITE BENCHMARK #1: TAG BOLT ON FIRE HYDRANT LOCATED AT SOUTHWESTERLY CORNER OF SITE. N 1,909,513.9' E 989,773.6' EL 697.17'	R REBAR S SURFACE T TIDE U UTM V VERTICAL W WIND X X-AXIS Y Y-AXIS Z Z-AXIS
CIVIL ENGINEER	<b>MeritCorp Group, LLC</b> 4222 MERIDIAN PARKWAY STE 112 AURORA, IL 60504 PH: (630)554.6555		SITE BENCHMARK #2: NW BOLT ON FIRE HYDRANT LOCATED APPROXIMATELY 635' SE OF SITE. N 1,908,946.5' E 990,004.4' EL 697.17'	A ADJUSTED ALT ALTIMETER B BACK SIGHT C CLOSURE D DIRECTION E ELEVATION F FATHOMS G GROUND H HORIZONTAL I INTERSECTION J JUNCTION K KNOTS L LEVEL M MILE N NORTH O OCEAN P POINT Q QUANTITY R READING S SURFACE T TIDE U UTM V VERTICAL W WIND X X-AXIS Y Y-AXIS Z Z-AXIS

RIVER 504 ROWHOMES 402 - 542 SOUTH 1ST STREET ST. CHARLES, ILLINOIS		 <b>MeritCorp</b> PROFESSIONAL PLANNING SERVICES, INCORPORATED 4222 Meridian Parkway, Suite 112 Aurora, IL 60094 Office 630.554.6555 Lic. No. 184-003860 www.meritcorp.com	
PROJECT NO. M24183		DATE: 03/10/25	
DRAWN BY: ZDS		ISSUED TO CITY OF ST. CHARLES	
CHECKED BY: TDR		REV'D PER CLIENT & PER CITY OF ST. CHARLES	
SHEET NO. C1 / 6		REV'D PER CITY OF ST. CHARLES	
TITLE SHEET			
NOT FOR CONSTRUCTION		Gurnee, IL	

NOT FOR CONSTRUCTION

# LEGEND

	EXISTING HANDHOLE
	EXISTING CURB
	EXISTING MANHOLE
	EXISTING CABLE TV PEDISTAL
	EXISTING ELECTRIC PEDISTAL
	EXISTING TELEPHONE PEDISTAL
	EXISTING MANHOLE
	EXISTING STORM CATCH BASIN
	EXISTING STORM INLET
	EXISTING STORM MANHOLE
	EXISTING FIRE HYDRANT
	EXISTING WATER BUFFALO BOX
	EXISTING WATER VALVE
	EXISTING WATER VALVE & VAULT
	EXISTING DEAD-END TREE (LEAF TREE)
	EXISTING CONSERVING TREE (NON-LEAF TREE)
	EXISTING ELECTRIC HANDHOLE
	EXISTING ELECTRIC MANHOLE
	EXISTING ELECTRIC TRANSFORMER
	EXISTING BEER OPTICS HANDHOLE
	EXISTING TELEPHONE MANHOLE
	EXISTING POWER POLE
	EXISTING GUY WIRE
	CONTROL POINT - CROSS CUT

# BENCHMARKS

SOURCE: NAVD83, AS ESTABLISHED BY TRIMBLE VRS NETWORK AT TIME OF TOPOGRAPHIC SURVEY.  
HORIZONTAL DATUM IS REFERENCING NAD 83

**SITE BENCHMARK #1:**  
TAG BOLT ON FIRE HYDRANT LOCATED AT SOUTHWESTERLY CORNER OF SITE.  
N 1.900.513.9' E 985.773.9'  
EL 697.17'

**SITE BENCHMARK #2:** (SEE SITE KEY FOR LOCATION OF OFF-SITE BM2)  
NAIL BOLT ON FIRE HYDRANT LOCATED APPROXIMATELY 630' SE OF SITE.  
N 1.900.046.9' E 990.066.4'  
EL 701.40'

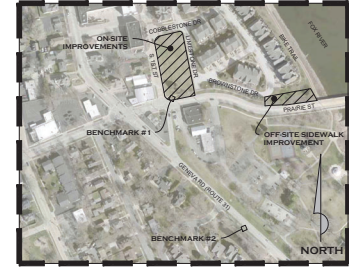
TOPOGRAPHIC SURVEY INFORMATION SHOWN FOR ON-SITE IMPROVEMENTS WAS COMPLETED BY REGIONAL LAND SERVICES. BENCHMARK INFORMATION IS SHOWN PER SAID SURVEY, AND WAS NOT COMPLETED BY MERITCORP GROUP, LLC.

TOPOGRAPHIC SURVEY INFORMATION SHOWN FOR OFF-SITE IMPROVEMENTS WAS COMPLETED BY MERITCORP GROUP, LLC ON 03/10/25

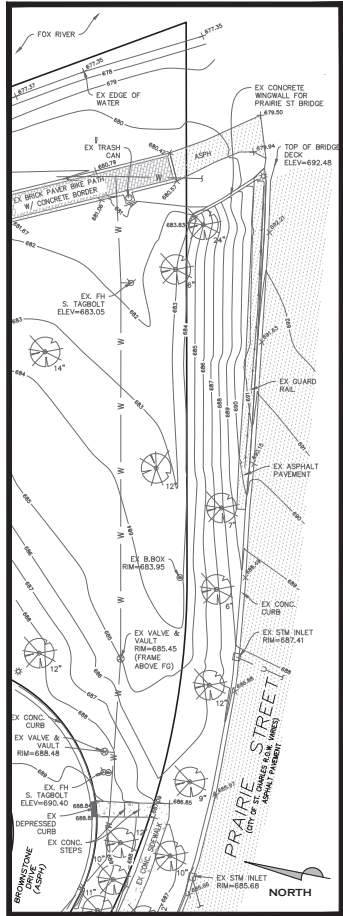
# UTILITY LOCATE INFO

JULIE DESIGN STAGE TICKET NUMBER: A25091350-004

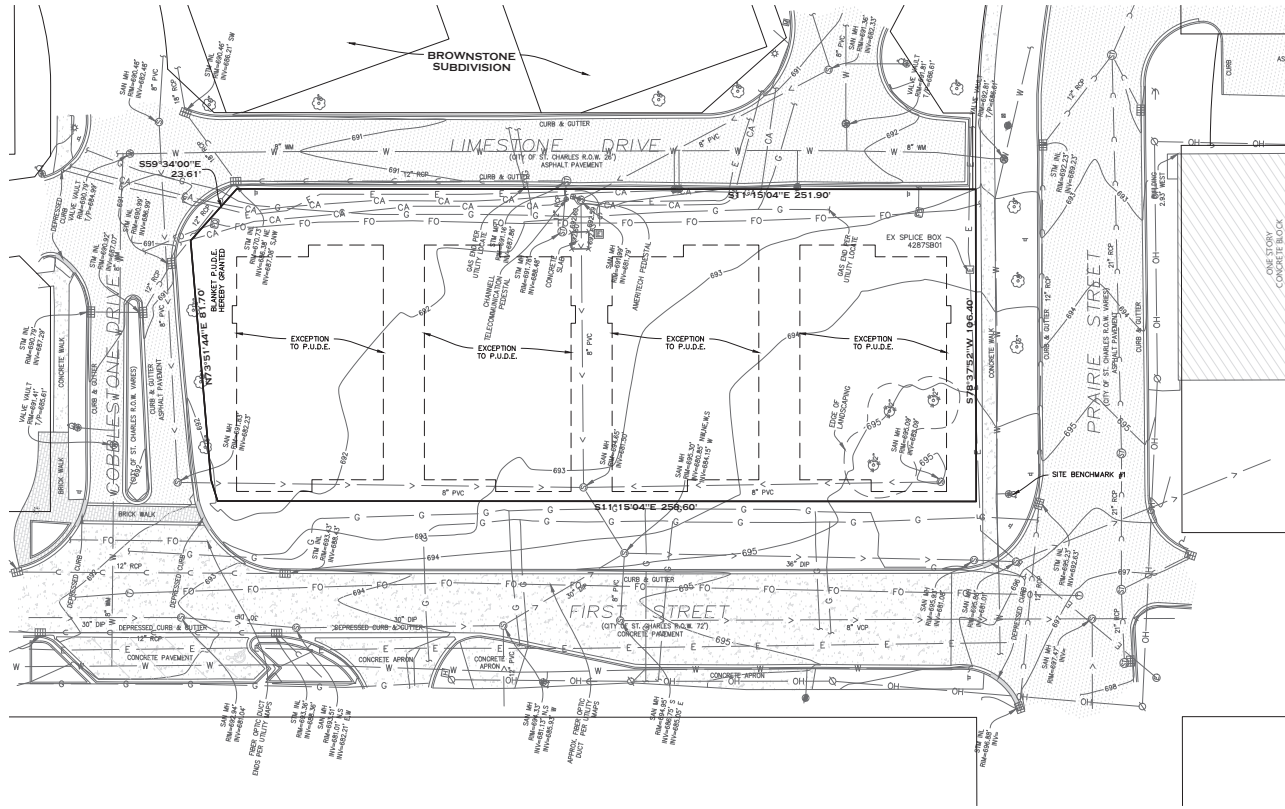
CONTACTS:  
ATTOSIA AT DISTRIBUTION, 800-800-0000, [J11629@ATT.COM](mailto:J11629@ATT.COM)  
COMCAST CABLE (INFINITY), MARTHA GERAS, 224-225-5862, [MARTHA.GERAS@CABLE.COMCAST.COM](mailto:MARTHA.GERAS@CABLE.COMCAST.COM)  
MIRADA METRO FIBERNET, LLC, 903-213-1100, [BLISS@MIRADAFIBERNET.COM](mailto:BLISS@MIRADAFIBERNET.COM)  
NORIDA NORIS GAS, UTILITY CONSULTANT GROUP, 630-388-2362  
SCHIBA ST. CHARLES, CITY OF, ERIC CREIGHTON, 630-762-7075, [MAPS@STCHARLES.IL.GOV](mailto:MAPS@STCHARLES.IL.GOV)



SITE KEY  
NTS



OFF-SITE SIDEWALK IMPROVEMENT  
SCALE: 1"=20'



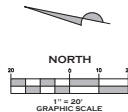
ON-SITE IMPROVEMENTS  
SCALE: 1"=20'

# NOTE

EXISTING ON-SITE TOPOGRAPHIC SURVEY WAS COMPLETED BY REGIONAL LAND SERVICES ON 12/21/21

EXISTING OFF-SITE TOPOGRAPHIC SURVEY WAS COMPLETED BY MERITCORP GROUP, LLC ON 03/10/25

EXISTING UTILITY SURVEY WAS UPDATED BY MERITCORP GROUP, LLC ON 02/06/25



GIVEN UNDER MY HAND AND SEAL ON THIS 5TH DAY OF MARCH, 2025.

*John D. Spink*  
JOHN D. SPINK  
ILLINOIS PROFESSIONAL LAND SURVEYOR No. 3868  
CURRENT LICENSE EXPIRES NOVEMBER 30, 2026.



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**MeritCorp**

DESCRIPTION: RIVER 504 ROWHOMES  
402-542 SOUTH 1ST STREET  
ST. CHARLES, ILLINOIS  
REV'D PER CLIENT & PER CITY OF ST. CHARLES  
REV'D PER CITY OF ST. CHARLES

DATE: 05/22/25  
DATE: 07/11/25

PROJECT NO. M24183  
DRAWN BY: ZDS  
CHECKED BY: TDR  
SHEET NO. C2 / 6

PRELIMINARY ENGINEERING PLANS  
EXISTING CONDITIONS PLAN  
NOT FOR CONSTRUCTION

Other Office Locations:  
Aurora, IL 00594  
Office 630.554.6655  
Lic. No. 184-006860  
www.meritcorp.com





# LEGEND

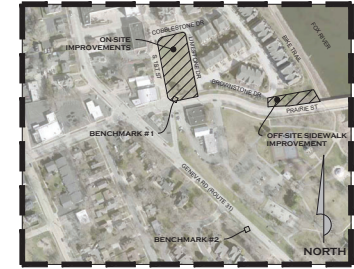
	DEPRESSED CURB AND GUTTER (SEE DETAIL SHEET)
	REVERSE PITCH CURB AND GUTTER (SEE DETAIL SHEET)
	HEAVY DUTY ASPHALT PAVEMENT (SEE DETAIL SHEET)
	STANDARD DUTY ASPHALT PAVEMENT (SEE DETAIL SHEET)
	CONCRETE PAVEMENT (SEE DETAIL SHEET)
	PROPERTY LINE
	EDGE OF STRIPE
	CENTER OF STRIPE
	BUILDING
	BACK OF CURB
	EDGE OF WALK
	RADIUS

# NOTES

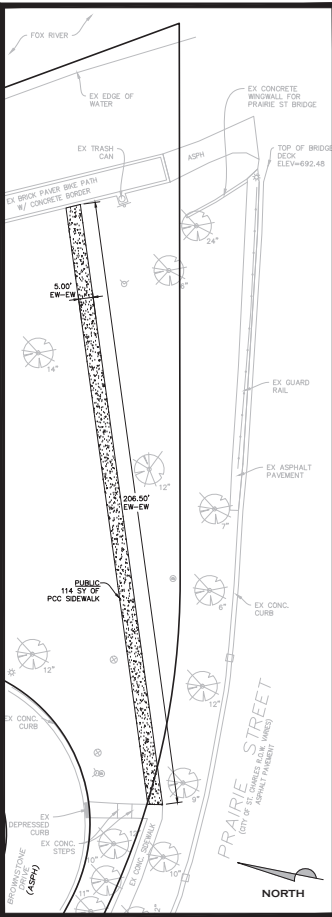
1. REFER TO CITY OF ST. CHARLES ENGINEERING DESIGN DETAILS FOR ALL SIDEWALK, PAVEMENT AND UTILITY DETAILS.
2. REFER TO ARCHITECTURAL PLANS FOR ALL BUILDING DIMENSIONS AND PRIVACY WALL DETAILS.
3. REFER TO PRELIMINARY PLAT FOR LOT DIMENSIONS & ZONING.
4. TRENCHBOX MAY BE REQUIRED TO CONSTRUCT PORTIONS OF THE SANITARY SEWER IN THE PRAIRIE STREET RIGHT-OF-WAY.

# PE CALC

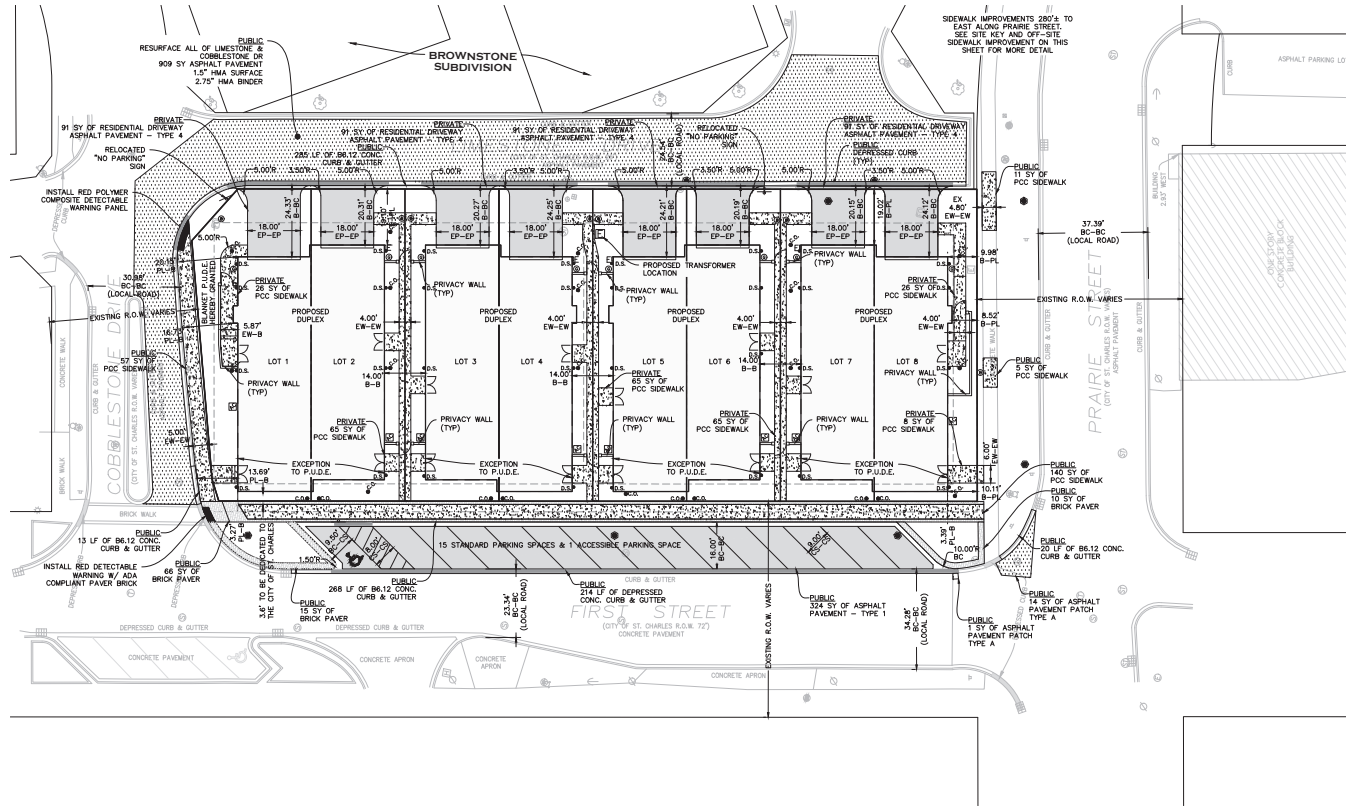
USING THE "SINGLE FAMILY HOME" DESIGNATION FROM THE IEPA @ 3.5 CAPITA \* 100 GPD/CAPITA.  
THE PE FOR EACH OF THE UNITS (1 THRU 8) = 359 GPD EACH FOR A TOTAL OF 2,880 GPD



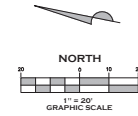
SITE KEY  
NTS



OFF-SITE SIDEWALK IMPROVEMENT  
SCALE: 1"=20'



ON-SITE IMPROVEMENTS  
SCALE: 1"=20'



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**MeritCorp**

DESCRIPTION: RIVER 504 ROWHOMES  
402 - 542 SOUTH 1ST STREET  
ST. CHARLES, ILLINOIS  
REV'D PER CLIENT & PER CITY OF ST. CHARLES  
REV'D PER CITY OF ST. CHARLES

DATE: 07/25  
DATE: 09/22/25  
DATE: 07/11/25

PROJECT NO. M24183  
DRAWN BY: ZDS  
CHECKED BY: TDR  
SHEET NO. C4 / 6

Other Office Locations:  
Gurnee, IL

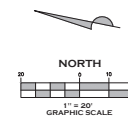
4222 Meridian Parkway, Suite 112  
Aurora, IL 60594  
Office: 630.554.6655  
Lic. No. 184-009880  
www.meritcorp.com

PRELIMINARY ENGINEERING PLANS  
PROPOSED GEOMETRY  
NOT FOR CONSTRUCTION



— OH —	OVERHEAD WIRES
FO	FIBER OPTIC
— CA —	COMMUNICATION LINE
— T — T —	TELEPHONE LINE
— E —	ELECTRIC LINE
— G —	GAS LINE
— < —	SANITARY LINE
— C —	SEWER SOWER
— W —	WATER LINE
●	SANITARY MANHOLE
● C.O.	CLEAN OUT
● O.S.	DOWNSPOUT
⌋	VALVE VAULT
⌋	FIRE HYDRANT
⊙	BUFFALO BOX
⊙	GAS METER
⊙	ELECTRIC METER (BY OTHERS)

1. REFER TO CITY OF ST. CHARLES ENGINEERING DESIGN DETAILS FOR ALL SIDEWALK, PAVEMENT AND UTILITY DETAILS.
2. REFER TO ARCHITECTURAL & MEP PLANS FOR ADDITIONAL DETAILS.
3. REFER TO PRELIMINARY PLAT FOR PROPOSED EASEMENTS & DESCRIPTIONS.
4. REFER TO ELECTRICAL PLANS FOR ELECTRICAL DESIGN AND CONDUIT ROUTING.
5. REFER TO ARCHITECTURAL PLANS FOR EXACT LOCATIONS OF DOWNSPOUTS.



		RIVER 504 ROWHOMES 402 · 542 SOUTH 1ST STREET ST. CHARLES, ILLINOIS	
		PRELIMINARY ENGINEERING PLANS	
		PROPOSED UTILITY PLAN	
PROJECT NO. M24183			
DRAWN BY: ZDS			
CHECKED BY: TDR			
SHEET NO. C5 / 6			

## LEGEND

	DEPRESSED CURB AND GUTTER (SEE DETAIL SHEET)
	REVERSE PITCH CURB AND GUTTER
	GRADING CONTOUR
	EMERGENCY OVERLAND FLOOD ROUTE
	DRAINAGE FLOW ARROW
	TOP OF FOUNDATION
	GARAGE FLOOR
	TOP OF STEP
	BOTTOM OF STEP
	EDGE OF PAVEMENT
	TOP OF CURB
	TOP OF DEPRESSED CURB
	MATCH EXISTING
	TOP OF WALL
	BOTTOM OF WALL
	HIGH WATER LEVEL
	NORMAL WATER LEVEL

## BENCHMARKS

SOURCE: NAVD83, AS ESTABLISHED BY TRIMBLE VRS NETWORK AT TIME OF TOPOGRAPHIC SURVEY. HORIZONTAL DATUM IS REFERENCING NAD 83

SITE BENCHMARK #1:  
TAG BOLT ON FIRE HYDRANT LOCATED AT SOUTHWESTERLY CORNER OF SITE.  
N 1,900.513.9' E 988.773.9'  
EL 697.17'

SITE BENCHMARK #2: (SEE SITE KEY FOR LOCATION OF OFF-SITE BENCHMARK)

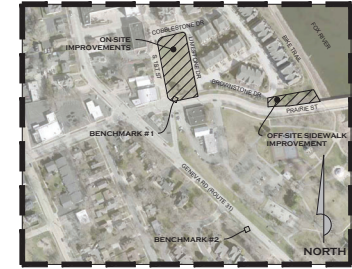
NW BOLT ON FIRE HYDRANT LOCATED APPROXIMATELY 630.56' SE OF SITE.  
N 1,908.046.9' E 988.080.4'  
EL 701.40'

TOPOGRAPHIC SURVEY INFORMATION SHOWN FOR ON-SITE IMPROVEMENTS WAS COMPLETED BY REGIONAL LAND SERVICES. BENCHMARK INFORMATION IS SHOWN PER SAID SURVEY, AND WAS NOT COMPLETED BY MERITCORP GROUP, LLC.

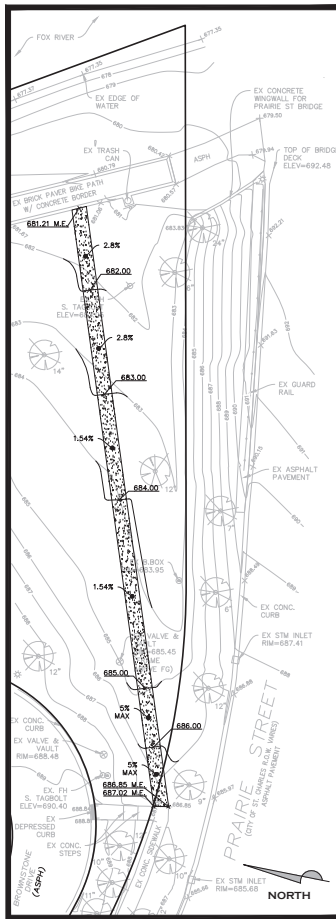
TOPOGRAPHIC SURVEY INFORMATION SHOWN FOR OFF-SITE IMPROVEMENTS WAS COMPLETED BY MERITCORP GROUP, LLC ON 03/01/25

## NOTES

1. STORMWATER MANAGEMENT FACILITIES INCLUDING DETENTION, BMP'S ARE NOT REQUIRED FOR THIS DEVELOPMENT.

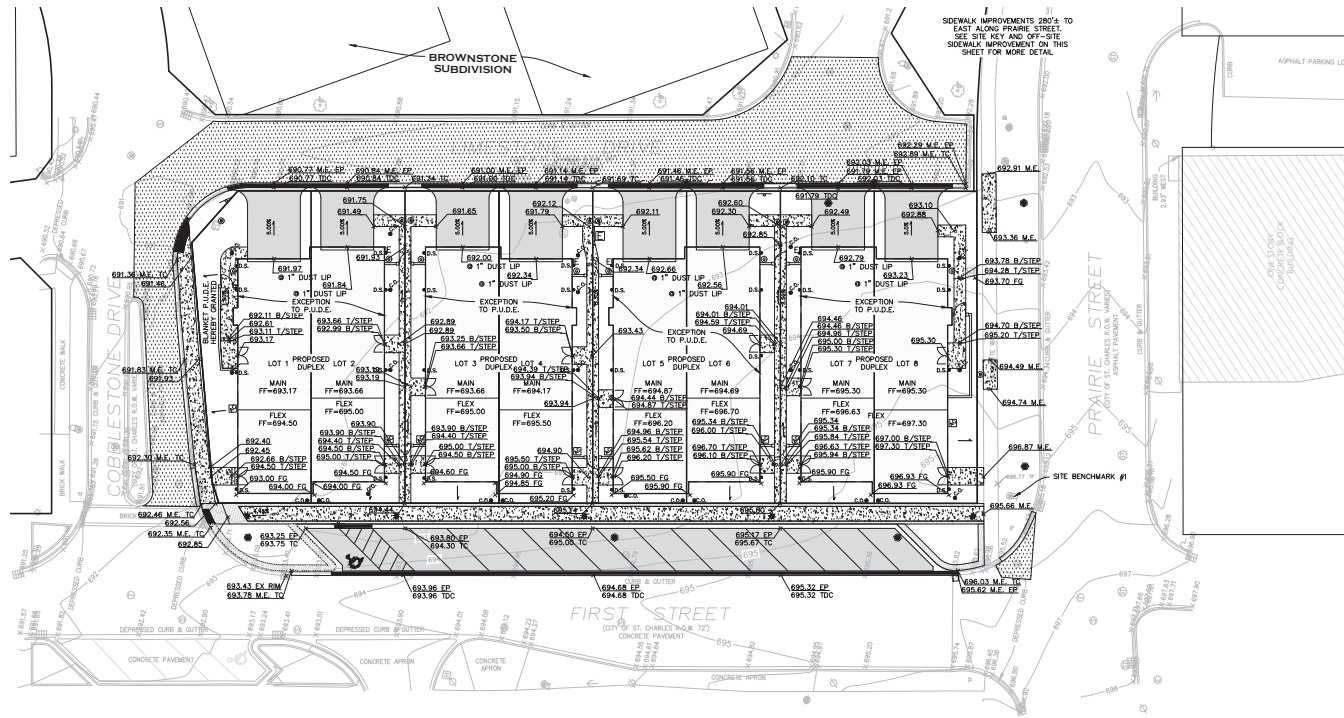


SITE KEY  
NTS



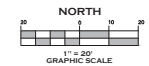
OFF-SITE SIDEWALK IMPROVEMENT

SCALE: 1"=20'



ON-SITE IMPROVEMENTS

SCALE: 1"=20'




COPYRIGHT © 2025	
<b>MeritCorp</b>	
DATE: 07/25/25	DESCRIPTION: RIVER 504 ROWHOMES 402-542 SOUTH 1ST STREET ST. CHARLES, ILLINOIS
DATE: 07/25/25	REVISION: REV'D PER CLIENT & PER CITY OF ST. CHARLES
DATE: 07/11/25	REVISION: REV'D PER CITY OF ST. CHARLES
<b>MeritCorp</b> ENGINEERING: PLANNING, SURVEYING, ENVIRONMENTAL 4222 Meridian Parkway, Suite 112 Aurora, IL 60594 Office: 630.554.6655 Lic. No. 184-008880 www.meritcorp.com Other Office Locations: Gurnee, IL	
PROJECT NO. M24183 DRAWN BY: ZDS CHECKED BY: TDR SHEET NO. C6 / 6	

NOT FOR CONSTRUCTION





Notes:	
- Mount Heights = MH as shown	
- No minor obstructions included in calculations Obstructions will affect footcandle levels	
 <div data-bbox="1948 1273 2047 1334"> <p>The Lighting Digest          210 W. State Street          Geneva, IL 60134          (800) 566-2274          Email Address:  <a href="mailto:info@TheLightingDigest.com">info@TheLightingDigest.com</a></p> </div>	
Project:	Project #
<b>Rowhome Development</b>	<b>250275</b>
Drawn By:	Date:
<b>Jeff Cascarelli</b>	<b>7/17/2025</b>
	Scale:
	<b>1" = 20ft</b>
Disclaimer:	
<p>This document represents The Lighting Digest's estimate only for informational purposes. The Lighting Digest does not representation or warranties regarding the calculations, measurements, or details presented in this drawing for any specific application or project. The data, calculations, and information are provided for general informational purposes only and are not intended to be used for any specific application or project. The user of this document is responsible for verifying the accuracy and completeness of the data and information provided and for obtaining all necessary permits and approvals. The Lighting Digest is not responsible for any errors, omissions, or damages of any kind resulting from the use of this document. The Lighting Digest is not a professional engineering or architectural firm and does not provide such services. The Lighting Digest is not responsible for any claims, damages, or liabilities of any kind resulting from the use of this document. The Lighting Digest is not responsible for any claims, damages, or liabilities of any kind resulting from the use of this document.</p>	



1 1ST STREET ELEVATION  
3/32" = 1'-0"



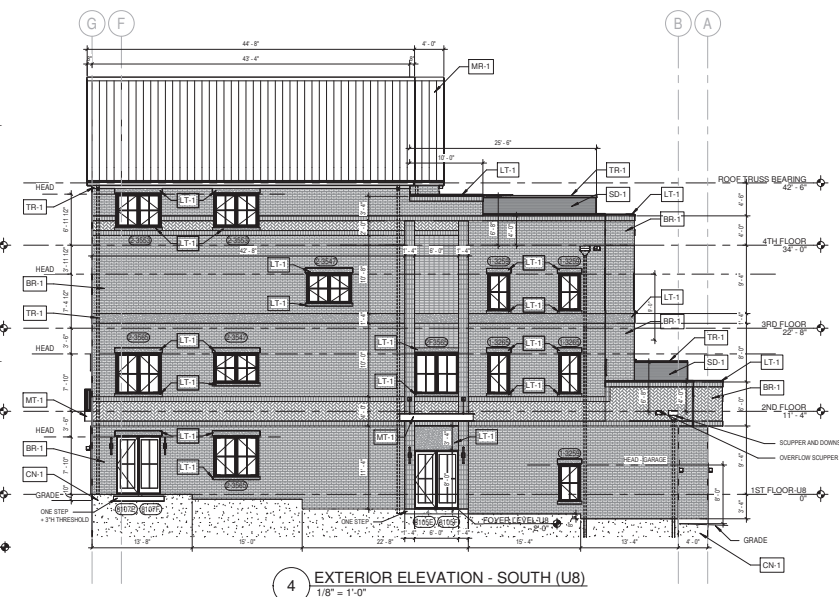
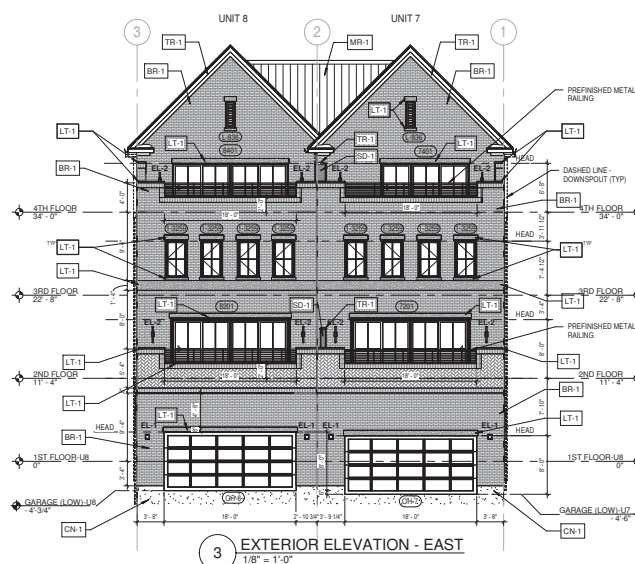
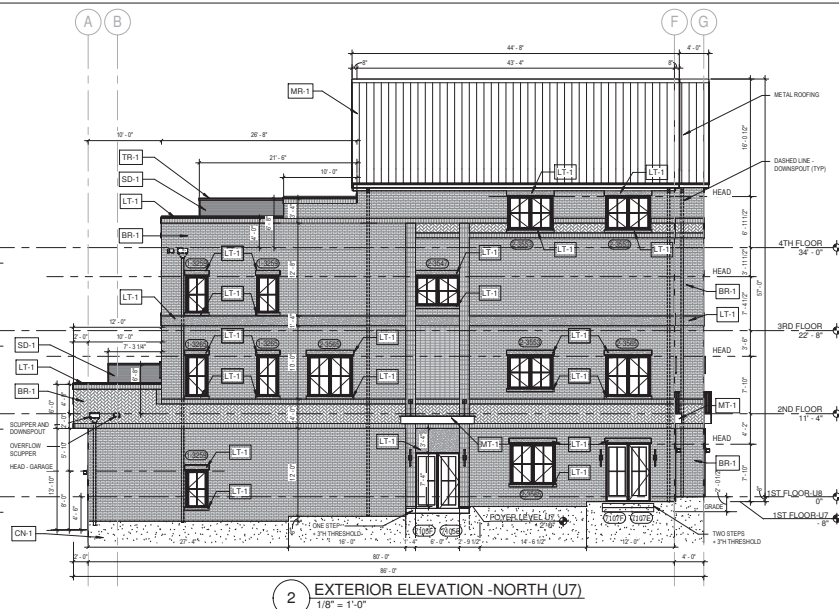
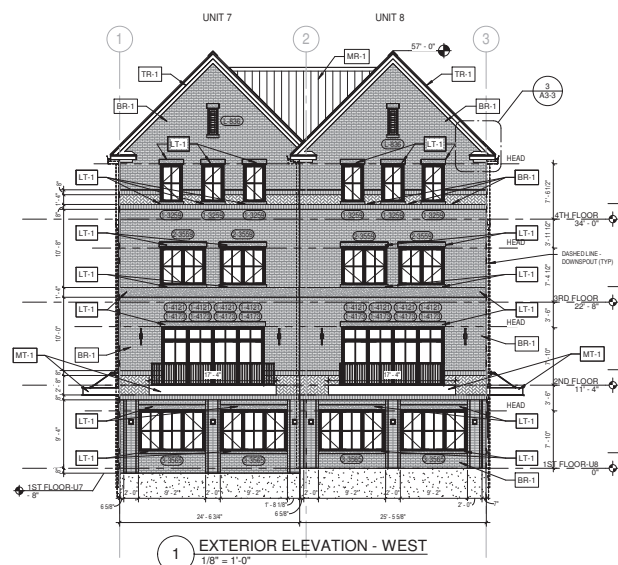
2 LIMESTONE DRIVE ELEVATION  
3/32" = 1'-0"



3 EXTERIOR ELEVATION - SOUTH  
3/32" = 1'-0"

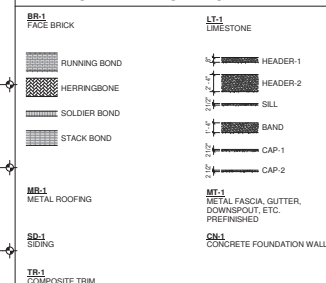


4 EXTERIOR ELEVATION - NORTH  
3/32" = 1'-0"

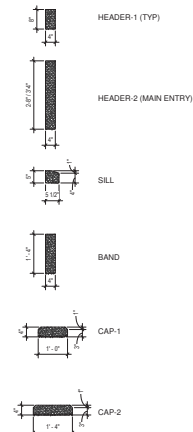


- | GENERAL EXTERIOR ELEVATION NOTES |   |
|----------------------------------|---|
| 1.                               | PROVIDE SEALANT AROUND ALL NEW ELECTRICAL OUTLETS, OVERFLOW NOZZLES, AND FREEZE-PROOF HOSE-BIBBS  |
| 2.                               | ALL WEATHER PROOF GFCI RECEPTACLES AND HOSE BIBBS UNITS TO BE MOUNTED AT 24" TO TOP OF UNIT. COMPLETELY SEAL PENETRATIONS. COORDINATE LOCATIONS WITH MEP DRAWINGS. VERIFY ALL LOCATIONS WITH ARCHITECT PRIOR TO INSTALLATION. |
| 3.                               | SEE STRUCTURAL DRAWINGS FOR FOUNDATIONS.  |
| 4.                               | PROVIDE TEMPERED GLAZING AS REQUIRED.   |

### EXTERIOR ELEVATION LEGEND

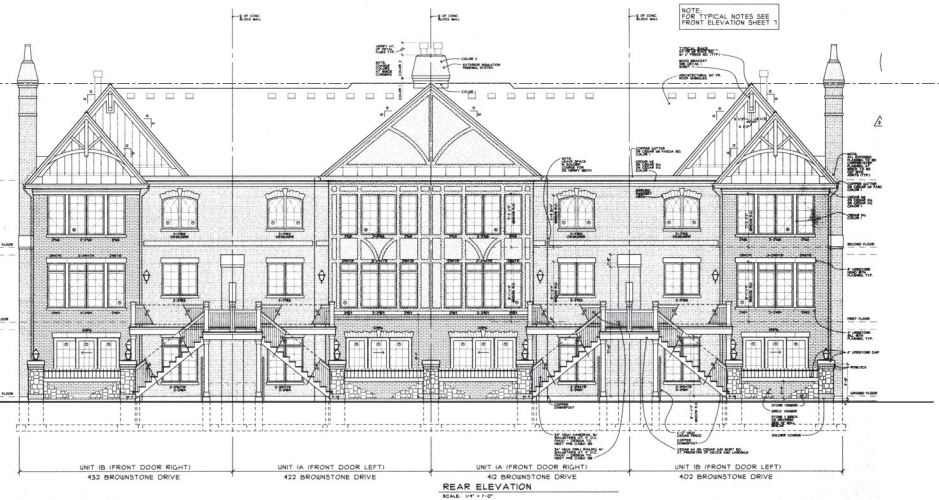
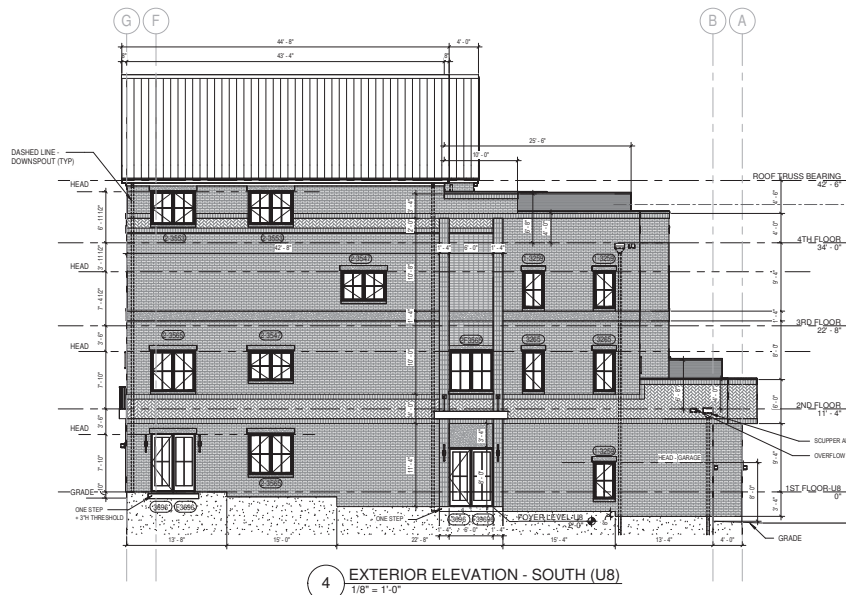
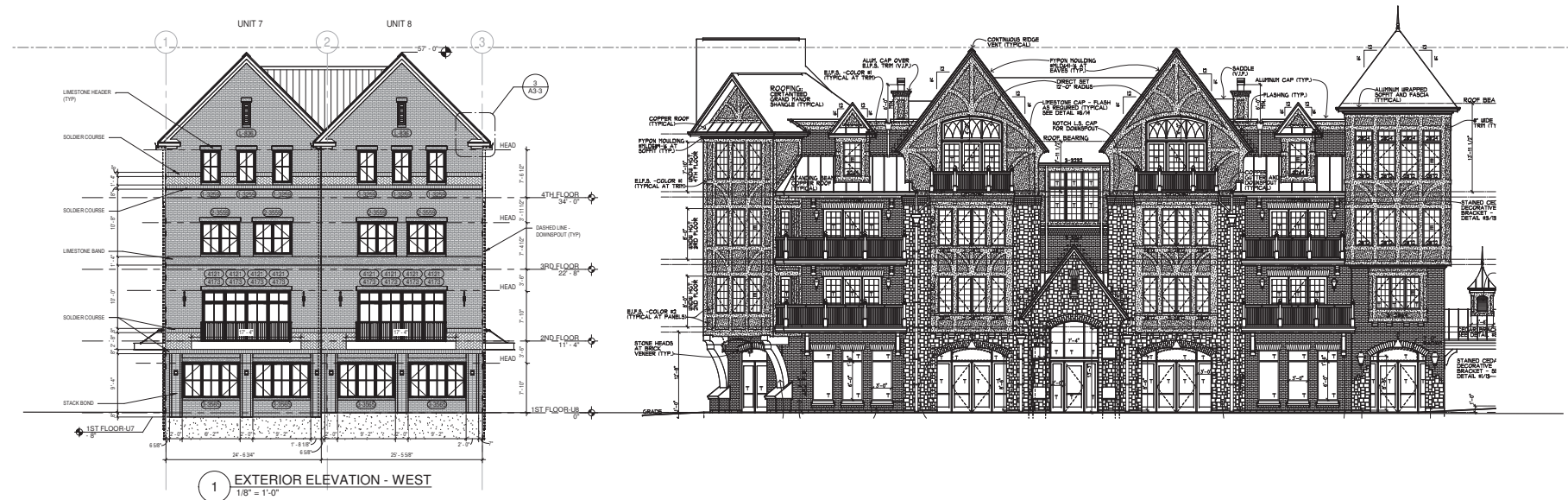


## LIMESTONE PROFILE - NOMINAL





# Building Height Comparison



**ALLEN PEPA**  
ARCHITECTS

ENGINEERS

**RIVER 504 ROWHOMES**  
402 - 542 SOUTH 1ST STREET, ST CHARLES, IL

Schematic Design  
Design Development  
Bidding  
Permit  
ISSUE DATE: 4/17/25  
POST BID PERMIT REVISIONS:  
No. Date Description

PROJECT NO. 24-019  
DRAWN BY: MOY YL  
CHECKED BY: EP

EXTERIOR ELEVATIONS

**A2-3**

85% CD 6/4/25

ALLEN/PEPA COPYRIGHT 5/5/2025 9:53:34 AM



**ALLEN  PEPA**  
ARCHITECTS

INFO@ALLENPEPA.COM  
TEL: (630) 578-1105  
215 FULTON ST., GENEVA, IL 60134

ENGINEER:

RIVER 504 ROWHOMES  
402 - 542 SOUTH 1ST STREET, ST CHARLES, IL

Schematic Design:
Design Development
Bidding:
Permit:
ISSUE DATE:
<b>POST BID/PERMIT REVISIONS:</b>
<b>No.      Date      Description</b>

PROJECT NO.	24-013
DRAWN BY	MO/ YL
CHECKED BY	EP
SHEET	7 OF 28

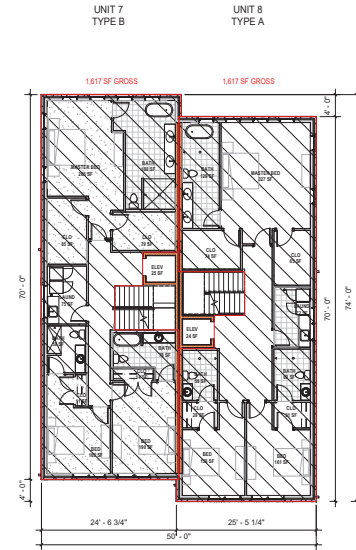
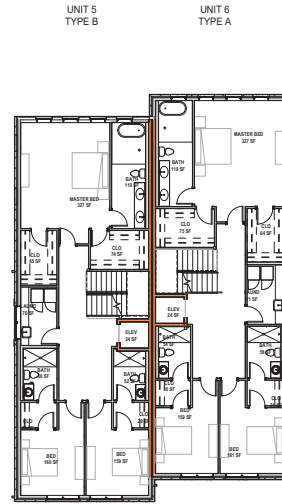
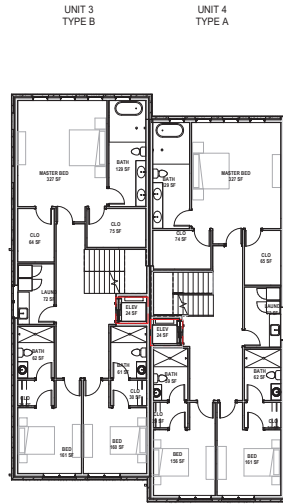
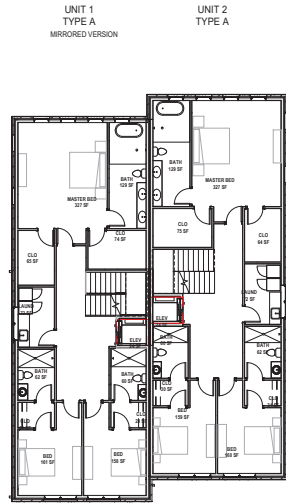
1ST & 2ND  
OVERALL FLOOR  
PLAN

**A0-2**

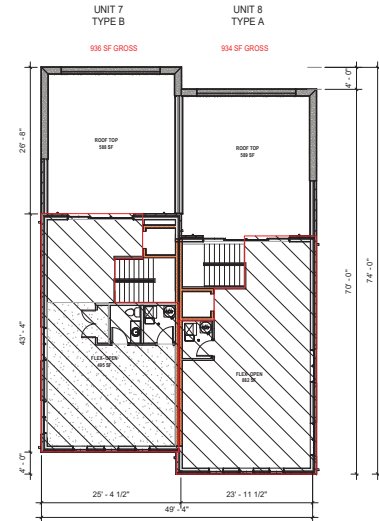
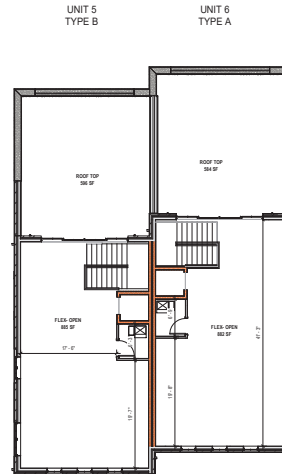
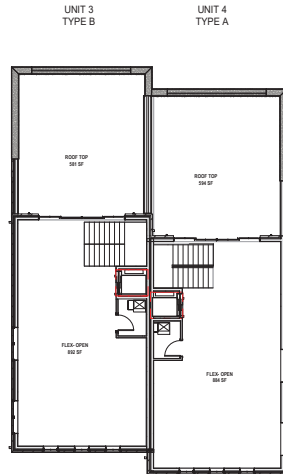
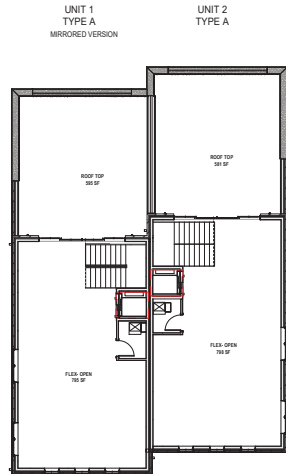
90% CD 7/7/25

ALLEN+PEPA COPYRIGHT 7/24/2025 4:00:00 PM

C:\Users\j2\OneDrive\Documents\24-013 River 504 - Unit 7B - Central - V23 - 3/24/2025\dwg\7F50.vrt



1 THIRD FLOOR PLAN  
3/32" = 1'-0"



2 FOURTH FLOOR PLAN  
3/32" = 1'-0"



# PRELIMINARY PLAT OF RIVER 504 ROWHOMES SUBDIVISION

BEING A RE-SUBDIVISION OF LOT 14, 15 AND PARCEL 9 IN BROWNSTONE IN THE CITY OF ST. CHARLES, KANE COUNTY, ILLINOIS, ACCORDING TO THE PLAT THEREOF RECORDED JANUARY 2, 2001 AS DOCUMENT 2001K000149, ALL BEING A PART OF THE NORTHWEST QUARTER OF SECTION 34, TOWNSHIP 40 NORTH, RANGE 8 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN THE CITY OF ST. CHARLES, KANE COUNTY, ILLINOIS.

## SITE ACREAGE

OVERALL SUBDIVISION:  
29,385 SF 0.675 AC  
RIGHT-OF-WAY TO BE DEDICATED:  
1,380 SF 0.032 AC  
TOTAL LOTS:  
28,005 SF 0.643 AC  
\*SEE LEFT FOR INDIVIDUAL LOT SIZES

## PUBLIC UTILITY & DRAINAGE EASEMENT (P.U.D.E.) NOTES:

A BLANKET P.U.D.E. IS HEREBY GRANTED OVER LOTS 1 THROUGH 8, EXCEPT THOSE PORTIONS WITHIN THE BUILDING FOOTPRINT, AS CONSTRUCTED. THE APPROXIMATE LOCATION OF SAID EXCEPTIONS ARE SHOWN HEREBY, BUT SUBJECT TO CHANGE DUE TO BUILDING RE-DESIGN, AS-BUILT LOCATIONS, AND CONSTRUCTION TOLERANCES.

## PUBLIC UTILITY & DRAINAGE EASEMENT (P.U.D.E.) PROVISION

A PERMANENT NON-EXCLUSIVE EASEMENT IS HEREBY GRANTED TO THE CITY OF ST. CHARLES AND TO ALL PUBLIC UTILITY COMPANIES OF ANY KIND OPERATING UNDER FRANCHISE GRANTING THEIR EASEMENT RIGHTS FROM SAID CITY OF ST. CHARLES, INCLUDING BUT NOT LIMITED TO, AMERITCH AND NICOR AND TO THEIR SUCCESSORS AND ASSIGNS (HEREIN COLLECTIVELY REFERRED TO AS "GRANTEES"), IN, UPON, ACROSS, OVER, UNDER, AND THROUGH THE AREAS SHOWN BY DASHED LINES AND LABELED "PUBLIC UTILITY AND DRAINAGE EASEMENT ON THE PLAT OF SUBDIVISION HEREON DRAWN FOR THE PURPOSE OF INSTALLING, CONSTRUCTING, INSPECTING, OPERATING, REPLACING, RENEWING, ALTERING, ENLARGING, REMOVING, REPAIRING, CLEANING, AND MAINTAINING ABOVE GROUND AND UNDERGROUND ELECTRICAL SYSTEMS, CABLE TELEVISION, COMMUNICATION, GAS, TELEPHONE OR OTHER UTILITY LINES OR APPURTENANCES, SANITARY AND STORM SEWERS, DRAINAGE WAYS, STORM WATER DETENTION AND RETENTION, WATER MAINS AND ANY AND ALL MANHOLES, HYDRANTS, PIPES, CONNECTIONS, CATCH BASINS, BUFFALO BOXES AND WITHOUT LIMITATION, SUCH OTHER INSTALLATIONS AS MAY BE REQUIRED TO FURNISH PUBLIC UTILITY SERVICE TO ADJACENT AREAS TOGETHER WITH THE RIGHT OF ACCESS ACROSS THE REAL ESTATE PLATTED HEREIN FOR THE NECESSARY PERSONNEL AND EQUIPMENT TO MAKE ANY OR ALL OF THE ABOVE WORK. THE PERMANENT NON-EXCLUSIVE EASEMENT IS HEREBY RESERVED FOR AND GRANTED TO THE CITY OF ST. CHARLES AND THE RESPECTIVE SUCCESSORS AND ASSIGNS FOR MAINTAINING THE UNINTERRUPTED AND UNIMPEDED CONVEYANCE, FLOW AND RUNOFF OF SURFACE STORM WATER ACROSS AND UPON THE AREAS DESIGNATED ON THIS PLAT AS DRAINAGE EASEMENT. THE RIGHT IS HEREBY GRANTED TO SAID GRANTEES TO CUT DOWN, TRIM, OR REMOVE ANY TREES, SHRUBS, OR OTHER PLANTS THAT INTERFERE WITH THE DRAINAGE WAYS AND OPERATION OF OR ACCESS TO SAID UTILITY INSTALLATIONS, WITHOUT LIMITATION, IN, ON, UPON OR ACROSS, UNDER, OR THROUGH SAID EASEMENTS.

NO PERMANENT BUILDINGS, TREES, GARDENS, SHRUBS, OR BERMING SHALL BE PLACED ON OR IN SAID EASEMENTS, EXCEPT AS OTHERWISE APPROVED BY THE CITY. BUT THE EASEMENT AREAS MAY BE USED FOR PAVING, FENCES, SIDEWALKS, AND OTHER PURPOSES THAT DO NOT INTERFERE WITH THE AFORESAID USES AND RIGHTS. WHERE AN EASEMENT IS USED FOR STORM OR SANITARY SEWERS, OTHER UTILITY INSTALLATIONS SHALL BE SUBJECT TO THE PRIOR APPROVAL OF SAID CITY OF ST. CHARLES SO AS NOT TO INTERFERE WITH THE GRAVITY FLOW IN SAID SEWER OR SEWERS. UTILITY INSTALLATIONS, OTHER THAN THOSE MANAGED BY THE CITY OF ST. CHARLES, SHALL BE SUBJECT TO THE APPROVAL OF THE CITY OF ST. CHARLES, AS TO DESIGN AND LOCATION, AND ALL OTHER INSTALLATIONS ARE SUBJECT TO THE ORDINANCES OF THE CITY OF ST. CHARLES.

FOLLOWING ANY WORK TO BE PERFORMED BY THE GRANTEES IN THE EXERCISE OF ITS EASEMENT RIGHTS GRANTED HEREIN, THE GRANTEES SHALL HAVE NO OBLIGATION WITH RESPECT TO SURFACE RESTORATION INCLUDING, BUT NOT LIMITED TO, THE RESTORATION, REPAIR, OR REPLACEMENT OF ANY LANDSCAPING PROVIDED, HOWEVER, THE GRANTEES SHALL BE OBLIGATED FOLLOWING ANY SUCH WORK, TO BACKFILL AND MOUND SO AS TO RETAIN SUITABLE DRAINAGE, REMOVE DEBRIS, AND LEAVE THE AREA IN GENERALLY CLEAN AND WORKMANLIKE CONDITION.

## SURVEYOR'S NOTES

1. SURVEY IS BASED ON FIELD WORK COMPLETED ON FEBRUARY 28, 2025.
2. ONLY THOSE EXISTING EASEMENTS PROVIDED BY CLIENT WERE REVIEWED FOR THIS SURVEY. REFER TO THE DEED, TITLE INSURANCE POLICY AND LOCAL ORDINANCES FOR OTHER RESTRICTIONS. ADDITIONAL EASEMENTS MAY EXIST THAT ARE NOT DEPICTED HEREON.
3. BASIS OF BEARING: GRID NORTH - ILLINOIS EAST ZONE, STATE PLANE COORDINATE SYSTEM, NAD 83.

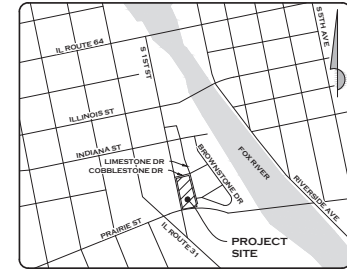
1. SUBDIVIDED PROPERTY LEGAL DESCRIPTION UTILIZED FOR THIS SURVEY WAS FROM WARRANTY DEED RECORDED 411/0022 AS DOCUMENT NUMBER 2022K019650.
2. IRON PIPES SET SHALL BE 3/4" DIAMETER, BY 24" LENGTH, WHICH EXCEEDS VILLAGE REQUIREMENTS AND MEETS THE STATE OF ILLINOIS REQUIREMENTS.

OWNER/SUBDIVIDER

J&B BUILDERS, INC.  
525 S. Tyler Rd, Unit P  
St. Charles, IL 60174  
PH: (630) 587-9900

SURVEYOR

MeritCorp Group, LLC  
4222 Meridian Parkway, Suite 112  
Aurora, Illinois 60004  
Ph: (330) 554-6655



VICINITY MAP

NOT TO SCALE

## LEGEND

PROPERTY BOUNDARY  
OFFSITE PROPERTY LINE  
PROPOSED LOT LINE  
EASEMENT LINE  
RECORD DIMENSION  
CONCRETE MONUMENT  
FOUND IRON PIPE  
FOUND IRON ROD  
SET 3/4" IRON PIPE  
SET CUT CROSS  
EXCEPTION TO P.U.D.E.

## ABBREVIATIONS

P.U.D.E. = PUBLIC UTILITY & DRAINAGE EASEMENT  
FND = FOUND  
SF = SQUARE FOOTAGE  
AC = ACREAGE

## SURVEYOR'S CERTIFICATE

STATE OF ILLINOIS )  
COUNTY OF DUPAGE ) SS

THIS IS TO CERTIFY THAT I, JONATHAN SPINAZZOLA, ILLINOIS LAND SURVEYOR NO. 3868, HAVE SURVEYED AND PROPOSES TO SUBDIVIDE THE FOLLOWING DESCRIBED PROPERTY:

LOT 14, 15 AND PARCEL 9 IN BROWNSTONE IN THE CITY OF ST. CHARLES, KANE COUNTY, ILLINOIS, ACCORDING TO THE PLAT THEREOF RECORDED JANUARY 2, 2001 AS DOCUMENT 2001K000149, ALL BEING A PART OF THE NORTHWEST QUARTER OF SECTION 34, TOWNSHIP 40 NORTH, RANGE 8 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN THE CITY OF ST. CHARLES, KANE COUNTY, ILLINOIS.

AS SHOWN BY THE SUBDIVISION PLAT WHICH IS A CORRECT REPRESENTATION OF SAID SURVEY AND SUBDIVISION, ALL DISTANCES ARE SHOWN IN FEET & DECIMALS THEREOF. THIS SUBDIVISION IS WITHIN THE CITY OF ST. CHARLES, ILLINOIS WHICH ADOPTED AN OFFICIAL COMPREHENSIVE PLAN AND IS EXERCISING THE SPECIAL POWERS AUTHORIZED BY THE STATE OF ILLINOIS ACCORDING TO 65 ILCS 5/11-12.6 AS HERETOFORE AND HEREAFTER AMENDED, AND THIS SITE FALLS WITHIN "OTHER AREAS ZONE 'X' (AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN) AS DEFINED BY THE FLOOD INSURANCE RATE MAP COMMUNITY PANEL 170330002K, MAP NUMBER 17080C0202H, EFFECTIVE DATE OF AUGUST 3, 2009.

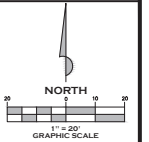
GIVEN UNDER MY HAND AND SEAL ON THIS 11TH DAY OF JULY, 2025.

JONATHAN D. SPINAZZOLA  
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 3868  
CURRENT LICENSE EXPIRES NOVEMBER 30, 2026.

THIS PROFESSIONAL SERVICE CONFORMS TO THE MINIMUM STANDARDS FOR A BOUNDARY SURVEY IN THE STATE OF ILLINOIS.



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MeritCorp



DESCRIPTION:  
ISSUED TO CITY OF ST. CHARLES  
REVISED PER CITY OF ST. CHARLES  
DATE:  
03/10/25  
07/21/25  
07/21/25

MeritCorp  
Professional Land Surveyors International  
4222 Meridian Parkway, Suite 112  
Aurora, IL 60006  
Office: (330) 554-6655  
Lic. No. 04-005860  
www.meritcorp.com  
Other Office Locations:  
Gurnee, IL



RIVER 504 ROWHOMES  
402-542 SOUTH 1ST STREET  
ST. CHARLES, ILLINOIS  
PRELIMINARY PLAT  
OF RIVER 504 ROWHOMES  
SUBDIVISION

PROJECT NO. M24183


DRAWN BY: JDS

CHECKED BY: JDS

SHEET NO. 1/1



Staff Report  
Plan Commission Meeting – August 19, 2025

<b>Applicant:</b>	Chris McGrath	<div>McGrath Business Center Phase 2 – Kia Parking Expansion</div> <div></div> <div>Subject Property</div>
<b>Property Owner:</b>	4085 E Main LLC	
<b>Location:</b>	East end of former Pheasant Run property	
<b>Purpose:</b>	Construct additional parking for McGrath Kia dealership	
<b>Applications:</b>	<ul style="list-style-type: none"><li>• PUD Preliminary Plan</li><li>• Final Plat of Subdivision</li></ul>	
<b>Public Hearing:</b>	Not required	
<b>Zoning:</b>	BR Regional Business / PUD	
<b>Current Land Use:</b>	Vacant	
<b>Comprehensive Plan:</b>	Corridor/Regional Commercial & Open Space	
<b>Summary of Proposal:</b>	<p>The McGrath Business Center PUD includes the McGrath Honda and McGrath Kia properties and the subject property, which is the last undeveloped portion of the PUD.</p> <p>A PUD Preliminary Plan has been submitted for the subject property, located east of McGrath Kia. Details:</p> <ul style="list-style-type: none"><li>• Plat the property as a single lot with required easements established.</li><li>• Construct a parking lot to be used by McGrath Kia.</li><li>• Access via existing right-in/right-out driveway off Rt 64, shared with McGrath Kia.</li><li>• Stormwater detention at the south end of the property.</li></ul>	
<b>Info / Procedure on Application:</b>	<p><b>PUD Preliminary Plan:</b></p> <ul style="list-style-type: none"><li>• Approval of plans for development of property within a PUD to ensure compliance with the PUD ordinance and applicable provisions of the Zoning Ordinance. Includes building elevations and site, landscape, and engineering plans.</li><li>• A public hearing is not required for this type of application.</li><li>• No findings of fact are applicable to this application.</li><li>• Recommendation is based on compliance with the concurrently approved Special Use for PUD standards and other city code requirements (including Zoning and Subdivision codes).</li></ul> <p><b>Final Plat of Subdivision:</b></p>	

- Final Plat is the actual plat document that will be recorded with the County to formally create new lots, dedicate streets, and provide easements, etc.
- Recommendation is based on compliance with all other code requirements (including Zoning & Subdivision Codes).

<b>Suggested Action:</b>	Review the PUD Preliminary Plan and Final Plat of Subdivision. A recommendation for approval should be subject to resolution of all staff comments prior to City Council action.
<b>Staff Contact:</b>	Ellen Johnson, Planner

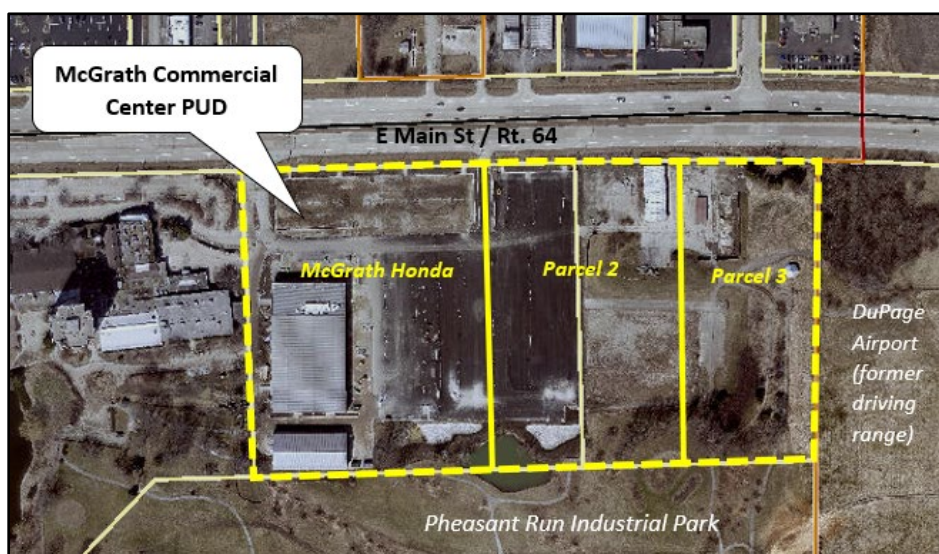
## I. PROPERTY INFORMATION

### A. History / Context

The subject property is a 4.83-acre parcel located in the McGrath Business Center PUD, a redevelopment of a portion of the former Pheasant Run Resort complex that included the Mega Center building, parking area east of the Mega Center, and the resort/golf course maintenance facility at the far east end of the property.

The McGrath Business Center PUD was approved by the City in 2021 under Ord. 2021-Z-3. The ordinance approved a number of zoning deviations for development of the property. Also approved were preliminary plans for the McGrath Honda dealership at the west end of the PUD, and a PUD Sketch Plan which provided a conceptual layout of possible future development for the two parcels east of the Honda dealership. Car dealerships were contemplated; however, the PUD allows any use permitted in the underlying BR Regional Business zoning district.

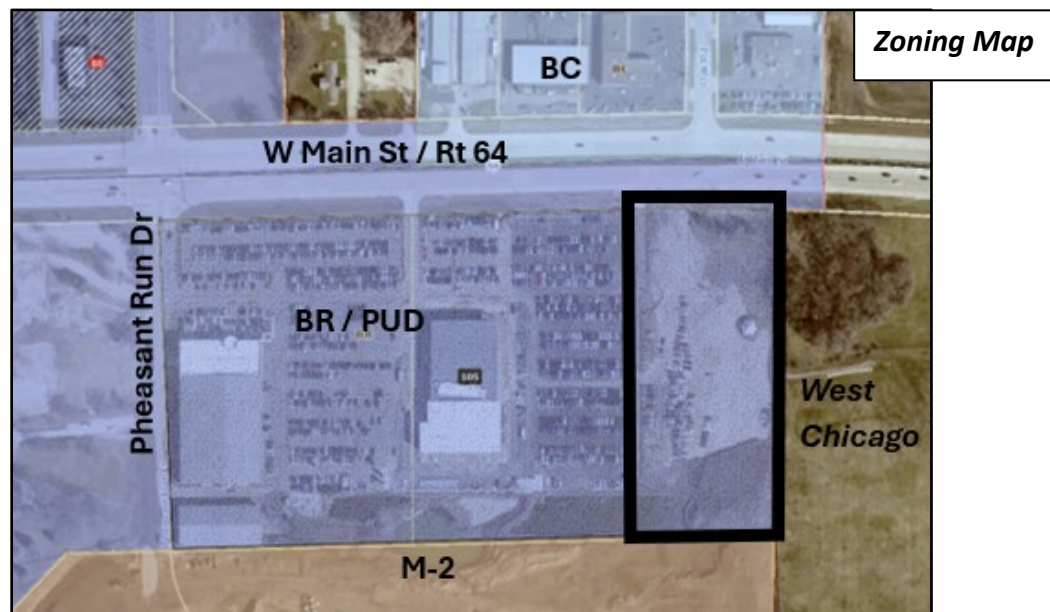
In 2023, the City approved a PUD Preliminary Plan for McGrath Kia, located on the property east of McGrath Honda. The remaining parcel is undeveloped and has been used as temporary parking for the Kia dealership.



## B. Zoning

The subject property is zoned BR Regional Business and is located in the McGrath Business Center PUD. Properties to the north and west fronting Rt 64 have commercial zoning, with manufacturing to the south and DuPage Airport to the east.

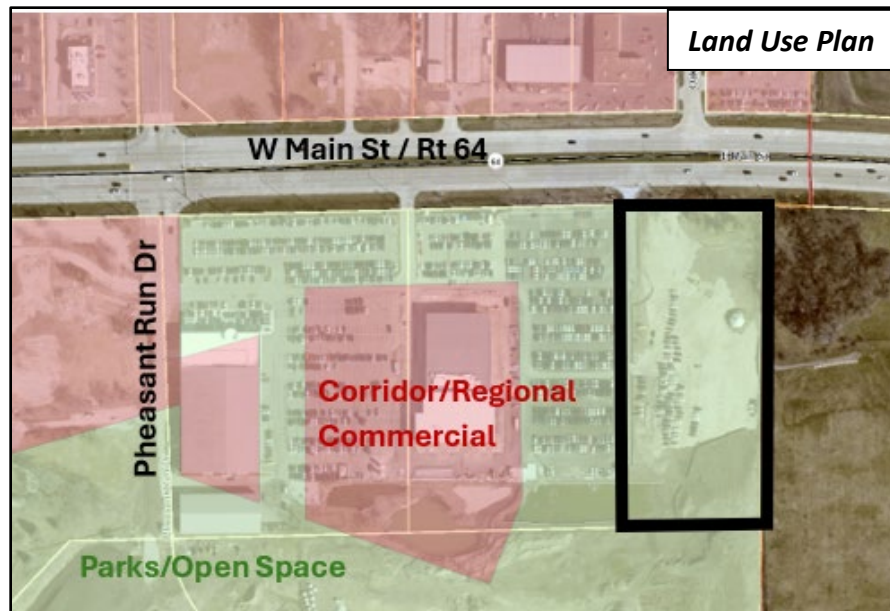
	<b>Zoning</b>	<b>Land Use</b>
<b>Subject Property</b>	BR Regional Business / PUD	Vacant
<b>North</b>	BR Regional Business BC Community Business R3 Single Family (DuPage County Zoning)	Baltria Auto Dealer; Public Storage
<b>East</b>	A Airport (City of West Chicago Zoning)	DuPage Airport
<b>South</b>	M-2 Limited Manufacturing	Pheasant Run Industrial Park
<b>West</b>	BR Regional Business / PUD	McGrath Kia / Honda



## C. Comprehensive Plan

The Land Use Plan adopted as part of the 2013 Comprehensive Plan identified the developed portions of the former Pheasant Run resort as “Corridor/ Regional Commercial” and the Golf Course as “Open Space”, including the subject property.





The subject property is located at the edge of the East Gateway Sub Area, which lists the following goals and objectives:

#### **Subarea Goals**

*The East Gateway subarea represents a unique opportunity for economic development, revitalization and stabilization with for a specific context within the City of St. Charles. The overall vision for the subarea includes the following:*

- *Revitalization of the Subarea's retail areas that maximizes the locational assets within this area of the City.*
- *Improved connectivity and circulation within the Subarea providing logical and efficient connections between compatible uses.*
- *Better separation of incompatible land uses to protect residential neighborhoods while at the same time help define the City's business areas.*
- *Attractive streets and sites to distinguish this Subarea and key corridors from neighboring communities.*
- *A mix of uses that that help diversify the City's economy and provide places to live, work, and shop.*

#### **Subarea Objectives**

- *Improve the appearance of the Kirk Road and Main Street Corridors to assist in strengthening the community's identity and appearance through installation of streetscaping, wayfinding and gateway elements.*
- *Use landscaping appropriately to enhance commercial areas, screen unsightly areas, and provide an attractive streetscape and overall setting for the area.*
- *Improve the overall connectivity and mobility within the Subarea through both public streets and internal connection to provide a predictable and navigable environment.*
- *Preserve surrounding neighborhoods through the use of screening, buffering, and better separation from commercial development.*
- *Create market-responsive development parcels that can accommodate projects of an appropriate scale and phasing over time.*
- *Take advantage of proximity to DuPage Airport and Pheasant Run as activity generators.*
- *Reposition the Charlestowne Mall site to foster its renaissance or its redevelopment.*

- *Enhance the character of both existing and new development through site improvements, facade enhancements, consistent signage regulation, and attractive building design and materials.*

## II. PROPOSAL

Chris McGrath, on behalf of property owner 4085 E. Main, LLC is proposing to develop the final phase of the McGrath Commercial Center PUD with an additional parking lot for the existing Kia Dealership adjacent to the west. Details of the proposal are as follows:

- Plat the property as a single lot and establish required easements.
- Construct a 474-stall parking lot to be used by McGrath Kia for vehicle inventory.
- Access via existing right-in/right-out driveway off Rt 64, shared with the Kia dealership.
- Stormwater detention at the south end of the property.

The following development applications have been submitted for approval:

1. PUD Preliminary Plan – Plan approval required for development of property that is within a Planned Unit Development to ensure compliance with zoning and PUD standards. Includes site plan, preliminary engineering, and landscape plan.
2. Final Plat of Subdivision – To subdivide the property to create new lots and establish required easements.

## III. PLANNING ANALYSIS

Staff has analyzed the submitted applications and plans for conformance with the standards established in the McGrath Commercial Center PUD (Ord. 2021-Z-3) and applicable sections of the Zoning and Subdivision ordinances, including:

- Title 16 Subdivisions & Land Improvements
- Ch. 17.14 Commercial Districts
- Ch. 17.24 Off-Street Parking
- Ch. 17.26 Landscaping & Screening

### A. Proposed Use

Proposed use of the subject property as a parking lot is permitted as an expansion of the adjacent dealership use, classified under the Zoning Ordinance as “Motor Vehicle Sales and Leasing”. The PUD contemplated an additional car dealership on this property, but any use permitted in the underlying BR District is acceptable.

### B. Bulk Standards

The table below compares the proposed plans with the bulk standards applicable to the BR District and McGrath Business Center PUD. All applicable standards are met.

Category	BR District / PUD Standard	Proposed
<b>Min. Lot Area</b>	1 acre	2.83 acres / 210,584 sf
<b>Min. Lot Width</b>	None	307 ft.
<b>Front Yard</b>	15 ft. parking/paving (PUD)	17.5 ft. parking/paving
<b>Interior Side Yard</b>	West: 0 ft. parking/paving (PUD) East: 15 ft. parking/paving	West: 0 ft. parking/paving East: 15 parking/paving
<b>Rear Yard</b>	0 ft. parking/paving (PUD)	100 ft. parking/paving

**C. Site Access**

Access to the property is provided from an existing right-in/right-out driveway off Rt 64/E Main St. The driveway is shared with the McGrath Kia property. Cross-access easements are in place allowing for internal cross access from the McGrath Honda entrance at Pheasant Run, which is a full signalized intersection, through McGrath Kia, to the subject property.

The property is at the east end of the City’s pedestrian circulation system. No new public sidewalks are shown; none were provided for McGrath Honda or Kia. Property to the east is part of the DuPage Airport where no sidewalks exist.

**D. Landscaping**

The McGrath Business Center PUD Ordinance granted a number of deviations from the landscaping requirements of Ch. 17.26 “Landscaping and Screening”. The PUD allows for the design of landscaping along the public street frontage, within the parking lot, and along the building foundation for all development within the PUD to be a “design of a similar character and number of plantings” as the plans for McGrath Honda, which were approved as part of the PUD Ordinance.

The landscape plan submitted for the subject property is similar to that of McGrath Honda and Kia. Plantings and trees are provided within the setback along Rt. 64, with a planting strip along the access drive into the site. Trees are provided along the east property line. The overall landscape area for the property is 22.8%, including the detention basin, which is over the 15% overall open space area required for the PUD as a whole.

**E. Site Lighting**

A photometric plan has been submitted depicting pole lighting. Per Section 17.22.040 “Sight Lighting”, light intensity along the property line abutting right-of-way (Rt. 64) is limited to an average of 15 horizontal foot-candles for motor vehicle display lots. This standard is met along the Rt. 64 lot line. Light intensity along property lines abutting other commercial zoning districts is typically limited to 2.0 horizontal foot-candles. This is met along the east lot line. Because the subject property is adjacent to a motor vehicle dealership on the east side, an average of 15 foot-candles is acceptable along the east lot line. This standard is met.

**F. Plat of Subdivision**

A Final Plat of Subdivision has been submitted, titled “McGrath Commerce Center Subdivision – Phase 2”. A single lot is proposed, covering the entire subject property and totaling 210,584 sf / 4.83 acres.

The Plat notes existing public utility, stormwater, and cross-access easements previously established. New easements for the expanded detention basin and sanitary sewer through the property are granted.

**IV. DEPARTMENTAL REVIEWS****A. Engineering Review**

Engineering plans and stormwater report have been approved by staff. Stormwater detention is proposed at the south end of the property as an expansion of the existing detention basin on the McGrath Kia property.

**B. Fire Dept. Review**

The plans meet Fire access requirements.

**C. Public Works**

Public Works has reviewed the plans and all comments have been addressed.

**V. OPTIONS FOR ACTION**

Review the PUD Preliminary Plan and Final Plat of Subdivision.

Staff has found the application materials to be complete. Staff recommends the any recommendation include a condition requiring resolution of all staff comments prior to City Council action.

**VI. ATTACHMENTS**

- Applications for PUD Preliminary Plan & Final Plat of Subdivision; received 12/2/24
- Final Plat of Subdivision
- Preliminary Plans

City of St. Charles  
Community Development Division  
2 E. Main Street  
St. Charles, IL 60174



Phone: (630) 377-4443  
Email: [cd@stcharlesil.gov](mailto:cd@stcharlesil.gov)

## PUD PRELIMINARY PLAN APPLICATION

### For City Use

Project Name: McGrath Bus. Center Phase 2 - Kia Parking Expansion  
Project Number: 2023 -PR- 003  
Cityview Project Number: PLPUD202400042

Received Date

**RECEIVED**

**DEC 10 2024**

City of St. Charles  
Community Development

- File this application to request approval of a PUD Preliminary Plan. Normally, this application is reviewed concurrently with an application for Special Use for PUD, unless a Special Use for PUD has previously been granted and no amendment is necessary.
- Complete the application and submit with all required attachments to the Community Development Division.
- The information you provide must be complete and accurate. If you have any questions please contact the Community Development Division.
- City staff will review the submittal for completeness and for compliance with applicable requirements. Staff will distribute the plans to other City departments for review when the application is complete.
- The PUD Preliminary Plan will be scheduled for Plan Commission review when staff has determined the plans are ready.

<b>1. Property Information:</b>	Location:	4085 E. Main Street	
	Parcel Number (s):	01-30-300-064	
	Proposed PUD Name:	McGrath Business Center Subdivision Phase 2	
<b>2. Applicant Information:</b>	Name:	Chris McGrath	Phone: 847-980-6184
	Address	2020 Randall Road Elgin, IL 60123	Email: mcgrathchris@yahoo.com
<b>3. Record Owner Information:</b>	Name:	4085 E Main LLC, c/o Chris McGrath	Phone: 847-980-6184
	Address:	2020 Randall Road Elgin, IL 60123	Email: mcgrathchris@yahoo.com

**4. Identify the Type of Application:**

- ☐ New proposed Planned Unit Development (PUD) – *Special Use Application to be filed concurrently*
- ☒ Existing Planned Unit Development (PUD)
- ☐ PUD Amendment required for proposed plan – *Special Use Application to be filed concurrently*

***Subdivision:***

- ☐ Proposed lot has already been platted and a new subdivision is not required.
- ☒ New subdivision of property is required:
- ☒ Final Plat of Subdivision Application filed concurrently
- ☐ Final Plat of Subdivision Application to be filed later

**5. Required Attachments:**

*If multiple zoning or subdivision applications will be submitted concurrently, do not submit duplicate checklist items or plans. Fee must be paid for each application.*

***Submit 1 copy of each required item, unless otherwise noted.***

- ☒ **APPLICATION FEE:** \$500
- ☒ **REIMBURSEMENT OF FEES AGREEMENT:** An original, executed Reimbursement of Fees Agreement and deposit of funds in escrow with the City, as provided by Appendix B of the Zoning Ordinance.
- ☒ **REIMBURSEMENT OF FEES INITIAL DEPOSIT:** Deposit of funds in escrow with the City. Required deposit is based on review items (number of applications filed) and the size of the subject property:

Number of Review Items	Under 5 Acres	5-15 Acres	16-75 Acres	Over 75 Acres
1	\$1,000	\$2,000	\$3,000	\$4,000
2 or 3	\$2,000	\$4,000	\$5,000	\$7,000
4 or more	\$3,000	\$5,000	\$7,000	\$10,000

- ☐ **PROOF OF OWNERSHIP:** a) A current title policy report; or  
b) A deed and a current title search

***NOTE: Private covenants and deed restrictions can limit private property rights with respect to the use of land even though the City's Zoning Ordinance may authorize the use or a less restrictive use. We strongly advise that you perform a title search on the property to determine if there any private covenants containing use restrictions or other deed restrictions. As those private covenants and deed restrictions may conflict with the City's Zoning Ordinance, it is further recommended that you consult with an attorney to obtain an opinion with respect to whether your intended use is compatible with those restrictions.***

- ☐ **OWNERSHIP DISCLOSURE:** Use the appropriate disclosure form (attached), if the owner or applicant is a Partnership, Corporation, Trust, or LLC.
- ☐ **LETTER OF AUTHORIZATION:** If the property owner is not the applicant, an original letter of authorization from the property owner permitting the applicant to file the zoning application with the City of St. Charles for the subject property.
- ☒ **LEGAL DESCRIPTION:** For entire subject property, on 8 1/2 x 11 inch paper and Microsoft Word file.
- ☒ **PLAT OF SURVEY:** A current plat of survey for the subject property showing all existing improvements on the property, prepared by a registered Illinois Professional Land Surveyor.

- ☒ **SOIL AND WATER CONSERVATION DISTRICT LAND USE OPINION APPLICATION:** As required by State law, submit a Land Use Opinion application and required fee directly to the Kane-DuPage Soil and Water Conservation District. Provide a copy of completed Land Use Opinion application to the City. The Land Use Opinion application can be found on the Kane-DuPage SWCD website: <http://www.kanedupageswcd.org/>
- ☒ **ENDANGERED SPECIES REPORT:** As required by State law, file an Endangered Species Consultation Agency Action with the Illinois Department of Natural Resources. Provide a copy of the report to the City. The online Ecological Compliance Assessment Tool (EcoCAT) should be utilized: <https://dnr2.illinois.gov/EcoPublic/>
- ☒ **STORMWATER MANAGEMENT:** Written information as described in the Stormwater Management Requirements for Preliminary Plans (attached).
- ☐ **PUBLIC BENEFITS, DEPARTURES FROM CODE:** A written statement describing how the PUD meets the purposes and requirements established in Section 17.04.400 of the Zoning Ordinance. List any requests for deviations (departures) from the requirements of Title 16 "Subdivisions and Land Improvement" and Title 17 "Zoning" and provide a rationale for each requested deviation.
- ☒ **CONSTRUCTION SCHEDULE:** Indicate the following:
- Phases in which the project will be built with emphasis on area, density, use and public facilities, such as open space, to be developed with each phase. Overall design of each phase shall be shown on the Plat of Subdivision and through supporting material.
  - Approximate dates for beginning and completion of each phase.
- ☐ **PARK AND SCHOOL LAND/CASH WORKSHEETS:** *For residential developments only.* Use the attached worksheet to calculate the estimated population and student yields and resulting land/cash contributions in accordance with Title 16 of the St. Charles Municipal Code.
- ☐ **INCLUSIONARY HOUSING WORKSHEET:** *For residential developments only.* Use the attached worksheet to calculate the affordable unit requirement and indicate how the development will comply with Title 19 of the St. Charles Municipal Code.

**PLANS:** All required plans shall be drawn on sheets no larger than 24" x 36", unless the Director of Community Development permits a larger size when necessary to show a more comprehensive view of the project. All required plans shall show north arrow and scale, and shall be drawn at the same scale (except that a different scale may be used to show details or specific features). All plans shall include the name of the project, developer or owner of site, person or firm preparing the plan, and the date of plan preparation and all revisions.

**Copies:** Ten (10) full size copies, one (1) 11" by 17", and PDF electronic file emailed to: [cd@stcharlesil.gov](mailto:cd@stcharlesil.gov)

- ☒ **PRELIMINARY PLAT OF SUBDIVISION / DRAWING REQUIREMENTS CHECKLIST:** If the PUD Preliminary Plan involves the subdivision of land, submit a Preliminary Plat of Subdivision that includes the information listed on the Subdivision Plat Drawing Requirements Checklist. Also submit a completed Subdivision Plat Drawing Requirements Checklist (attached).
- ☒ **PRELIMINARY ENGINEERING PLANS / DRAWING REQUIREMENTS CHECKLIST:** Plans shall include the following information. Also submit a completed Drawing Requirements Checklist- Preliminary Engineering Plans.
1. Accurate boundary lines with dimensions
  2. Existing and proposed easements: location, width, purpose
  3. Streets on and adjacent to the tract: Name and right-of-way width, center line elevation, and culverts
  4. Location, size, shape, height, and use of existing and proposed structures



5. Location and description of streets, sidewalks, and fences
6. Surrounding land uses
7. Legal and common description
8. Date, north point, and scale
9. Existing and proposed topography
10. All parcels of land intended to be dedicated for public use or reserved for the use of all property owners with the proposal indicated
11. Location of utilities
12. Building/use setback lines
13. Location of any significant natural features
14. Location of any 100-year recurrence interval floodplain and floodway boundaries
15. Location and classification of wetland areas as delineated in the National Wetlands Inventory
16. Existing zoning classification of property
17. Existing and proposed land use
18. Area of property in square feet and acres
19. Proposed off-street parking and loading areas
20. Number of parking spaces provided, and number required by ordinance
21. Angle of parking spaces
22. Parking space dimensions and aisle widths
23. Driveway radii at the street curb line
24. Width of driveways at sidewalk and street curb line
25. Provision of handicapped parking spaces
26. Dimensions of handicapped parking spaces
27. Depressed ramps available to handicapped parking spaces
28. Location, dimensions and elevations of freestanding signs
29. Location and elevations of trash enclosures
30. Provision for required screening, if applicable
31. Provision for required public sidewalks
32. Certification of site plan by a registered land surveyor or professional engineer
33. Geometric plan showing all necessary geometric data required for accurate layout of the site
34. Grading plans showing paving design, all storm sewers, and detention/retention facilities including detention/retention calculations) and erosion control measures
35. Utility plans showing all storm sewers, sanitary sewers, watermain, and appropriate appurtenant structures
36. Exterior lighting plans showing:
  - Location, height, intensity and fixture type of all proposed exterior lighting
  - Photometric information pertaining to locations of proposed lighting fixtures
37. Typical construction details and specifications
38. Certification of site engineering plans by a registered professional engineer
39. Proof of application for Stormwater Management Permit

☐ **ARCHITECTURAL PLANS:** Architectural plans and data for all principal buildings with sufficient detail to permit an understanding of the exterior appearance and architectural style of the proposed buildings, the number, size and type of dwelling units, the proposed uses of nonresidential and mixed-use buildings, total floor area and total building coverage of each building.

☐ **TREE PRESERVATION PLAN:** When required, in accordance with Ch. 8.30 of the St. Charles Municipal Code (see attached "Tree Preservation Requirements for Preliminary Plans". This information may be included as part of the Landscape Plan set.

☒ **LANDSCAPE PLAN:** Landscape Plan showing the following information:

- Delineation of the buildings, structures, and paved surfaces situated on the site and/or contemplated to be built thereon
- Delineation of all areas to be graded and limits of land disturbance, including proposed contours as shown on the Site/Engineering Plan.
- Accurate property boundary lines
- Accurate location of proposed structures and other improvements, including paved areas, berms, lights, retention and detention areas, and landscaping
- Site area proposed to be landscaped in square feet and as a percentage of the total site area
- Percent of landscaped area provided as per code requirement
- Dimensions of landscape islands
- Setbacks of proposed impervious surfaces from property lines, street rights-of-way, and private drives
- Location and identification of all planting beds and plant materials
- Planting list including species of all plants, installation size (caliper, height, or spread as appropriate) and quantity of plants by species
- Landscaping of ground signs and screening of dumpsters and other equipment

☐ **SKETCH PLAN FOR LATER PHASES OF PUD:** For phased PUDs only, submit a plan that shows, at minimum, the following:

- General location of arterial and collector streets
- Location of any required landscape buffers
- Location of proposed access to the site from public streets
- Maximum number of square feet of floor area for nonresidential development
- Maximum number of dwelling units for residential development
- Open space and storm water management land

I (we) certify that this application and the documents submitted with it are true and correct to the best of my (our) knowledge and belief.



11/19/2024

Record Owner

Date



11/19/2024

Applicant or Authorized Agent

Date

**OWNERSHIP DISCLOSURE FORM  
LIMITED LIABILITY COMPANY (L.L.C.)**

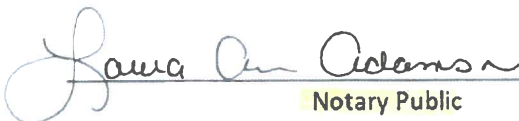
STATE OF ILLINOIS       )  
                                      ) SS.  
KANE COUNTY            )

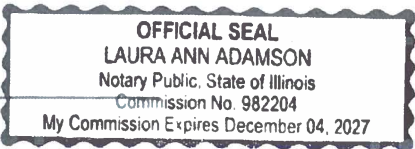
I, Chris McGrath, being first duly sworn on oath depose and say that I am  
Manager of HOBS E MAIN LLC, an Illinois Limited Liability  
Company (L.L.C.), and that the following persons are all of the members of the said L.L.C.:

<u>Gary McGrath</u>	_____
_____	_____
_____	_____
_____	_____
_____	_____
_____	_____
_____	_____

By: , Manager

Subscribed and Sworn before me this 20<sup>th</sup> day of  
NOV, 20 24.

  
Notary Public



City of St. Charles  
Community Development Division  
2 E. Main Street  
St. Charles, IL 60174



Phone: (630) 377-4443  
Email: [cd@stcharlesil.gov](mailto:cd@stcharlesil.gov)

## FINAL PLAT OF SUBDIVISION APPLICATION

### For City Use

Project Name: McGrath Bus. Center Phase 2 - Kid Parking Expansion  
Project Number: 2023 -PR- 003  
Cityview Project Number: PLP 202400041

Received Date

**RECEIVED**

DEC 2 5 2023

City of St. Charles  
Community Development

- File this application to request approval of a Final Plat of Subdivision.
- Complete the application and submit with all required attachments to the Community Development Division.
- The information you provide must be complete and accurate. If you have any questions please contact the Community Development Division.
- City staff will review the submittal for completeness and for compliance with applicable requirements. Staff will distribute the plans to other City departments for review when the application is complete.
- The Final Plat of Subdivision will be scheduled for Plan Commission review when staff has determined the plans are ready.

<b>1. Property Information:</b>	Location:		4085 E. MAIN STREET
	Parcel Number (s):		01-30-300-064
	Proposed Subdivision Name:		McGrath Business Center Subdivision Phase 2
<b>2. Applicant Information:</b>	Name:	Chris McGrath	Phone: 847-980-6184
	Address	2020 Randall Road Elgin, IL 60123	Email: mcgrathchris@yahoo.com
<b>3. Record Owner Information:</b>	Name:	4085 E Main LLC, c/o Chris McGrath	Phone: 847-980-6184
	Address:	2020 Randall Road Elgin, IL 60123	Email: mcgrathchris@yahoo.com

**4. Identify the Type of Application:**

- ☒ **Subdivision:**
- ☐ Preliminary Subdivision Plat was previously approved by the City
  - ☒ Combined Preliminary-Final Review Process (Preliminary Plat Application filed concurrently)
- ☒ **Planned Unit Development (PUD):**
- ☐ PUD Preliminary Plan was previously approved by the City
  - ☒ Combined Preliminary-Final Review Process (PUD Preliminary Plan Application filed concurrently)
  - ☐ PUD Final Plat Application filed concurrently

*\*Note- This application is not required for a Minor Subdivision. File only a Minor Subdivision-Final Plat application for a subdivision which qualifies as a Minor Subdivision per City Code Section 16.04.040.*

**5. Required Attachments:**

*If multiple zoning or subdivision applications will be submitted concurrently, do not submit duplicate checklist items or plans. Fee must be paid for each application.*

- ☒ **APPLICATION FEE:** \$300
- ☒ **REIMBURSEMENT OF FEES AGREEMENT:** An original, executed Reimbursement of Fees Agreement and deposit of funds in escrow with the City, as provided by Appendix B of the Zoning Ordinance.
- ☒ **REIMBURSEMENT OF FEES INITIAL DEPOSIT:** Deposit of funds in escrow with the City. Required deposit is based on review items (number of applications filed) and the size of the subject property:

Number of Review Items	Under 5 Acres	5-15 Acres	16-75 Acres	Over 75 Acres
1	\$1,000	\$2,000	\$3,000	\$4,000
2 or 3	\$2,000	\$4,000	\$5,000	\$7,000
4 or more	\$3,000	\$5,000	\$7,000	\$10,000

- ☐ **FEE FOR INSTALLATION OF CITY BENCHMARKS:** Payment for installation of City benchmarks in accordance with Appendix F of the Subdivision Code (City Code Title 16). Required payment is based on the size of the subdivision:

Subdivision Acreage	Number of Benchmarks	Fee at \$2500 per Benchmark
20+	2	\$5000
10 to 20	1	\$2500
5 to 10	0.5	\$1250
1 to 5	0.25	\$625
Less than 1	0.10	\$250

- ☐ **PROOF OF OWNERSHIP:** a) A current title policy report; or  
b) A deed and a current title search

*NOTE: Private covenants and deed restrictions can limit private property rights with respect to the use of land even though the City's Zoning Ordinance may authorize the use or a less restrictive use. We strongly advise that you perform a title search on the property to determine if there any private covenants containing use restrictions or other deed restrictions. As those private covenants and deed restrictions may conflict with the City's Zoning Ordinance, it is further recommended that you consult with an attorney to obtain an opinion with respect to whether your intended use is compatible with those restrictions.*

- ☒ **OWNERSHIP DISCLOSURE:** Use the appropriate disclosure form (attached), if the owner or applicant is a Partnership, Corporation, Trust, or LLC.
- ☐ **LETTER OF AUTHORIZATION:** If the property owner is not the applicant, an original letter of authorization from the property owner permitting the applicant to file the zoning application with the City of St. Charles for the subject property.
- ☒ **STORMWATER MANAGEMENT PERMIT APPLICATION:** If application is not already filed.
- ☒ **STORMWATER REPORT**
- ☒ **ENGINEER'S COST ESTIMATE SPREADSHEET:** Use the attached form.
- ☐ **FINANCIAL GUARANTEE & LAND IMPROVEMENT AGREEMENT:** With this application, provide a draft or description of the proposed guarantee for the payment and completion of Land Improvements. Include the proposed form, amount, and provider of completion guarantee collateral – bond, cash, or letter of credit. These items must be provided prior to the City signing the Final Plat of Subdivision and recording the plat.
  - **Letter of Credit form** – see City Code Title 16, Appendix C
  - **Land Improvement Agreement** – see City Code Title 16, Appendix D
- ☐ **PARK AND SCHOOL LAND/CASH WORKSHEETS:** *For residential developments only.* Use the attached worksheet to calculate the estimated population and student yields and resulting land/cash contributions in accordance with Title 16 of the St. Charles Municipal Code.
- ☐ **INCLUSIONARY HOUSING WORKSHEET:** *For residential developments only.* Use the attached worksheet to calculate the affordable unit requirement and indicate how the development will comply with Title 19 of the St. Charles Municipal Code.
- ☐ **COPIES OF THIRD PARTY PERMIT/APPROVALS:** Provide the applicable required items which may include:
  - Illinois EPA Water Pollution Control Permit for sanitary sewer extension
  - Illinois EPA Division of Public Works Supplies Permit for water mains
  - Notice of Intent (NOI) letter/permit for NPDES Stormwater Discharge for sites 5 acres and larger
  - IDNR Office of Water Resources Permit (for work in floodplain)
  - Wetlands Permit from Army Corps of Engineers
  - Kane County DOT and/or IDOT signature on Final Plat (if applicable)
  - Offsite easements and right-of-way necessary to construct the required Land Improvements

**PLANS:** All required plans shall be drawn on sheets no larger than 24" x 36", unless the Director of Community Development permits a larger size when necessary to show a more comprehensive view of the project. All required plans shall show north arrow and scale, and shall be drawn at the same scale (except that a different scale may be used to show details or specific features). All plans shall include the name of the project, developer or owner of site, person or firm preparing the plan, and the date of plan preparation and all revisions.

**Copies:** Ten (10) full size copies, one (1) 11" by 17", and PDF electronic file emailed to: [cd@stcharlesil.gov](mailto:cd@stcharlesil.gov)

- ☒ **FINAL PLAT OF SUBDIVISION / DRAWING REQUIREMENTS CHECKLIST:** A Final Plat of Subdivision that includes the information listed on the Subdivision Plat Drawing Requirements Checklist. Also submit a completed Checklist (attached).
- ☒ **FINAL ENGINEERING PLANS / DRAWING REQUIREMENTS CHECKLIST:** Final Engineering plans that include the information listed on the Drawing Requirements Checklist-Final Engineering Plans. Also submit a completed Checklist (attached).



I (we) certify that this application and the documents submitted with it are true and correct to the best of my (our) knowledge and belief.



11/19/2024

Record Owner

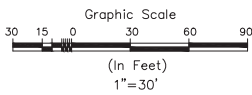
Date



11/19/2024

Applicant or Authorized Agent

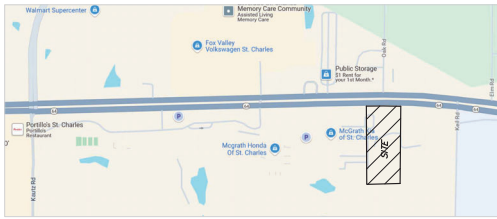
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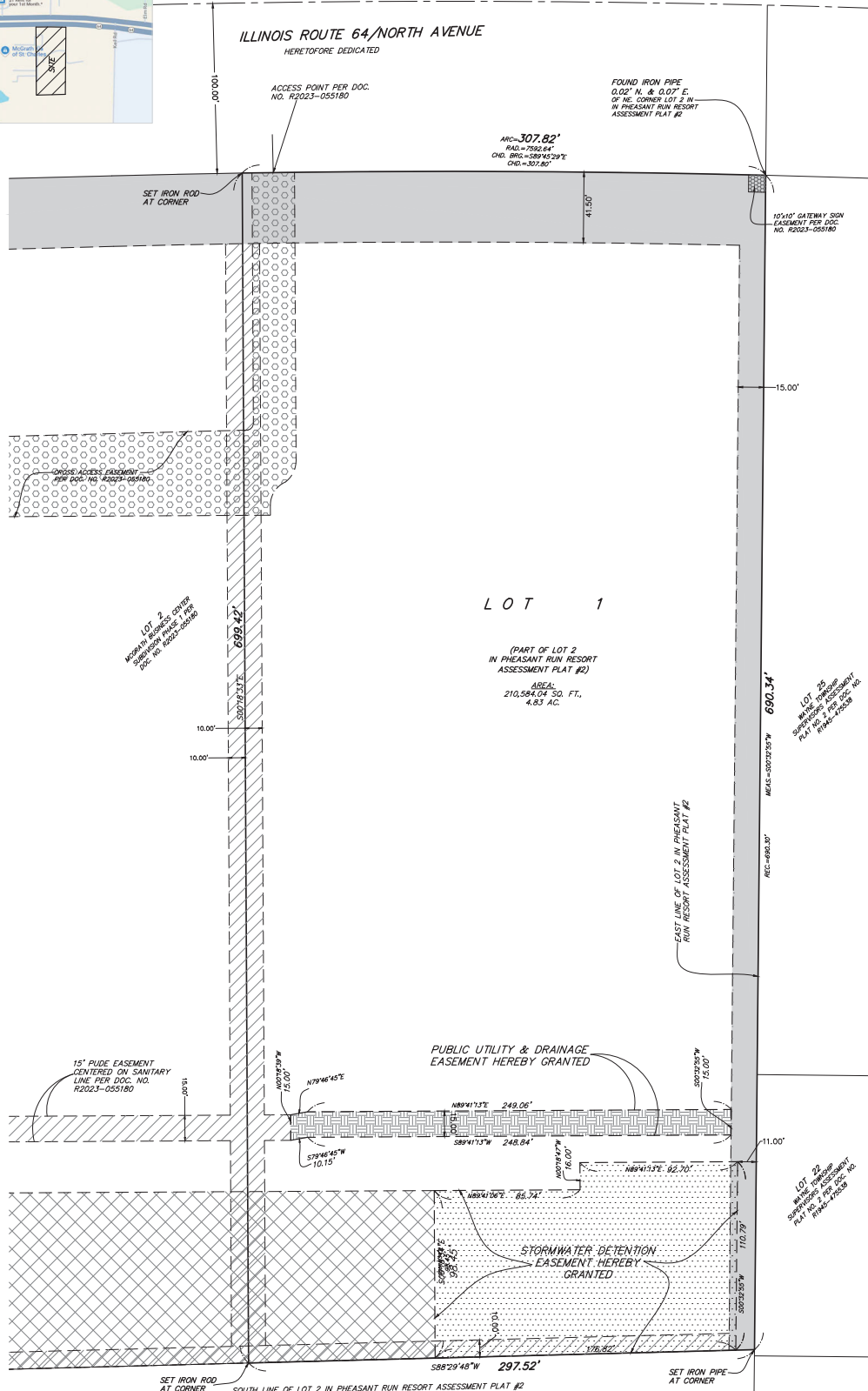
# McGRATH BUSINESS CENTER SUBDIVISION PHASE 2

BEING A RESUBDIVISION OF PART OF THE SOUTH HALF OF SECTION 30, TOWNSHIP 40 NORTH, RANGE 9, EAST OF THE THIRD PRINCIPAL MERIDIAN, IN DUPAGE COUNTY, ILLINOIS

P. I. N: 01-30-300-064



VICINITY MAP



## LEGEND

- PUBLIC UTILITY AND DRAINAGE EASEMENT GRANTED PER DOC. NO. R2023-047461
- STORMWATER DETENTION EASEMENT GRANTED PER DOC. NO. R2023-055180
- PUBLIC UTILITY AND DRAINAGE EASEMENT GRANTED PER DOC. NO. R2023-055180
- CROSS ACCESS EASEMENT GRANTED PER DOC. NO. R2023-055180
- STORMWATER DETENTION EASEMENT HEREBY GRANTED
- PUBLIC UTILITY AND DRAINAGE EASEMENT HEREBY GRANTED

OWNER'S NAME & ADDRESS:  
JOS E HAY LLC  
2020 N RANDALL RD  
ELGIN IL 60123

## EASEMENT AREA TABLE

EASEMENT DESCRIPTION	AREA (SQ. FT.)
EXISTING PUBLIC UTILITY AND DRAINAGE EASEMENT PER DOC. NO. R2023-047461	22,499.86
EXISTING STORMWATER DETENTION EASEMENT GRANTED PER DOC. NO. R2023-055180	10,921.30
EXISTING PUBLIC UTILITY AND DRAINAGE EASEMENT GRANTED PER DOC. NO. R2023-055180	9,536.98
EXISTING CROSS ACCESS EASEMENT GRANTED PER DOC. NO. R2023-055180	5,038.91
PROPOSED STORMWATER DETENTION EASEMENT	16,628.40
PROPOSED PUBLIC UTILITY AND DRAINAGE EASEMENT	3,884.28

NO.	DATE	DESCRIPTION	BY
1	06/02/2023	REVISED PER ENCL. DATED 05/01/2023	WMS



GENTILE & ASSOCIATES, INC.  
PROFESSIONAL LAND SURVEYORS  
550 E. ST. CHARLES PLACE  
LOMBARD, ILLINOIS 60148  
PHONE (630) 916-6262

PREPARED FOR: RWG ENGINEERING, LLC  
DRAWN BY: WMS  
ORDER NO.: 22034-24 SUB-REV1  
SHEET 1 OF 2

ILLINOIS PROFESSIONAL DESIGN  
FIRM LICENSE NO. 184.002870



RWG Engineering, LLC  
CIVIL ENGINEERING - REAL ESTATE CONSULTING - PROJECT MANAGEMENT  
ILLINOIS PROFESSIONAL DESIGN FIRM #184-006370  
LIMITATION OF WARRANTY OF ENGINEER'S INSTRUMENTS OF SERVICE

THE ENGINEER AND HIS CONSULTANTS DO NOT WARRANT OR GUARANTEE THE ACCURACY AND COMPLETENESS OF THE DELIVERABLES HEREIN BEYOND A REASONABLE DILIGENCE. IF ANY MISTAKES, OMISSIONS, OR DISCREPANCIES ARE FOUND TO EXIST WITHIN THE DELIVERABLES, THE ENGINEER SHALL BE PROMPTLY NOTIFIED SO THAT HE MAY HAVE THE OPPORTUNITY TO TAKE THE NECESSARY STEPS TO CORRECT THEM. FAILURE TO PROMPTLY NOTIFY THE ENGINEER OF SUCH CONDITIONS SHALL ABSOLVE THE ENGINEER FROM ANY RESPONSIBILITY FOR THE CONSEQUENCES OF SUCH FAILURE. ACTIONS TAKEN WITHOUT THE KNOWLEDGE AND CONSENT OF THE ENGINEER, OR IN CONTRADICTION TO THE ENGINEER'S DELIVERABLES OR RECOMMENDATIONS, SHALL BECOME THE RESPONSIBILITY NOT OF THE ENGINEER BUT OF THE PARTIES RESPONSIBLE FOR TAKING SUCH ACTION.

LEGEND	
EXISTING	PROPOSED
SANITARY MANHOLE	⊙
STORM MANHOLE	⊙
CATCH BASIN	⊙
INLET	⊙
PRECAST FLARED END SECTION	⊙
CONCRETE HEADWALL	⊙
VALVE VAULT	⊙
VALVE BOX	⊙
FIRE HYDRANT	⊙
BUFFALO BOX	⊙
CLEANOUT	⊙
SANITARY SEWER	—
FORCE MAIN	—
STORM SEWER	—
WATER MAIN	—
CONSTRUCT WATER MAIN UNDER SEWER	—
GRANULAR TRENCH BACKFILL	—
STREET LIGHT	—
ELECTRICAL CABLE	—
SECURITY BOLLARD	—
ELECTRICAL TRANSFORMER ON PEDESTAL	—
POWER POLE	—
STREET SIGN	—
GAS MAIN	—
TELEPHONE LINE	—
CONTOUR	—
SPOT ELEVATION	—
WETLANDS	—
FLOODWAY	—
FLOODPLAIN	—
HIGH WATER LEVEL (HWL)	—
NORMAL WATER LEVEL (NWL)	—
DIRECTION OF SURFACE FLOW	—
DITCH OR SWALE	—
OVERFLOW RELIEF ROUTING	—
SLOPE BANK	—
TREE WITH TRUNK SIZE	—
SOIL BORING	—
TOPSOIL PROBE	—
FENCE LINE, WIRE OR SILT	—
FENCE LINE, CHAIN LINK OR IRON	—
FENCE LINE, WOOD OR PLASTIC	—
CONCRETE SIDEWALK	—
CURB AND GUTTER	—
DEPRESSED CURB	—
REVERSE PITCH CURB & GUTTER	—
EASEMENT LINE	—

ABBREVIATIONS	
BL	BASE LINE
C	LONG CHORD OF CURVE
C & G	CURB AND GUTTER
CB	CATCH BASIN
CE	CENTERLINE
D	DEGREE OF CURVE
ED	EDGE OF PAVEMENT
FF	FINISHED FLOOR
FG	FINISHED GRADE
FL	FLOW LINE
FP	FLOODPLAIN
FR	FRAME
FW	FLOODWAY
HWL	HIGH WATER LEVEL
INV	INVERT
L	LENGTH OF CURVE
MH	MANHOLE
NWL	NORMAL WATER LEVEL
PC	POINT OF CURVATURE
PT	POINT OF TANGENCY
PVI	POINT OF VERTICAL INTERSECTION
R	RADIUS
ROW	RIGHT-OF-WAY
SAN	SANITARY SEWER
ST	STORM SEWER
T	TANGENCY OF CURVE
TB	TOP OF BANK
TC	TOP OF CURB
TF	TOP OF FOUNDATION
TP	TOP OF PIPE
TS	TOP OF SIDEWALK
TW	TOP OF WALK
WM	WATER MAIN
Δ	INTERSECTION ANGLE

**811** Know what's below.  
Call before you dig.

Formerly JULIE 1-800-892-0123

# FINAL ENGINEERING PLANS

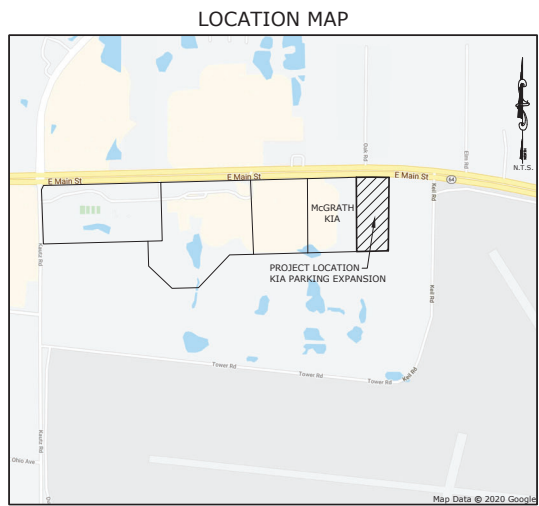
FOR

## KIA PARKING EXPANSION

AT

### McGRATH BUSINESS CENTER

4085 E. MAIN STREET  
ST. CHARLES, IL



### GENERAL NOTES

- The contractor shall notify the following governmental agencies at least two working days prior to commencement of construction:
  - City of St. Charles Public Works Department (630-377-4405)
- The contractor shall notify all utility companies and arrange for their facilities to be located prior to work in any easement, right-of-way, or suspected utility location. Repair of any damage to existing facilities shall be the responsibility of the contractor. Utility locations shown herein are for graphic illustration only and are not to be relied upon.
- Prior to commencement of any offsite construction, the contractor shall secure written authorization that all offsite easements have been secured, and that permission has been granted to enter onto private property.
- Elevations shown herein reflect NAVD 1988 datum.
- The boundary survey data for this project is based on a boundary and topographic survey prepared by Gentile and Associates, Inc. dated September 27, 2024. The contractor shall verify existing conditions prior to commencing construction and shall immediately notify the engineer in writing of any differing conditions.
- RWG Engineering, LLC, its employees and agents are not responsible for the safety of any party at or on the construction site. Safety is the sole responsibility of the contractor, and any other entity performing work at the site. Neither the owner nor the engineer assumes any responsibility for job site safety or for the means, methods or sequences of construction.
- Except where modified by the contract documents, all work proposed herein shall be in accordance with the following specifications, which are hereby made a part hereof:
  - "Standard Specifications for Road and Bridge Construction in Illinois," as prepared by I.D.O.T. latest edition.
  - "Standard Specifications for Water and Sewer Main Construction in Illinois," latest edition.
  - "Standard Specifications for Sewage Works," as published by the I.E.P.A., latest edition.
  - The subdivision and development codes and standards of the City of St. Charles, as published by the Municipality.
  - "Illinois Recommended Standards for Sewage Works," as published by the State of Illinois Capital Development Board, effective October 23, 2018.
  - The National Electric Code.
  - "Illinois Urban Manual" as prepared by the U.S. Dept. of Agriculture latest edition.
- City of St. Charles Development Ordinance shall take precedence if a conflict in project specifications occurs.

PLANS PREPARED FOR  
**4085 E MAIN LLC**  
2020 RANDALL ROAD  
ELGIN, IL 60123  
630-721-9500

### INDEX OF SHEETS

- TITLE SHEET
- EXISTING CONDITIONS PLAN (1"=40')
- DEMOLITION PLAN (1"=40')
- SITE GEOMETRIC AND PAVING PLAN (1"=40')
- SOIL EROSION AND SEDIMENT CONTROL PLAN (1"=40')
- S.E.S.C. DETAILS AND SCHEDULES
- GRADING PLAN (1"=40')
- UTILITY PLAN (1"=40')
- MISCELLANEOUS DETAILS
- PROJECT NOTES AND SPECIFICATIONS
- CONSTRUCTION STANDARDS AND DETAILS

### BENCHMARKS

**BENCHMARK:** DUPAGE COUNTY B.M. NO. WC105  
THE STATION IS LOCATED AT THE NORTHEAST CORNER OF THE INTERSECTION OF POWIS ROAD AND POWIS COURT. THE STATION IS 55.4 FEET EAST OF THE CENTERLINE OF POWIS ROAD, 47.8 FEET NORTH OF THE CENTERLINE OF POWIS COURT AND 16.2 FEET NORTHWEST OF A STORM MANHOLE. THE MONUMENT IS A 3.5 INCH ALUMINUM DISK WITH A LID. THE MONUMENT IS FLUSH WITH GRADE AND 2.8 FEET ABOVE ROAD SURFACE AND IS FERROMAGNETIC.  
ELEVATION=757.1 (NAVD 88 DATUM)

**SITE BENCHMARK:**  
CROSS OUT ON TOP OF CONCRETE CURB NEAR NORTHWEST CORNER OF SUBJECT PROPERTY, WEST SIDE OF DRIVE.  
ELEVATION=759.65 (NAVD 88 DATUM)

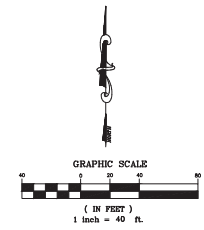
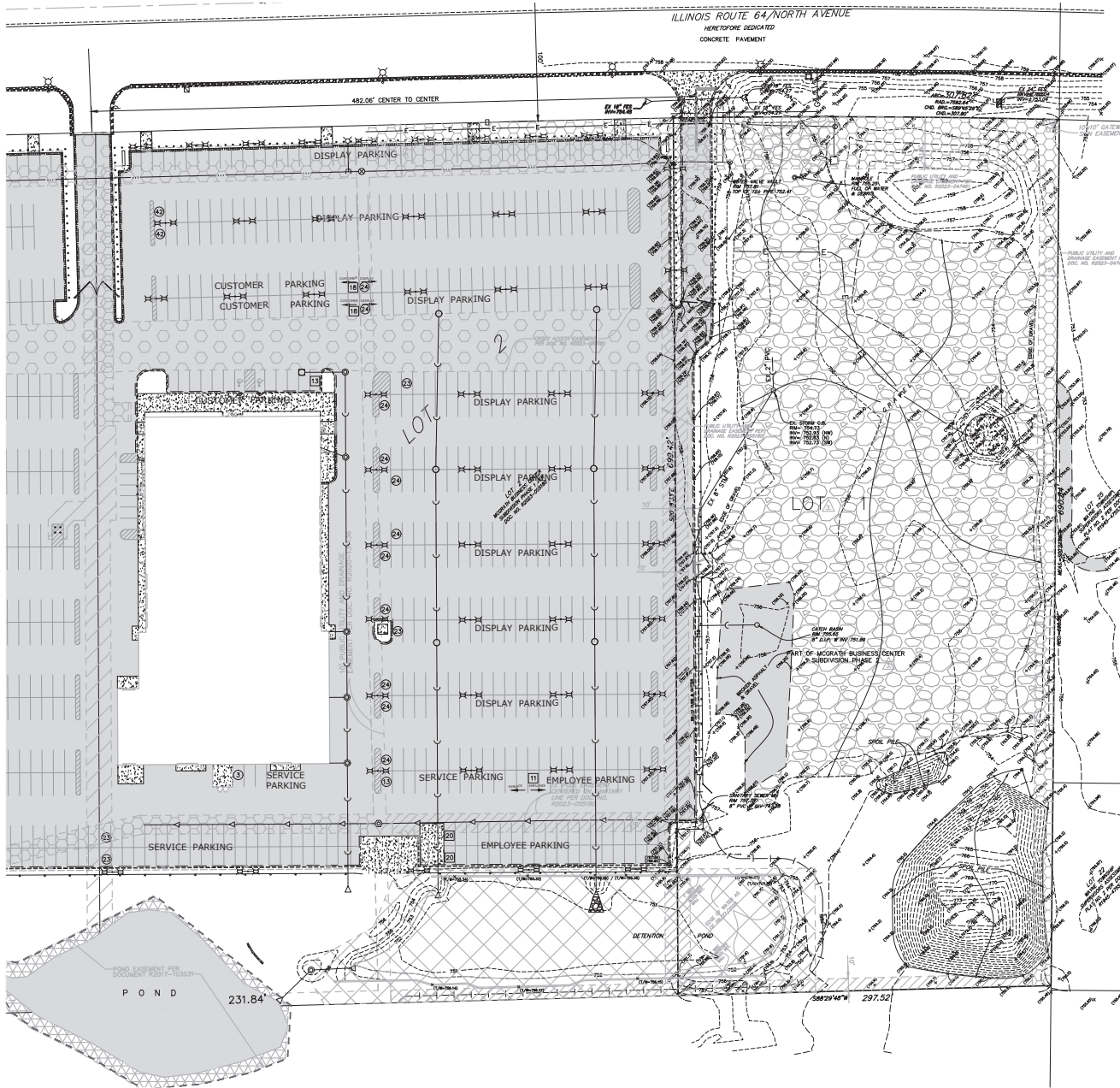


DATE	11/20/24	REVISIONS	1. 11/20/24
DRAWN BY	AVS	BY	AVS
CHECKED BY	AVS	DATE	11/20/24
PROJECT NO.	80056934	SCALE	NONE
PROJ. MGR.	AVS	PROJ. ASSOC.	AVS
DRAWN BY	AVS		
SHEET			
C1 of 11			

KIA PARKING EXPANSION  
ST. CHARLES, IL  
TITLE SHEET

RWG Engineering, LLC  
975 E. 22nd St., Suite 400  
St. Charles, IL 60123  
630-480-7889  
www.rwg-engineering.com





THE INFORMATION SHOWN HEREON IS BASED ON A BOUNDARY AND TOPOGRAPHIC SURVEY DONE BY GENTLE AND ASSOCIATES, INC. JOB NO. 22034-24 TOPG, DATED 08/27/24.

- ABBREVIATIONS LEGEND**  
(IN ADDITION TO TITLE SHEET LEGEND)
- EX = EXISTING
  - PR = PROPOSED
  - BC = BACK OF CURB
  - FC = FACE OF CURB
  - EP = EDGE OF PAVEMENT
  - PL = PROPERTY LINE
  - FB = FACE OF BUILDING
  - EC = EDGE OF CONCRETE
  - RW = RETAINING WALL
  - ROW = RIGHT OF WAY
  - BC/BC = BACK OF CURB TO BACK OF CURB
  - SW = SIDEWALK
  - R = RADIUS
  - (TYP) = TYPICAL

- EASEMENT LEGEND**
- PUBLIC UTILITY AND DRAINAGE EASEMENT PER DEC. NO. R2023-047461
  - STORMWATER DETENTION EASEMENT GRANTED PER DEC. NO. R2023-055180
  - PUBLIC UTILITY AND DRAINAGE EASEMENT GRANTED PER DEC. NO. R2023-055180
  - CROSS ACCESS EASEMENT GRANTED PER DEC. NO. R2023-055180

**KIA PARKING EXPANSION  
ST. CHARLES, ILLINOIS  
EXISTING CONDITIONS PLAN**

975 E. 22nd St. Suite 400  
St. Charles, IL 63304  
630.480.7889  
www.rwg-engineering.com

**RWG Engineering, LLC**  
Civil Engineering • Survey • Construction • Project Management

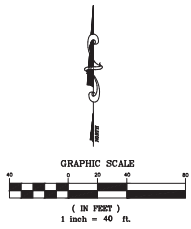
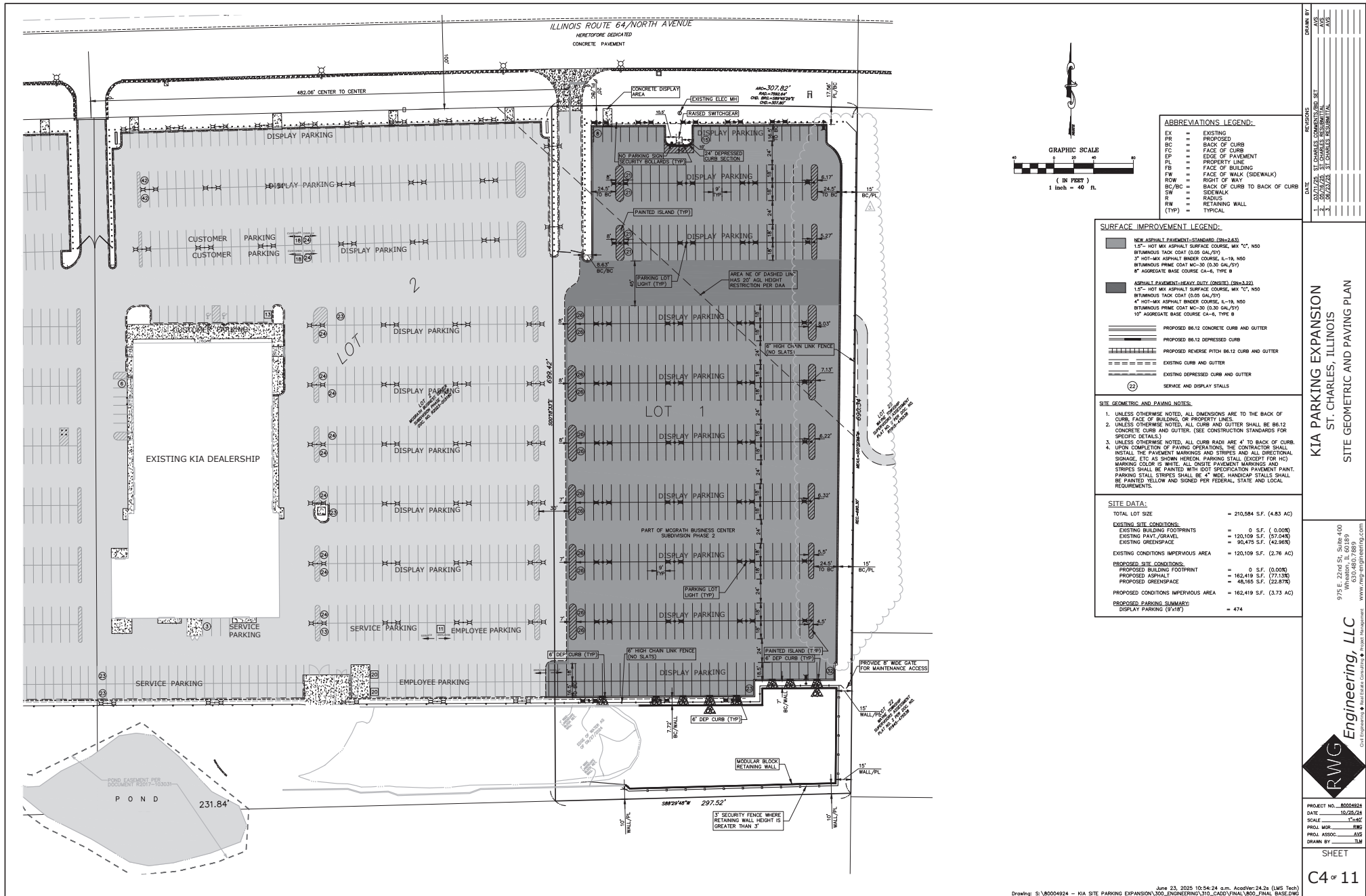
PROJECT NO. 80004924  
DATE 10/25/24  
SCALE 1"=40'  
PROJ. MGR. JRM  
PROJ. ASSOC. AVS  
DRAWN BY TJM

SHEET

C2 of 11







**ABBREVIATIONS LEGEND:**

EX	=	EXISTING
PR	=	PROPOSED
BC	=	BACK OF CURB
FC	=	FACE OF CURB
EP	=	EDGE OF PAVEMENT
PL	=	PROPERTY LINE
FW	=	FACE OF BUILDING
ROW	=	RIGHT OF WAY
BC/BW	=	BACK OF CURB TO BACK OF CURB
SW	=	SIDEWALK
R	=	RADIUS
RW	=	RETAINING WALL (TYP)

**SURFACE IMPROVEMENT LEGEND:**

	NEW ASPHALT PAVEMENT-STANDARD (2in-2.43)
	1.5\"/>
	3\"/>
	BITUMINOUS PRIME COAT MC-30 (0.50 GAL/SY)
	8\"/>
	PROPOSED B6.12 CONCRETE CURB AND GUTTER
	PROPOSED B6.12 DEPRESSED CURB
	PROPOSED REVERSE PITCH B6.12 CURB AND GUTTER
	EXISTING CURB AND GUTTER
	EXISTING DEPRESSED CURB AND GUTTER
	SERVICE AND DISPLAY STALLS

- SITE GEOMETRIC AND PAVING NOTES:**
- UNLESS OTHERWISE NOTED, ALL DIMENSIONS ARE TO THE BACK OF CURB, FACE OF BUILDING, OR PROPERTY LINE.
  - UNLESS OTHERWISE NOTED, ALL CURB AND GUTTER SHALL BE B6.12 CONCRETE CURB AND GUTTER. (SEE CONSTRUCTION DETAILS FOR SPECIFIC DETAILS).
  - UNLESS OTHERWISE NOTED, ALL CURB RADI ARE 4' TO BACK OF CURB. UPON COMPLETION OF PAVING OPERATIONS, THE CONTRACTOR SHALL INSTALL THE PAVEMENT MARKINGS AND STRIPES AND ALL DIRECTIONAL SIGNS, ETC. AS SHOWN HEREON. PARKING STALL (EXCEPT FOR HO) MARKING COLOR IS WHITE. ALL ONSITE PAVEMENT MARKINGS AND STRIPES SHALL BE PAINTED WITH DOT SPECIFICATION PAVEMENT PAINT. PARKING STALL STRIPES SHALL BE 4" WIDE. HANDICAP STALLS SHALL BE PAINTED YELLOW AND SIGNED PER FEDERAL, STATE AND LOCAL REQUIREMENTS.
  -

**SITE DATA:**

TOTAL LOT SIZE	=	210,584 S.F. (4.83 AC)
EXISTING SITE CONDITIONS:		
EXISTING BUILDING FOOTPRINTS	=	0 S.F. (0.00%)
EXISTING PAVT./DRAVAIL	=	120,109 S.F. (57.04%)
EXISTING GREENSPACE	=	90,475 S.F. (42.96%)
EXISTING CONDITIONS IMPERVIOUS AREA	=	120,109 S.F. (2.76 AC)
PROPOSED SITE CONDITIONS:		
PROPOSED BUILDING FOOTPRINT	=	0 S.F. (0.00%)
PROPOSED ASPHALT	=	162,419 S.F. (77.13%)
PROPOSED GREENSPACE	=	48,165 S.F. (22.87%)
PROPOSED CONDITIONS IMPERVIOUS AREA	=	162,419 S.F. (3.73 AC)
PROPOSED PARKING SUMMARY:		
DISPLAY PARKING (9,181)	=	474

**KIA PARKING EXPANSION**  
**ST. CHARLES, ILLINOIS**  
**SITE GEOMETRIC AND PAVING PLAN**

**Engineering, LLC**

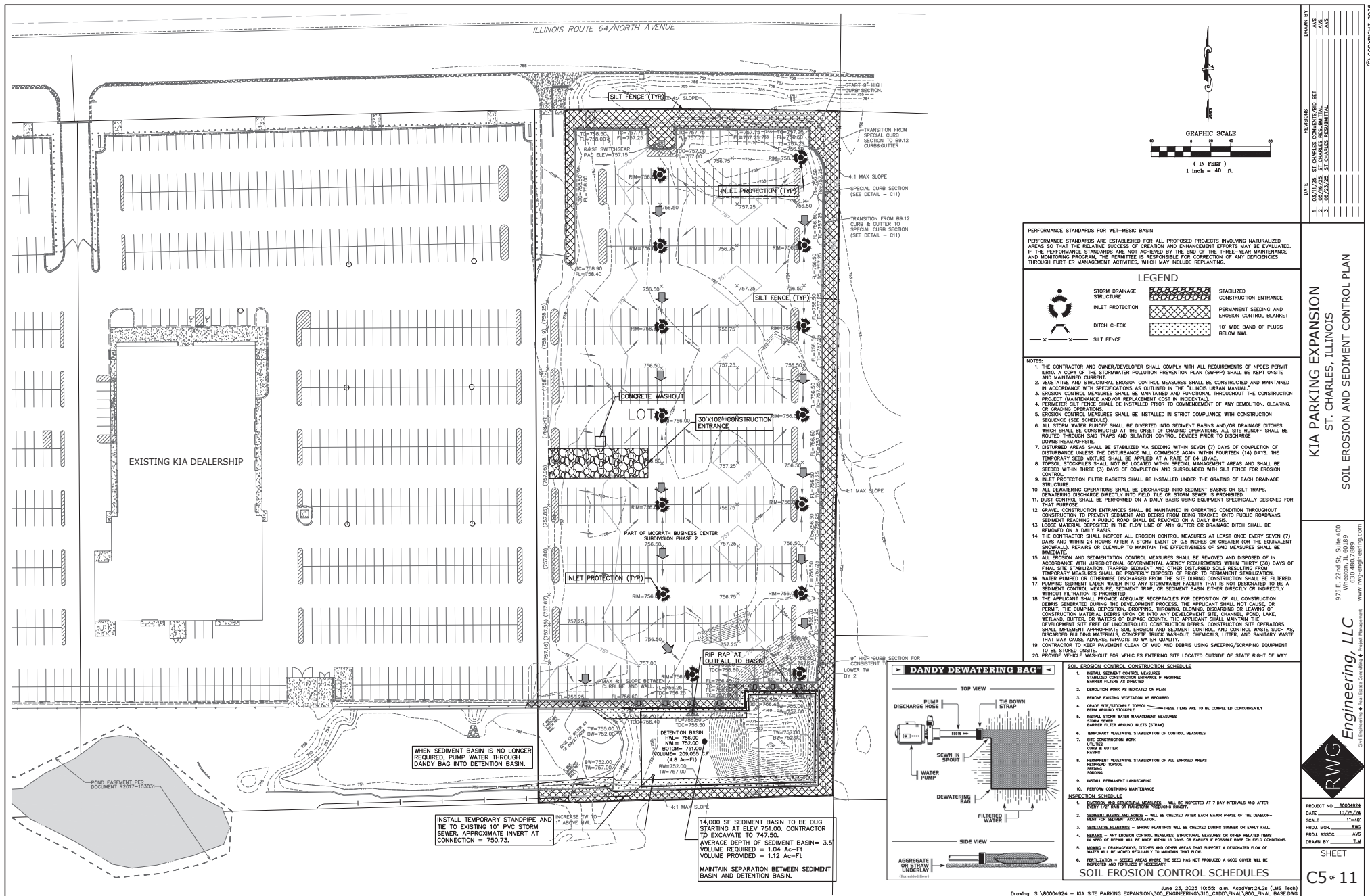
PROJECT NO.: 80004924  
DATE: 10/25/24  
SCALE: 1"=40'  
PROJ. MGR.: JRM  
PROJ. ASSOC.: AVS  
DRAWN BY: JLM

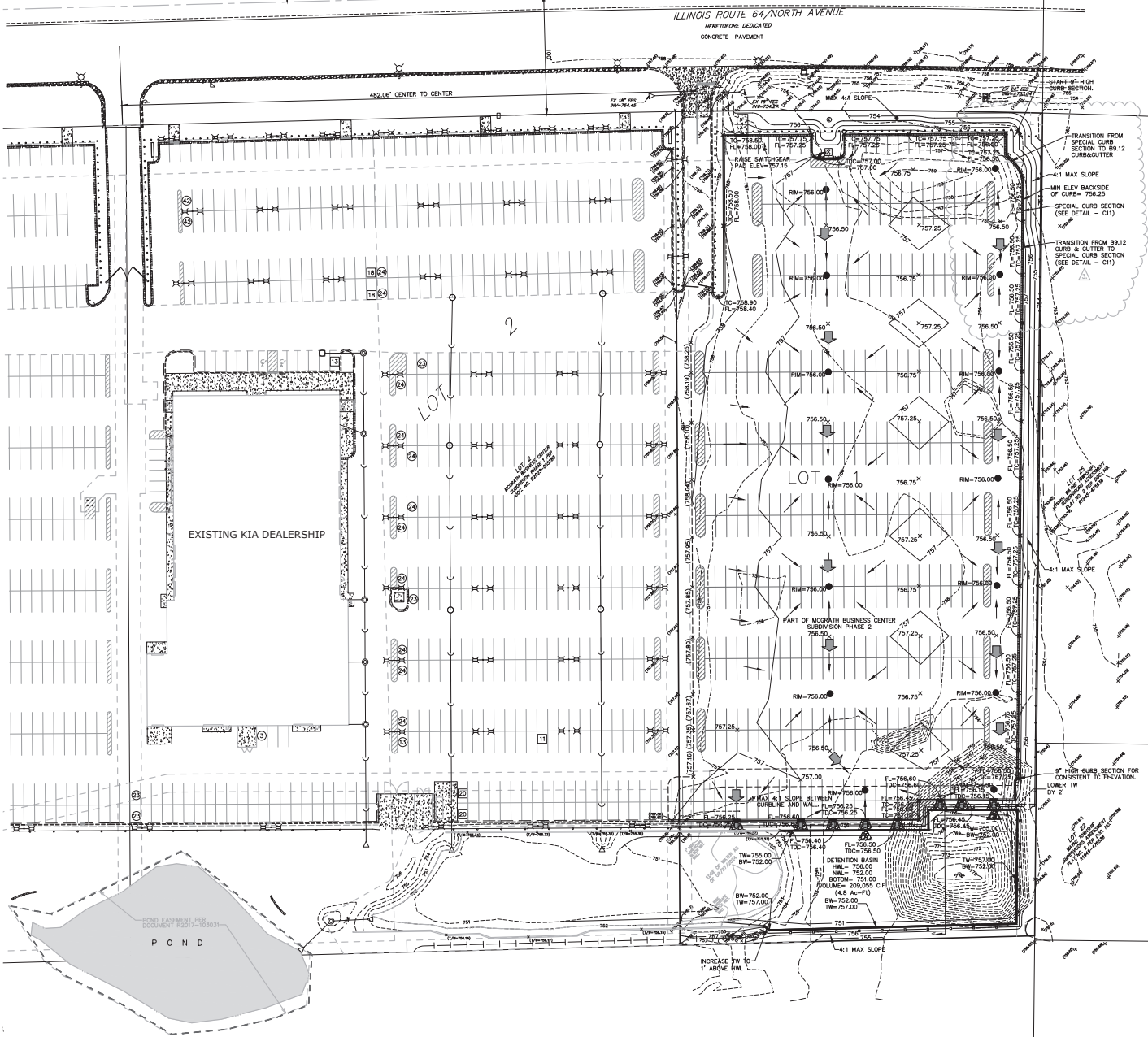
SHEET  
**C4 of 11**

REVISIONS

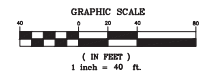
DATE	BY	REVISIONS
10/25/24	AVS	1. ST. CHARLES SUBMITTAL SET
10/25/24	AVS	2. ST. CHARLES SUBMITTAL

DRAWN BY: AVS





- GRADING NOTES.**
1. UNLESS SPECIFICALLY INDICATED OTHERWISE, EXISTING GRADES ARE TO BE MET AT PROJECT PERIMETER PROPERTY LINES.
  2. THE CONTRACTOR SHALL REFER TO THE SOIL EROSION AND SEDIMENT CONTROL PLANS AND DETAILS (AND SWPPP DOCUMENTS IF APPLICABLE) FOR THE INSTALLATION OF EROSION CONTROL MEASURES PRIOR TO BEGINNING GRADING OPERATIONS.
  3. UNLESS OTHERWISE NOTED, SPOT ELEVATIONS AND PROPOSED CONTOURS REFLECT THE FINISHED SURFACE GRADE. TOP OF CURB GRADE, OR FINISHED GROUND ELEVATION AS APPLICABLE.
  4. RM GRADES FOR DRAINAGE STRUCTURES REFLECT THE FLOW LINE ELEVATION OF THE GUTTER, PAVEMENT OR DRAINAGE SWALE (AS APPLICABLE).
  5. WHEREVER THE DESIGN FOR SURFACE FLOW OF DRAINAGE IS DIRECTED AWAY FROM A CURB, THE CONTRACTOR SHALL INSTALL REVERSED GUTTER PITCH.
  6. CONTRACTOR TO REFER TO THE LANDSCAPE PLANS TO DETERMINE TOPSOIL REPLACEMENT DEPTH IN ALL DISTURBED GREENSPACE AREAS.
  7. CONTRACTOR TO PROVIDE STRUCTURAL DRAWINGS FOR RETAINING WALLS FOR REVIEW PRIOR TO CONSTRUCTION. DRAWINGS WILL TAKE INTO ACCOUNT FENCE POSTS WITHIN THE GEODIG AND DEPRESSED CURB OVERFLOW LOCATIONS.



**PLAN ABBREVIATION LEGEND**  
(IN ADDITION TO TITLE SHEET)

P	= PAVEMENT
R	= RM (OR RADIUS)
TO	= TOP OF CURB
SW	= SWALK (OR TOP OF WALK)
FT	= FINISHED FLOOR
EP	= EDGE OF PAVEMENT
FL	= FLOW LINE
TW	= TOP OF WALL ELEV
BW	= BOTTOM OF WALL (GRADE) ELEV
TOC	= TOP OF DEPRESSED CURB

**GRADING LEGEND**

EXISTING	PROPOSED
CONCRETE HEADWALL	CONCRETE HEADWALL
PRECAST FLARED END SECTION	PRECAST FLARED END SECTION
STORM DRAINAGE STRUCTURE	STORM DRAINAGE STRUCTURE
RETAINING WALL	RETAINING WALL
CONTOUR	CONTOUR
5' CONTOUR	5' CONTOUR
SPOT ELEVATION	SPOT ELEVATION
DIRECTION OF SURFACE FLOW	DIRECTION OF SURFACE FLOW
DITCH OR SWALE	DITCH OR SWALE
OVERFLOW RELIEF ROUTING	OVERFLOW RELIEF ROUTING
SLOPE BANK	SLOPE BANK

**STORMWATER MANAGEMENT BASIN DATA:**  
(COMBINED DEALERSHIP & PARKING EXPANSION)

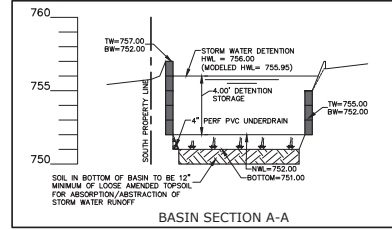
REQUIRED DETENTION VOLUME =	224,253 C.F.
PROVIDED DETENTION VOLUME =	209,055 C.F.
REQUIRED BMP VOLUME =	25,594 C.F.
PROVIDED BMP VOLUME =	36,678 C.F.
ALLOWABLE RELEASE RATE =	0.832 CFS
ACTUAL BASIN RELEASE RATE =	0.790 CFS
TOTAL DESIGN RELEASE RATE =	0.832 CFS
(+ 0.042 CFS FROM UNDERDRAIN)	

**STORMWATER MANAGEMENT BASIN DATA:**  
(DEALERSHIP REQUIRED AND AS-BUILT)

REQUIRED DETENTION VOLUME =	111,900 C.F.
AS-BUILT DETENTION VOLUME =	123,357 C.F.
REQUIRED BMP VOLUME =	13,041 C.F.
AS-BUILT BMP VOLUME =	17,561 C.F.
ALLOWABLE RELEASE RATE =	0.502 CFS
AS-BUILT RELEASE RATE =	0.473 CFS

**STAGE STORAGE DISCHARGE TABLE:**  
(COMBINED DEALERSHIP & PARKING EXPANSION)

STAGE	ELEV	STORAGE	DISCHARGE
0.00	753.00	0	0.00
1.00	752.00	36,678	0.00
2.00	753.00	77,940	0.38
3.00	754.00	120,439	0.57
4.00	755.00	163,505	0.71
5.00	756.00	209,055	0.82



**REVISIONS**

DATE	BY	REVISION
10/26/24	AVS	1. INITIAL SET
10/26/24	AVS	2. REVISIONS

**KIA PARKING EXPANSION**  
ST. CHARLES, ILLINOIS  
GRADING PLAN

975 E. 22nd St., Suite 400  
St. Charles, IL 60187  
630.480.7889  
www.rwg-engineering.com



PROJECT NO. 80004924  
DATE 10/26/24  
SCALE 1"=40'  
PROJ. MGR. JRM  
PROJ. ASSOC. AVS  
DRAWN BY JLM

SHEET  
C7 of 11





REMOVE EXISTING PARKWAY TREES, AND REPAIR DAMAGE LAWN WITH SEEDED LAWN TYPE 'A' SEE GENERAL NOTES

SEEDED LAWN TYPE 'A' SEE GENERAL NOTES

3 SR 27 JC 4 SR 8 SC

44 JC 1 QW

EXISTING SHRUBS TO REMAIN

SOODED LAWN

EARLIER PHASE, ALREADY CONSTRUCTED

PERIMETER SECURITY FENCE, SEE CIVIL DRAWINGS

SEEDED LAWN TYPE 'B' SEE GENERAL NOTES

RIP-RAP, SEE CIVIL DRAWINGS

DETENTION POND, SEE CIVIL DRAWINGS

RETAINING WALLS, SEE CIVIL DWGS.

10' WIDE BAND OF PLUGS, BELOW NWL 1,820 PLUS TOTAL SPACED 2' OC. SEE GENERAL NOTES

SEEDED LAWN TYPE 'B' SEE GENERAL NOTES

Age Group	Percentage
0-5	10
5-10	15
10-20	10
20-40	35
40-80	30

[illegible]

Diagram illustrating the cross-section of a tree bed with the following instructions:

- SHREDDED HARDWOOD BARK MULCH, 2" DEPTH. TAPER MULCH DOWN AT ROOT FLARE SO THAT MULCH DOES NOT TOUCH STEMS. EXTEND MULCH TO ENDS OF BED, AS SHOWN ON PLANS.
- REMOVE CONTAINER AND LOOSEN ANY POT BOUND ROOTS.
- PLANTING MIX. SEE GENERAL NOTES.
- LOOSEN OR SCARIFY EXISTING SOIL BELOW PLANTING BED.

PLUGS TO BE INSTALLED IN A 10" WIDE BAND BELOW THE NORMAL WATER LINE, AND ARE TO BE SPACED 24" OC. SEE GENERAL NOTES FOR SPECIAL STORMWATER MANAGEMENT REQUIREMENTS.

PLUGS SPACED 24" O.C. SEE LIST AND GENERAL NOTES

10'

SEE CIVIL DRAWINGS FOR RETAINING WALLS

NWL

This diagram shows a cross-section of a retaining wall. A horizontal dimension line indicates a distance of 10 feet from the wall face to the center of a plug. A note above the wall states 'PLUGS SPACED 24" O.C. SEE LIST AND GENERAL NOTES'. Another note points to the wall face, stating 'SEE CIVIL DRAWINGS FOR RETAINING WALLS'. The water level is indicated by a wavy line labeled 'NWL' (Normal Water Level). Arrows point downwards from the water level to the plugs.

PLANT LIST						
CODE	BOTANICAL NAME	COMMON NAME	SIZE	QTY	ADDITIONAL NOTES	
DECIDUOUS TREES						
QW	QUERCUS WAREI 'LONG'	REGAL PRINCE OAK	3" CALIPER	5	BRANCHED UP 6'	
SR	SYRINGA RETICULATA 'IVORY SILK'	IVORY SILK JAPANESE TREE LILAC	3" CALIPER	7	BRANCHED UP 6'	
EVERGREEN SHRUBS						
JC	JUNIPERUS CHINENSIS 'SEA GREEN'	SEA GREEN JUNIPER	18" HT x 24" W	71		
DECIDUOUS SHRUBS						
SC	SPIRAEA x CINEREA 'GRIFFSHEIM'	GARLAND SPIREA	18" HT x 24" W	8		

1. ALL DIMENSIONS AND SITE CONDITIONS ARE TO BE VERIFIED IN THE FIELD BY THE CONTRACTOR. NOTIFY THE LANDSCAPE ARCHITECT OF ANY DISCREPANCIES PRIOR TO STARTING WORK. SITE PLAN, ARCHITECTURE PLAN AND ANY OTHERS WERE PROVIDED BY OTHERS.

2. THE CONTRACTOR SHALL HAVE A MINIMUM OF FIVE YEARS EXPERIENCE WITH PROJECTS OF SIMILAR SIZE AND TYPE. CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL REQUIRED PERMITS.

3. SPECIES AND SIZES OF PLANTS LISTED IN THE PLANT LIST ARE SUBJECT TO AVAILABILITY AT TIME OF INSTALLATION. IF SUBSTITUTIONS ARE REQUIRED, THE CONTRACTOR SHALL SUBMIT REQUESTS TO THE LANDSCAPE ARCHITECT FOR APPROVAL.

4. ALL OVERHEAD AND UNDERGROUND UTILITIES ARE TO BE LOCATED PRIOR TO DIGGING OR EXCAVATION. IF UTILITIES OR OTHER OBSTACLES ARE ENCOUNTERED, THE CONTRACTOR SHALL NOTIFY THE LANDSCAPE ARCHITECT IMMEDIATELY. NOTIFY THE LANDSCAPE ARCHITECT SO THAT ADJUSTMENTS MAY BE MADE PRIOR TO PROCEEDING.

5. PROVIDE POSITIVE DRAINAGE AT ALL TIMES. DO NOT OBSTRUCT THE NATURAL OR ENGINEERED DRAINAGE FLOW PATTERN. NOTIFY THE LANDSCAPE ARCHITECT OF ANY DRAINAGE CONDITIONS.

6. TOPSOIL PLANT DEPTH FOR ALL BEDS TO BE 18" TOPSOIL DEPTH FOR SEEDED LAWN TYPE 'A' AREAS TO BE 9" TOPSOIL DEPTH FOR SEEDED LAWN TYPE 'B' AREAS TO BE 6".

7. IMPORTED TOPSOIL FOR THIS PROJECT SHALL BE FINE LOAM OR SANDY LOAM, COMPRISED OF BETWEEN 20% SILT, 10% AND 15% CLAY, AND 65% SAND. THE pH LEVEL SHALL BE BETWEEN 6.0 AND 7.2 AND THE ORGANIC MATTER CONTENT SHALL BE BETWEEN 3% AND 6%. TOPSOIL SHALL BE FREE OF SEEDS, STONES, STICKS, CLUMPS, ROOTS, LEAVES AND DEBRIS, AND SHALL NOT BE DELIVERED WHILE IN A FROZEN OR EXCESSIVELY WET CONDITION. CONTRACTOR SHALL PROVIDE A RECENT SOIL TEST ANALYSIS FOR ALL SOIL TO BE USED. THE ANALYSIS MUST INCLUDE THE INFORMATION REQUIRED ABOVE. IF TESTED TOPSOIL REQUIRES AMENDMENTS TO MEET THE REQUIREMENTS OF THIS SPECIFICATION, THE CONTRACTOR SHALL RE-TEST AND RESUBMIT THE ANALYSIS FOR APPROVAL PRIOR TO BRINGING THE TOPSOIL ON SITE.

8. SOO SHALL BE A TALL FESCUE BLEND SOO, SUCH AS BLACK BEAUTY OR APPROVED EQUIV. BLUEGRASS BLEND SOO IS NOT ACCEPTABLE, UNLESS APPROVED BY THE LANDSCAPE ARCHITECT.

9. SEEDED LAWN TYPE 'A' SHALL BE IODT CLASS 2E NAL TOLERANT ROADSIDE GRASS SEED MIXTURE. INSTALL AT A RATE OF 100 LBS PER ACRE. SEEDING SHALL BE INSTALLED BETWEEN MAY 1 AND JUNE 15, OR BETWEEN AUGUST 15 AND OCTOBER 15.

10. SEEDED LAWN TYPE 'B' SHALL BE IODT CLASS 1B LOW MAINTENANCE GRASS SEED MIXTURE. INSTALL AT A RATE OF 200 LBS PER ACRE. SEEDING SHALL BE INSTALLED BETWEEN MAY 1 AND JUNE 15, OR BETWEEN AUGUST 15 AND OCTOBER 15.

11. TYPE 'A' AND 'B' SEEDED GRASS AREAS ARE TO BE COVERED WITH A HYDROMULCH WITH TACKIFIER. DETENTION SEEDING AREAS AND AREAS TO BE COVERED WITH AN EROSION CONTROL BLANKET.

12. WETLAND PLUGS IN THE BOTTOM OF DETENTION AREA ARE TO BE INSTALLED IN A 10' WIDE BAND (4 ROWS) BELOW THE NWL, AROUND THE EDGE OF THE POND. PLUGS ARE TO BE 5" BY 2" AND ARE TO BE PLANTED 24" APART. THERE ARE 1,620 TOTAL PLUGS. THEY ARE TO CONSIST OF 40% VIRGINIA WILD RYE (ELYMUS VIRGINICUS), 40% SOD GRASS (Panicum capillare), 10% BLUEGRASS (POA COCCINEA), 5% CRYSTAL BLUEGRASS (POA COCCINEA), 5% CORDGRASS (SPARTINA PECTINATA). EVENLY DISPERSE THE DIFFERENT PLUG VARIETIES ACROSS THE AREA TO PROVIDE A MIXED EFFECT. PROTECT THE PLUGS FROM TRAFFIC. INSTALL PER THE METHODS AND WITHIN THE DATES RECOMMENDED BY THE NURSERY PROVIDER.

13. WETLAND PLUGS TO MEET THE REQUIREMENTS OF THE KANE COUNTY STORMWATER MANAGEMENT ORDINANCE, SECTION 9-109. SEE SHEET L-102. THE CONTRACTOR MUST FAMILIARIZE THEMSELVES WITH THE REQUIREMENTS, SPECIFICATIONS AND ORDINANCE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROPOSING VEGETATED AREAS, BE MAINTAINED AND MONITORED FOR THREE YEARS, AND FULFILL THE REQUIREMENTS OF ALL REGULATORY AGENCIES.

14. ALL TREES AND SHRUBS MUST BE SPECIMEN QUALITY. TRUE SPECIES AND VARIETY, WITH FULL DENSE AND HEALTHY FORMS. PLANTS THAT ARE NOT SPECIMEN QUALITY WILL NOT BE ACCEPTED. ALL PLANTS MUST HAVE BEEN GROWN AND BEING GROWN AND INSPECTED BY THE NURSERY. LOCATED WITHIN 50 MILES OF THE PROJECT SITE. PROVIDE STATE CERTIFICATION FOR EACH NURSERY USED.

15. ALL TREES SHALL BE INSPECTED BY THE LANDSCAPE ARCHITECT, AT THE NURSERY, PRIOR TO DIGGING. TREES THAT ARE ACCEPTABLE WILL BE SEALED. ALL SEALS APPLIED BY THE LANDSCAPE ARCHITECT MUST REMAIN ON THE TREE UNTIL THE TREE IS GROWN AND INSPECTED BY THE LANDSCAPE ARCHITECT. ALL TREES MUST HAVE A GUARANTEE OR WARRANTY OR BE AUTOMATICALLY REJECTED. HEEL/DIN IN TREES WILL NOT BE ACCEPTED WITHOUT PRIOR APPROVAL FROM THE LANDSCAPE ARCHITECT.

16. ALL SHRUBS MUST MEET THE MINIMUM SIZES GIVEN IN THE PLANT LIST AND BE SPECIMEN QUALITY. SHRUBS THAT DO NOT MEET THESE GUIDELINES WILL BE REJECTED.

17. PLANT MATERIAL SHALL ONLY BE INSTALLED WITHIN THE FOLLOWING DATES: APRIL 15 THROUGH NOVEMBER 15.

18. TEST TREE PITS AND PLANT BEDS FOR ADEQUATE DRAINAGE. FILL PLANT PIT WITH 12" OF WATER. IF WATER LEVEL DOES NOT DROP A MINIMUM OF ONE INCH PER HOUR, NOTIFY THE LANDSCAPE ARCHITECT OF DRAINAGE ISSUES.

19. PLANTING MIX FOR ALL PLANTINGS SHALL BE A BLEND OF 90% APPROVED TOPSOIL AND 10% SOIL AMENDMENT. ACCEPTABLE AMENDMENTS INCLUDE ONE PLY SLIP CONDITIONER OR PM5 PLANTER MIX FROM MIDWEST TOPSOILS. APPROVED AMENDMENTS MUST BE BLENDED WITH TOPSOIL COMPONENTS SHALL BE COMPLETED OUTSIDE OF THE PLANTING PITS. PRIOR TO PLANTING.

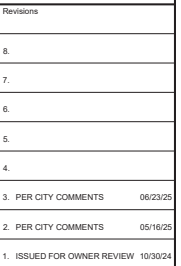
20. ALL PLANT BEDS ARE TO BE WELL SHAPED 'SPADE CUT' EDGES, 3" DEPTH. IN LINES OR IN CURVES AS SHOWN ON THE DRAWINGS.

21. ALL PLANT MATERIAL, INCLUDING SEED SOO, SHALL BE MAINTAINED BY THE CONTRACTOR UNTIL ACCEPTANCE IS OBTAINED. MAINTENANCE INCLUDES WATERING, WEEDING, PRUNING, STRAIGHTENING, SPRAYING, MULCH REPLACEMENT AND MOWING.

22. THE CONTRACTOR SHALL REQUEST AN INSPECTION OF LANDSCAPE PLANTING UPON SUBSTANTIAL COMPLETION FOR 'ACCEPTANCE'. REQUEST MUST BE SUBMITTED TO LANDSCAPE ARCHITECT AT LEAST TEN DAYS PRIOR TO PLANTING INSPECTION. UPON INSPECTION, THE LANDSCAPE ARCHITECT SHALL PREPARE A PUNCH LIST OF UNACCEPTABLE OR OUTSTANDING ITEMS. AFTER THE ITEMS HAVE BEEN SATISFACTORILY COMPLETED AND REVIEWED, THE LANDSCAPE ARCHITECT WILL RECOMMEND 'ACCEPTANCE'. SOO AND SEED SHALL BE REVIEWED AND ACCEPTED. MULCH, AND OTHER ITEMS THAT HAVE BEEN REVIEWED AND ACCEPTED.

23. ALL PLANT MATERIAL SHALL BE GUARANTEED FOR A PERIOD OF ONE YEAR FROM DATE OF 'ACCEPTANCE'. AT OR NEAR THE END OF THE GUARANTEE PERIOD, THE CONTRACTOR SHALL REQUEST AN INSPECTION OF LANDSCAPE PLANTING FOR 'FINAL ACCEPTANCE'. REQUEST MUST BE SUBMITTED TO LANDSCAPE ARCHITECT AT LEAST TEN DAYS PRIOR TO AN INSPECTION. UPON INSPECTION, THE LANDSCAPE ARCHITECT SHALL PREPARE A PUNCH LIST OF UNACCEPTABLE ITEMS. AFTER THE ITEMS HAVE BEEN SATISFACTORILY REPLACED OR REPAIRED, REVIEWED AND APPROVED, THE LANDSCAPE ARCHITECT WILL RECOMMEND 'FINAL ACCEPTANCE'.

24. DETENTION AREA SHALL BE MAINTAINED FOR A PERIOD OF THREE YEARS. SEE SHEET L-102 FOR MAINTENANCE PLAN.



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Date 10/30/2024	Project No. DWP 24-159
Scale 1" = 40'-0"	Sheet No.  <b>L-101</b>
Drawn By FERGUSON	
Approved	

