Please practice social distancing while attending this meeting. If social distancing is not possible please wear a mask.

AGENDA CITY OF ST. CHARLES PLANNING & DEVELOPMENT COMMITTEE ALD. RITA PAYLEITNER – CHAIR MONDAY, OCTOBER 12, 2020 - 7:00 PM CITY COUNCIL CHAMBERS 2 E. MAIN STREET

- 1. CALL TO ORDER
- 2. ROLL CALL
- 3. OMNIBUS VOTE

Items with an asterisk (*) are considered to be routine matters and will be enacted by one motion. There will be no separate discussion on these items unless a council member/citizen so requests, in which event the item will be removed from the consent agenda and considered in normal sequence on the agenda.

4. COMMUNITY & ECONOMIC DEVELOPMENT

- a. Presentation of a Concept Plan for Oliver-Hoffmann Resubdivision.
- b. Presentation of a Concept Plan for 1023 W Main St.
- c. St. Charles Initiative Update and Request to Approve Concept Design and Services Agreement with Serena Sturm.
- d. Plan Commission recommendation to approve a Zoning Map Amendment, Special Use for Planned Unit Development and PUD Preliminary Plan for Munhall Glen.
- e. Recommendation regarding 2021 Inclusionary Housing Fee.
- *f. Historic Preservation Commission recommendation approve a Landmark Nomination 216 Cedar Ave.
- *g. Historic Preservation Commission recommendation approve a Landmark Nomination for 316 Cedar St.
- *h. Historic Preservation Commission recommendation to approve a Façade Improvement Grant Agreement for 13 S. 2nd St.

5. ADDITIONAL BUSINESS

6. EXECUTIVE SESSION

• Personnel –5 ILCS 120/2(c)(1)

- Pending Litigation 5 ILCS 120/2(c)(11)
- Probable or Imminent Litigation 5 ILCS 120/2(c)(11)
- Property Acquisition 5 ILCS 120/2(c)(5)
- Collective Bargaining 5 ILCS 120/2(c)(2)
- Review of Executive Session Minutes 5 ILCS 120/2(c)(21)

7. ADDITIONAL ITEMS FROM MAYOR, COUNCIL, STAFF OR CITIZENS.

8. ADJOURNMENT

ADA Compliance

Any individual with a disability requesting a reasonable accommodation in order to participate in a public meeting should contact the ADA Coordinator, Jennifer McMahon, at least 48 hours in advance of the scheduled meeting. The ADA Coordinator can be reached in person at 2 East Main Street, St. Charles, IL, via telephone at (630) 377 4446 or 800 526 0844 (TDD), or via e-mail at jmcmahon@stcharlesil.gov. Every effort will be made to allow for meeting participation. Notices of this meeting were posted consistent with the requirements of 5 ILCS 120/1 et seq. (Open Meetings Act).

A	AGENDA	AGENDA ITEM EXECUTIVE SUMMARY Agenda Item Number: 4a				ber: 4a
CITY OF	Title:	Presentation of a Concept Plan for Oliver Hoffmann Resubdivision			1	
ST. CHARLES ILLINOIS • 1834	Presenter:	: Russell Colby				
Meeting: Planning & Development C			ommittee Date	: Octob	er 12, 2020	
Proposed Cost: \$	•		Budgeted Amount: \$		Not Budgeted	: 🗆

Background:

The subject property, known as the Oliver-Hoffmann Property, is comprised of five parcels totaling 28.54 acres northeast of Charlestowne Mall and south of Foxfield Rd. The property is currently vacant/agriculture.

Joe Segobiano of STCPR Consulting Inc. is seeking feedback on a Concept Plan to develop single-family homes and townhomes on the property. Details of the proposal are as follows:

- 37 single-family home lots with a range of lot sizes
- 23 attached single family buildings (92 Townhomes)
- Variety of home models.
- Access via previously dedicated collector street with connections to Foxfield Dr. and Smith Rd.
- Stormwater detention area at the northeast corner.

The Land Use Plan adopted as part of the 2013 Comprehensive Plan identifies the subject property as "Corridor/Regional Commercial" (west portion) and "Multi-Family Residential" (east portion). The property is also part of the East Gateway subarea plan.

Consent Decree

The property is subject to a 2009 Consent Decree settlement agreement between the City and property owner. The alignment of the collector street and the current lot layout and zoning districts reflect the terms of this agreement. The Concept Plan is being reviewed to determine if the City desires to modify the Consent Decree in order to accommodate the project. The potential process to modify, terminate or replace the Consent Decree has not yet been defined and will require further consideration by the City Attorney and the property owner.

Plan Commission Review

Plan Commission reviewed the Concept Plan on 9/22/20. Comments are summarized as follows:

- General support of overall concept and density.
- Provide buffer adjacent to Charlestowne Mall.
- Concerns over prominence of garages on the front of homes.
- Provide more social spaces/open space for residents.
- Determine solutions to utilize the excess portion of Lot 7.
- Proposed alley/shared drive access could be a connection to the Mall property.
- Consider whether the collector street could be realigned to improve the plan.
- Traffic study needs to consider traffic control at the Foxfield Dr. and King Edward/Charter One Ave. intersection.

Attachments (please list):

Concept Plan Application, Plans, Correspondence, 2009 Consent Decree

Recommendation/Suggested Action (briefly explain):

Provide comments - Staff is recommending the Committee provide comments on the following topics:

- Proposed land use and compatibility with surrounding development
- Site layout and access to adjacent properties
- Proposed building architecture
- Is a PUD appropriate or desirable for the project?
- Is there interest to modify the Consent Decree to accommodate this project?

Community & Economic Development

Phone: (630) 377-4443



Staff Report

TO: Chair Rita Payleitner

And Members of the Planning & Development Committee

FROM: Rachel Hitzemann, Planner

Russell Colby, Assistant Director of Community & Economic Development

RE: Oliver Hoffman Resubdivision Concept Plan

DATE: October 6, 2020

I. APPLICATION INFORMATION:

Project Name: Oliver Hoffman Resubdivision

Applicant: Joe Segobiano, STCPR Consulting Inc.

Purpose: Obtain feedback on a Concept Plan for residential development

General Information:

Site Information		
Location	Northeast of Charlestowne Mall of Bricher Rd., South of Foxfield Dr.	
Acres	28.54 acres	

Application:	Concept Plan
Applicable	Ch. 17.12 – Residential Districts
City Code	Ch. 17.06 - Design Review
Sections	Ch. 17.26 – Landscaping & Screening

Existing Conditions			
Land Use	Vacant/Agriculture		
Zoning	BC- Commercial Business and RM-3- General Residential District		

	Zoning Summary			
North	RM-2- Medium Density Multi-Family Residential	Multi- Family and Single Family		
	and RS-4- Suburban Single Family Residential	Residential		
East	Unincorporated Kane County and OR- Office	Vacant Agricultural, Office and		
	Research	Bank		
South	BR- Regional Business and Charlestowne Mall PUD	Charlestowne Mall		
West	BR- Regional Business, Charlestowne Mall and RS-	Charlestowne Mall and Single		
	4	Family Residential		

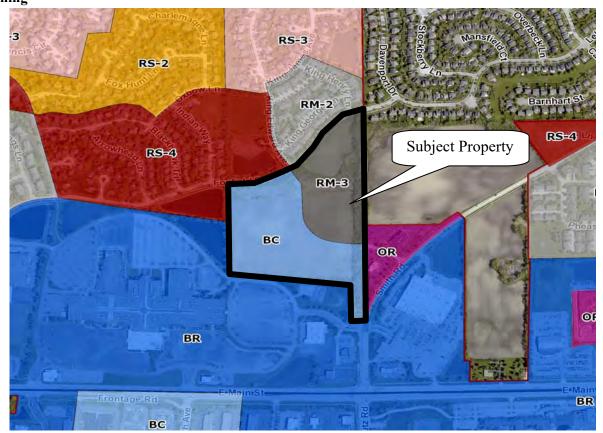
Comprehensive	Plan	Designation
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Parks/ Open Space, Single Family Attached Residential, Corridor/ Regional Commercial, Multi-Family Residential

Aerial



Zoning



II. OVERVIEW

A. BACKGROUND

The subject property is a 28.54-acre site located to the Northeast of Charlestowne Mall and to the South of Foxfield Dr. The property is currently a vacant agricultural site that is split into 5 separate lots. The south and west portion of the property is zoned BC and the north and east portion is zoned RM-3.

The property historically had been known as the "Oliver Hoffmann" property, which is the name of the development company that has owned the site since its annexation into the City in 1991.

While the site has remained undeveloped, a number of proposals were presented to the City for consideration over the years.

In order to improve traffic circulation and connectivity to the residential neighborhoods to the north, the City has identified a need for a collector street connection between Smith Road and Foxfield Drive, which would traverse the property. A stub portion of this street (now known as King Edward Ave.) was constructed from Smith Road north to facilitate this connection.

B. CONSENT DECREE

In 2006, a proposal was submitted for development of a Walmart Supercenter on the property, which would have limited the potential for the planned collector street connection.

In 2007, the City filed a Complaint of Condemnation to acquire through eminent domain a strip of property for completion of the collector street. A settlement was reach between the City and property owner in 2009, resulting in a Consent Decree document.

The Consent Decree defines development parameters that the City and property owner agreed to follow, including a collector street route, subdivision lot layout and zoning classifications. The current lot layout and zoning districts reflect the terms of the Consent Decree.

The Consent Decree further requires that any future development proposal be submitted as a Planned Unit Development (PUD) and the project include construction of the collector street.

C. PROPOSAL

Joe Segobiano, on behalf of STCPR Consulting Inc. is seeking feedback on a Concept Plan to develop 37 single family detached homes and 92 townhomes.

Details of the proposal are as follows:

- Resubdivide the 5 parcels into single family lots and townhomes.
- Provide 37 single family homes and 92 Townhomes
- Rezone the entire property to RM-2 or RM-3
- Previously platted collector street (shown as Charter One Ave. but to be known as King Edward Ave.) to be constructed along the current 80 ft. right of way.
- A stormwater management facility in the northeast corner of the property.
- Sidewalks provided within the development.

D. CONCEPT PLAN REVIEW PROCESS

The purpose of the Concept Plan review is to enable the applicant to obtain informal input on a concept prior to spending considerable time and expense in the preparation of detailed plans and architectural drawings. The Concept Plan process also serves as a forum for citizens and owners of neighboring property to ask questions and express their concerns and views regarding the potential development. Following the conclusion of the Concept Plan review, the developer can decide whether to formally pursue the project.

This Concept Plan is additionally being reviewed to determine if the City desires to modify the Consent Decree in order to accommodate the project. The potential process to modify, terminate or replace the Consent Decree has not yet been defined and will require further consideration by the City Attorney and the property owner.

III. ANALYSIS

A. COMPREHENSIVE PLAN

The Land Use Plan adopted as part of the 2013 Comprehensive Plan identifies the subject property as "Corridor/ Regional Commercial" (west portion) and "Multi-Family Residential" (east portion).

Corridor/Regional Business land use is described as follows:

"Areas designated as corridor/regional commercial are intended to accommodates larger shopping centers and developments that serve a more regional function, drawing on customer base that extends beyond City limits. These areas often have a mix of "big box" stores, national retailers, and a "critical mass" of multiple stores and large shared parking areas. Areas designated for corridor/regional commercial are located primarily in larger consolidated areas along the City's heavily traveled corridors and intersections. Commercial service uses can also have an appropriate place in corridor/regional commercial areas, but must be compatible with adjacent and nearby retail and commercial shopping areas and be located as to not occupy prime retail locations." (pg. 39)

Multi-Family Residential land use is described as follows:

"Multi-family residential structures contain multiple housing units, are usually stacked vertically and attached horizontally, and typically have common hallways and other amenities. Examples of multi-family residential developments include apartments, condominiums, and senior housing. Most multifamily developments are located in or near areas of intense commercial development with access to goods, services, and the transportation network. Because of market pressures, many single-family residences Downtown have been converted to multi-family. In addition to areas designated MultiFamily Residential, the land use plan also provides for multifamily units within the Mixed-Use land use designation." (p.38)

Chapter 4 of the Comprehensive Plan provides the following Residential and Mixed-Use land use policies relevant to the proposed development: (p. 43-44):

Prioritize infill development over annexation and development. While the era of substantial residential growth is over in St. Charles, there remain some isolated

opportunities for residential development on the City's west side. While most of these opportunities are within unincorporated Kane County, they fall within the City's 1.5-mile extraterritorial planning jurisdiction defined by State statute. It is recommended that the City carefully consider annexation and growth into these areas while vacant and/or underutilized residential properties exist within the City's boundaries. When residential development does occur within the City's growth areas, it should occur in areas immediately adjacent to existing developed areas so as to prevent "leap frog" development and the resulting costs and burdens of unnecessarily extending infrastructure systems in an unwise manner.

Locate new multi-family residential developments in appropriate locations within the City and consider the implications of concentrating units in one location or area of the City. In addition to assisting with the community's goals to provide affordable housing in the community, multi-family housing contributes to residential density which can improve the viability of shopping areas in the community. Recognizing that this Plan is dynamic and not "set in stone", the City should promote multi-family housing in areas identified in the Land Use and Residential Areas Plans, but consider proposals in other areas provided any significant impact on schools, traffic, and other infrastructure can be mitigated."

Ensure residential areas are adequately screened/ buffered from adjacent non-residential uses and activity. The composition of the City's commercial districts along corridors that transect the City means that there are many areas where commercial uses abut residential properties and neighborhoods. The use of horizontal and vertical buffering and screening, including berms, fencing, and landscaping, should be promoted to protect neighborhoods from abutting commercial or industrial land uses. The City should identify areas where land use conflicts are problematic and explore solutions to mitigate the conflicts, including buffering and screening. Additionally, the City's landscape ordinance could be revised to require enhanced screening and an amortization schedule to ensure compliance for non-conforming properties within a set time frame.

Chapter 8- Sub Area Plans

The Subject Property is located within the East Gateway Subarea. Goals and Objectives (p. 102) are listed below:

Subarea Goals

The East Gateway subarea represents a unique opportunity for economic development, revitalization and stabilization with for a specific context within the City of St. Charles. The overall vision for the subarea includes the following:

- Revitalization of the Subarea's retail areas that maximizes the locational assets within this area of the City.
- <u>Improved connectivity and circulation within the Subarea providing logical and efficient connections between compatible uses.</u>
- Better separation of incompatible land uses to protect residential neighborhoods while at the same time help define the City's business areas.
- Attractive streets and sites to distinguish this Subarea and key corridors from neighboring communities.
- A mix of uses that that help diversify the City's economy and provide places to live, work, and shop.

Subarea Objectives

- Improve the appearance of the Kirk Road and Main Street Corridors to assist in strengthening the community's identity and appearance through installation of streetscaping, wayfinding and gateway elements.
- Use landscaping appropriately to enhance commercial areas, screen unsightly areas, an provide an attractive streetscape and overall setting for the area.
- Improve the overall connectivity and mobility within the Subarea through both public streets and internal connection to provide a predictable and navigable environment.
- <u>Preserve surrounding neighborhoods through the use of screening, buffering, and better separation from commercial development.</u>
- Create market-responsive development parcels that can accommodate projects of an appropriate scale and phasing over time.
- Take advantage of proximity to DuPage Airport and Pheasant Run as activity generators.
- Reposition the Charlestowne Mall site to foster its renaissance or its redevelopment.
- Enhance the character of both existing and new development through site improvements, façade enhancements, consistent signage regulation, and attractive building design and materials

East Gateway Sub Area Plan Catalyst Sites (p. 104)

A portion of the Subject Property is identified as Catalyst Site "D".

The Oliver Hoffman Resubdivision Concept Plan site area is shown in the yellow dashed box.

• Site D: Constructed in 1991, the Charlestowne Mall has been well maintained and is in good physical



condition, however a lack of a critical mass of retailers and a high volume of vacancy have placed the Charlestowne Mall in jeopardy. Once a shopping destination within the community and surrounding area, most retailers have left the interior of the mall. Von Maur, Classic Cinemas, Carson Pirie Scott and Kohls occupy four of the mall's five anchor spaces and are complemented by a handful of smaller retailers and services. Internal hall - ways are desolate, parking fields are vast and empty and the Charlestowne Mall needs intervention to reposition the site to improve the mall's future viability or its full-scale redevelopment.

Charlestowne Mall Framework Plan (p. 105)

The Charlestowne Mall site represents the single greatest opportunity to redefine the character and function of the East Gateway. This Framework Plan highlights recommendations that could be implemented regardless of the timing or end vision for the repositioning of the mall structure itself. Within this framework, specific repositioning alternatives can be considered as mall tenancy, local market conditions, and other factors play out over time.

Proposed Street or Circulator. These include public streets and on-site circulators designed to enhance on-site access and mitigate the impacts of traffic on surrounding neighborhoods. The key recommendation is a new street that would run along the north side of the mall property and make Foxfield Drive a residential street.

Out Lot Commercial, Retail, and Office Development.

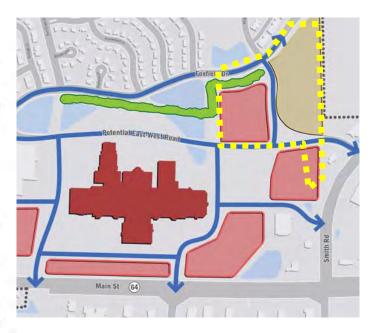
This includes development sites located along Main Street that could capitalize on high visibility and more prominent access point offered by the proposed grid of streets and circulators. This also includes development sites located to the rear of the mall property with less visibility but enhanced access from the proposed grid of streets and circulators.

Single Family Attached/Multi-Family Residential.

Attached single family or multi-family development is permitted in this area and would complement existing housing development. It would also increase the number of residents that could support the mall site and other commercial properties in the eastern portion of the City.

Natural Buffer/Screening. Screening and buffering should be provided between commercial and residential uses. This will minimize the impacts of non-residential uses on existing and future neighborhoods.

Repositioned Charlestowne Mall. Within the proposed framework of streets and peripheral uses, the mall structure could be repositioned to be more responsive to contemporary consumer needs. The following page illustrates some proposed alternative approaches.



B. ZONING REVIEW

The subject property is zoned BR- Regional Business and RM-3- General Residential District. The applicant has proposed rezoning the entire property to RM-3 or RM-2- Medium Density Multi-Family. Staff suggests rezoning the property to RM-2 and creating a PUD to deviate from the required bulk standards where necessary. While RM-3 zoning is similar to RM-2 zoning, RM-3 allows for more intensive multi-family use, which is not being proposed as part of this development. Therefore, staff has concluded that RM-2 would be a more appropriate zoning.

The purpose of the RM-2 District as stated in the Zoning Ordinance is:

"To accommodate a range of housing densities and a variety of housing types and styles, with a maximum density of approximately ten units per acre." RM-2 zoning is consistent with the adjacent residential neighborhood to the north of the subject property. This subdivision is known as Kingswood.

The applicant is proposing the following bulk standards for the property. The table below compares the RM-2 and RM-3 District requirements with the Concept plan. Zoning deviations that would be required are denoted in *bold italics*.

	RM-2	RM-3	Concept Plan
Min. Lot Area	SF: 5,000 sf TH: 4,300 sf/du	SF: 5,000 sf TH: 4,300 sf/du	SF: 6,760sf TH:5,473 sf/du
Min. Lot Width	SF: 50 ft. TH: 24 ft/du	SF: 50 ft. TH: 24 ft/du	SF: 52 ft. TH: 30 ft.
Max. Building Coverage	35 %	40%	SF: 37% TH: 39%
Max. Building Height	35 ft or 3 stories, whichever is less	35 ft or 3 stories, whichever is less	SF: 2 stories TH: 3 stories
Min. Front Yard	30 ft., 20ft. when adjoining a local street	30 ft.	20 ft.
Min. Exterior Side Yard	Abutting an arterial or collector street: 30 ft. Abutting local street: 20ft.	30 ft.	Abutting an arterial or collector street: 30 ft. Abutting local street: 20ft.
Min. Interior Side Yard	SF: combined width 14ft, not less than 5 ft each side TH: 10 ft. each side	SF: combined width 14ft, not less than 5 ft each side TH: 10 ft. each side	SF: combined width 14ft, not less than 5 ft each side TH: 10 ft. each side
Min. Rear Yard	25 ft.	30 ft.	25 ft.

C. LANDSCAPING

A landscape plan will be required for any common areas. This includes the detention ponds and surrounding open space.

A landscape buffer is not required for the RM-2 district. However, the property is adjacent to OR Office-Research and BR Regional Business zoning on the southwest and southeast portion of the property. These adjacent properties would have been required to provide a landscape buffer if developed after the residential use.

Consideration should be given to trees or other buffering in the rear of the adjacent lots which will help to provide a minimal buffer between adjacent non-residential uses. Privacy fencing should also be considered for additional screening.

D. BUILDING ARCHITECTURE

The applicant has submitted a number of home model designs intended to be offered for the proposed development for the Plan Commission's information. In RM districts, townhomes will be subjected to Design Review standards, and architectural plan would need to be approved with a PUD. Single family homes are not subjected to Design Review and architectural plans are not required to be approved as part of a single-family PUD.

E. <u>SITE ACCESS/STREET IMPROVEMENTS</u>

Access to the site is provided by the construction of a collector street to be known as an extension of "King Edward Ave." (shown on the plans as Charter One Ave.). The collector street will connect Foxfield Dr. and Smith Rd. This street has already been platted as part of a previous subdivision.

Individual lots and residential units will be accessed from secondary streets within the development. The applicant has requested variances for the following items in regards to these streets:

- Local street width to 31 ft. instead of 33 ft. (Would accommodate parking on one side of the street, instead of both sides of the street)
- A local street right-of-way width of 50 ft. adjacent to the attached single-family units and 60 ft. feet adjacent to the detached single-family units as opposed to the typical 66ft. right-of way required by ordinance.
- Permit a horizontal centerline radius of 100 ft. instead of the typical 200 ft. required for local streets per ordinance.

A ROW of 66 ft. is typically required for single-family subdivisions of this size, however the proposed design may be acceptable with adequate front yard utility easements to accommodate utilities, as needed.

Consideration should be given to future vehicular and/or pedestrian connections to adjacent properties:

- Future connection to the Charlestowne Mall property could be provided at the southwest corner of the site. The 20 ft. "alley" could instead be a stubbed street connection.
- Future connection east to the Petkus property could be provided.

F. ENGINEERING REVIEW

Engineering comments on the Concept Plan have been provided to the developer. Items raised will need to be addressed in the Preliminary Plan submittal, should the development move forward.

- Stormwater detention volume will need to be verified at the time of Preliminary Engineering review.
- The wetlands and mitigation measures could impact the layout of streets and lots within the subdivision.
- Long-term maintenance and monitoring of wetlands should be considered and will need to be addressed in future submittals.
- The configuration of Lot 7 is unusual and undesirable to have a large odd shaped parcel under private ownership.
- Off-site sanitary sewer capacity for the project will need to be assessed.

G. INCLUSIONARY HOUSING

This development will be subject to the Inclusionary Housing Ordinance, Title 19 of the City Code. A fee worksheet has been submitted indicating the applicant's intent to pay a fee inlieu of providing 13 affordable units. Based on a fee in-lieu amount of \$39,665.75 per required affordable unit, a total fee in-lieu amount of \$511,688.17 would be due at the time of building permit. The fee is set on an annual basis by the City Council with input from the Housing Commission.

H. SCHOOL AND PARK FEE-IN-LIEU CONTRIBUTIONS

The applicant will be required to provide the School and Park Districts with a cash contribution in lieu of physical land per the standards established in the Subdivision Code, Chapter 16.10 "Dedications".

IV. FUTURE APPROVAL PROCESS

If the applicant chooses to move forward with the proposed development at the conclusion of the Concept Plan process, the following zoning/subdivision approvals would be necessary, assuming the project would require a PUD:

- 1. Map Amendment to rezone the BC property to an RM zoning district, or alternately rezoned the entire site RM-2.
- 2. Special Use for PUD: To establish a PUD ordinance with unique zoning use and standards to accommodate the project.
- 3. PUD Preliminary Plan: To approve the physical development of the property, including site, engineering, and landscape plans.
- 4. Final Plat of Subdivision: To re-plat and divide the property into building lots.

V. PLAN COMMISSION REVIEW

Plan Commission reviewed the Concept Plan on 9/22/20. Comments are summarized as follows:

- General support of overall concept and density.
- Provide buffer adjacent to Charlestowne Mall.
- Concerns over prominence of garages on the front of homes.
- Provide more social spaces/open space for residents.
- Determine solutions to utilize the excess portion of Lot 7.
- Proposed alley/shared drive access could be a connection to the Mall property.
- Consider whether the collector street could be realigned to improve the plan.
- Traffic study to consider traffic control at the Foxfield Dr. and King Edward/Charter One Ave. intersection.

VI. SUGGESTED ACTION

Review the Concept Plan and provide comments to the applicant. Staff recommends the Commission provide feedback on the following:

- ✓ Proposed land use and compatibility with surrounding development
- ✓ Site layout and access to adjacent properties
- ✓ Proposed Building Design
- ✓ Is a PUD appropriate or desirable for this project? Does the plan adequately advance one or more of the purposes of the PUD procedure:
 - 1. To promote a creative approach to site improvements and building design that results in a distinctive, attractive development that has a strong sense of place, yet becomes an integral part of the community.
 - 2. To create places oriented to the pedestrian that promote physical activity and social interaction, including but not limited to walkable neighborhoods, usable open space and recreational facilities for the enjoyment of all.
 - 3. To encourage a harmonious mix of land uses and a variety of housing types and prices.
 - 4. To preserve native vegetation, topographic and geological features, and environmentally sensitive areas.
 - 5. To promote the economical development and efficient use of land, utilities, street improvements, drainage facilities, structures and other facilities.
 - 6. To encourage redevelopment of sites containing obsolete or inappropriate buildings or uses.
 - 7. To encourage a collaborative process among developers, neighboring property owners and residents, governmental bodies and the community
- ✓ Is there interest to modify the Consent Decree to accommodate this project?

VI. ATTACHMENTS

- Concept Plan Application; received 8/17/20
- Plans

CITY OF ST. CHARLES

TWO EAST MAIN STREET ST. CHARLES, ILLINOIS 60174-1984



COMMUNITY DEVELOPMENT DIVISION

PHONE: (630) 377-4443 EMAIL: cd@stcharlesil.gov

CONCEPT PLAN APPLICATION

CITYVIEW

Project Name:

Oliver-Hoffman Resubdivision 2020-PR-012

Project Number:

Cityview Project Number: PLCP202000042



To request review of a Concept Plan for a property, complete this application and submit it with all required attachments to the Planning Division.

When the application is complete and has been reviewed by City staff, we will schedule a Plan Commission review, as well as a review by the Planning and Development Committee of the City Council. While these are not formal public hearings, property owners within 250 ft. of the property are invited to attend and offer comments.

The information you provide must be complete and accurate. If you have a question please call the Planning Division and we will be happy to assist you.

1.	1. Property Information:	Location: SOUTH OF FOXFIELD DRIVE AT KING EDWARD AVENUE		
	Parcel Number (s): 09-25-245-002, 09-25-250-003, 09-25-250-004, 09-25-250-005, 09-25-250-006			
	Proposed Project Name:			
2. Applicant Information:	Name JOE SEGOBIANO	Phone 312-504-7705		
	Address STCPR CONSULTING INC.	Fax		
		4155 MEADOW VIEW DR ST. CHARLES, IL 60175	Email jsegobiano@lpsi.com	
3. Record Owner		Name WEST SUBURBAN BANK TR #2580	Phone	
	Information:	Address AMCO PROPERTIES	Fax	
		2863 9TH ST, UNIT 143-373 NAPERVILLE, IL 60564	Email	

Please check the type of application: **PUD Concept Plan:** Proposed Name: **OLIVER-HOFFMAN RESUBDIVISION** Subdivision Concept Plan Proposed Name: Other Concept Plan Zoning and Use Information: **RM-3 & BC** Current zoning of the property: NO Is the property a designated Landmark or in a Historic District? **VACANT - AGRICULTURE** Current use of the property: **RM-3** Proposed zoning of the property: PUD? DETACHED AND ATTACHED SINGLE-FAMILY RESIDENTIAL Proposed use of the property: MULTI-FAMILY RESIDENTIAL & CORRIDOR/REGIONAL COMMERCIAL

Attachment Checklist

Comprehensive Plan Designation:

□ REIMBURSEMENT OF FEES AGREEMENT:

An original, executed Reimbursement of Fees Agreement and deposit of funds in escrow with the City, as provided by Appendix B of the Zoning Ordinance.

□ REIMBURSEMENT OF FEES INITIAL DEPOSIT:

Deposit of funds in escrow with the City. Required deposit is based on review items (number of applications filed) and the size of the site:

Number of Review Items	Under 5 Acres	5-15 Acres	16-75 Acres	Over 75 Acres
1	\$1,000	\$2,000	\$3,000	\$4,000
2 or 3	\$2,000	\$4,000	\$5,000	\$7,000
4 or more	\$3,000	\$5,000	\$7,000	\$10,000

□ PROOF OF OWNERSHIP and DISCLOSURE:

- a) a current title policy report; or
- b) a deed and a current title search.

If the owner is not the applicant, an original letter of authorization from the owner permitting the applicant to act on his/her behalf is required. If the owner or applicant is a Trust, a disclosure of all beneficiaries; if the owner or applicant is a Partnership, a disclosure of all partners; if the owner or applicant is a Corporation, a disclosure of all owners with an interest of at least ten percent (10%).

NOTE: Private covenants and deed restrictions can limit private property rights with respect to the use of land even though the City's Zoning Ordinance may authorize the use or a less restrictive use. We strongly advise that

you perform a title search on the property to determine if there any private covenants containing use restrictions or other deed restrictions. As those private covenants and deed restrictions may conflict with the City's Zoning Ordinance, it is further recommended that you consult with an attorney to obtain an opinion with respect to whether your intended use is compatible with those restrictions.

□ LEGAL DESCRIPTION: For entire subject property, on 8 ½ x 11 inch paper

□ PLAT OF SURVEY:

A current plat of survey for the Subject Realty showing all existing improvements on the property, prepared by a registered Illinois Professional Land Surveyor.

□ AERIAL PHOTOGRAPH:

Aerial photograph of the site and surrounding property at a scale of not less than 1"=400', preferably at the same scale as the concept plan.

D PLANS:

All required plans shall be drawn on sheets no larger than 24" x 36", unless the Director of Community Development permits a larger size when necessary to show a more comprehensive view of the project. All required plans shall show north arrow and scale, and shall be drawn at the same scale (except that a different scale may be used to show details or specific features). All plans shall include the name of the project, developer or owner of site, person or firm preparing the plan, and the date of plan preparation and all revisions. A pdf document file or files of all plans shall be required with each submittal. The number of paper plans required shall be as determined by the Director of Community Development, based upon the number of copies needed for review.

Copies of Plans:

Initial Submittal - Ten (10) full size copies for non-residential projects OR Twelve (12) full size copies for residential projects; Three (3) 11" by 17"; and a PDF electronic file (On a CD-ROM or may be emailed to the Project Manager). For subsequent submittals, please contact the Project Manager to determine how many copies are required.

Concept Plans shall show:

1. Existing Features:

- Name of project, north arrow, scale, date
- · Boundaries of property with approximate dimensions and acreage
- Existing streets on and adjacent to the tract
- Natural features including topography, high and low points, wooded areas, wetlands, other vegetative cover, streams, and drainage ways.
- General utility locations or brief explanation providing information on existing sanitary sewer, storm sewer, water, and other utilities necessary to service the development.

2. Proposed Features:

- · Name of project, north arrow, scale, date
- · Boundaries of property with approximate dimensions and acreage
- Site plan showing proposed buildings, pedestrian and vehicular circulation, proposed overall land use pattern, open space, parking, and other major features.
- Architectural elevations showing building design, color and materials (if available)
- General utility locations or brief explanation providing information on existing sanitary sewer, storm sewer, water, and other utilities necessary to service the development

SUMMARY OF DEVELOPMENT:

Written information including:

- List of the proposed types and quantities of land use, number and types of residential units, building
 coverage, floor area for nonresidential uses and height of proposed buildings, in feet and number of
 stories.
- Statement of the planning objectives to be achieved and public purposes to be served by the development, including the rationale behind the assumptions and choices of the applicant
- · List of anticipated exceptions or departures from zoning and subdivision requirements, if any

□ PARK AND SCHOOL LAND/CASH WORKSHEETS

For residential developments, Park and School land/cash worksheets in accordance with Title 16 of the St. Charles Municipal Code with population projections establishing anticipated population and student yields.

- □ INCLUSIONARY HOUSING SUMMARY: For residential developments, submit information describing how the development will comply with the requirements of Title 19, "Inclusionary Housing" of the St. Charles Municipal Code.
- LIST OF PROPERTY OWNERS WITHIN 250 FT.

Fill out the attached form or submit on a separate sheet. The form or the list must be signed and notarized.

I (we) certify that this application and the documents submitted with it are true and correct to the best of my (our) knowledge and belief.

Record Owner Date

Applicant or Authorized Agent Date

□ SUMMARY OF DEVELOPMENT:

Written information including:

- List of the proposed types and quantities of land use, number and types of residential units, building
 coverage, floor area for nonresidential uses and height of proposed buildings, in feet and number of
 stories.
- Statement of the planning objectives to be achieved and public purposes to be served by the development, including the rationale behind the assumptions and choices of the applicant
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Fill out the attached form or submit on a separate sheet. The form or the list must be signed and notarized.

Date

I (we) certify that this application and the documents submitted with it are true and correct to the best of my (our) knowledge and belief.

WEST SUBURBAN BANK

NOT PERSONALLY BUT AS

TRUSTER VIT. NO. 2580

Record Owner

JRUST OFFICER

Date

Applicant or Authorized Agent

rHIS INSTRUMENT is executed by West Suburban Bank ("WSB") not personally or individually, but solely as trustee as aforesaid in the exercise of the power and authority conferred upon and vested in it as such trustee. All of the statements, warranties, and representations set forth herein are made solely on information and belief without any independent inquiry or investigation by WSB and should be construed accordingly. Notwithstanding any provision to the contrary set forth in this instrument, any recourse against WSB shall be limited to the assets comprising the trust estate and no personal liability shall be asserted or be enforceable against WSB by reason of the terms, promises, agreements, covenants, warranties, representations, indemnifications, or other matters herein set forth, all such ersonal liability of WSB being expressly waived.

OWNERSHIP DISCLOSURE FORM LAND TRUST

STATE OF ILLINOIS DUPAGE KANE COUNTY)) SS.)		
Trust Officer of	PAWLAK, being first duly West Suburban Bar e beneficiaries of Land Trust No.	nk	, and that the following
	Properties, UP, an II		
<u> </u>			
By: Christ	The Paulak Trust Officer		
Subscribed and Swo	orn before me this 2612	day of	
August	Jale Hang		"OFFICIAL SEAL" ANGELA J. YOUNG NOTARY PUBLIC, STATE OF ILLINOIS My Commission Expires 01/10/2021



August 26, 2020

Harry Leipsitz, Esq. Freeborn Peters 311 South Wacker Drive Suite 300 Chicago IL 60606

RE: West Suburban Bank Land Trust #2580

Concept Plan Application
Ownership Disclosure Form

Dear Mr. Leipsitz:

Per Ada Marie Aman, Amoco Properties, LP's Letter of Direction, enclosed are the signed and notarized above referenced documents.

If I may be of further assistance, please do not hesitate to contact me at 630-652-2225.

Sincerely,

Christine Pawlak Trust Officer /cp Enclosure



SUMMARY OF DEVELOPMENT

This development is the re-subdivision of the Oliver-Hoffman Subdivision into thirty-seven (37) detached single family lots and twenty-three (23) attached single-family units for a total of 129 dwelling units. The development will include the construction of roadways, utilities, and grading necessary to prepare the site for dwelling construction. The previously platted and dedicated Charter One Avenue will be constructed along the current 80-foot wide right-of-way from its stubbed location on the east side of the property to its intersection with Foxfield Drive and King Edward Drive. A stormwater management facility will be constructed at the northeast corner of the property to meet the stormwater management requirements of the City of St Charles.

The portion of the property located north of Charter One Avenue is currently zoned as RM-3. The portion of the property located south of Charter One Avenue is currently zoned as BC. The applicant is requesting the rezoning of the parcels to RM-2.

Variances from the City of St. Charles Engineering Design and Inspection Policy Manual and the City of St. Charles Zoning Ordinance are being requested for this development. The applicant is requesting a local street width of 31 feet instead of the typical 33 feet width per the ordinance. The applicant is requesting a local right-of-way width of 50 feet adjacent the attached single-family units and 60 feet adjacent the detached single-family units as opposed to the typical 66 feet wide right-of-way required per ordinance. The applicant is also requesting a variance to permit a horizontal centerline radius of 100 feet instead of the typical 200 feet required for local roadways per ordinance.

EXHIBIT A – LEGAL DESCRIPTION

LOTS 1, 2, 3, 4 AND 5 OF THE PLAT OF RESUBDIVISION OF OLIVER-HOFFMAN CHARLESTOWN SUBDIVISION BEING A RESUBDIVISION OF LOTS 1, 2 AND 3 OF THE OLIVER-HOFFMAN CHARLESTOWN DEVELOPMENT A SUBDIVISION OF THAT PART OF THE SOUTHEAST 1/4 OF SECTION 24, TOWNSHIP 40 NORTH, RANGE 8 EAST, AND PART OF THE NORTHEAST 1/4 OF SECTION 25, TOWNSHIP 40 NORTH, RANGE 8 EAST OF THE THIRD PRINCIPAL MERIDIAN, RECORDED JULY 20, 2010 AS DOCUMENT 2010K046244, IN KANE COUNTY, ILLINOIS.

INCLUSIONARY HOUSING REQUIREMENTS

Background:

St. Charles Municipal Code Title 19 "Inclusionary Housing", requires developers of new residential developments to provide a proportionate share of affordable housing units within the development, or to pay a fee in-lieu of providing affordable housing units. Developers may also provide a mix of affordable housing units and fee in-lieu.

Affordable housing is defined as housing that has a sales price or rental amount that is within the means of a household with income at or below 80% Area Median Income (AMI) for for-sale units and at or below 60% AMI for rental units, adjusted for household size. See Title 19 for complete definitions.

The maximum price of affordable for-sale units and affordable rental units required by Title 19 shall be no greater than the affordable purchase price and affordable rent established annually by the Illinois Housing Development Authority (IHDA). Contact the Planning Division for the most recent affordable prices from IHDA.

Development Cost Offsets:

Developments that provide affordable housing units may be eligible for the following:

- Density Bonus equivalent to one bonus unit for each affordable unit constructed, up to 120% of base density.
- Waiver of building permit, demolition, plan review, and sewer/water connection fees for the required affordable units.
- Waiver of School and Park cash contributions (when required in lieu of park and land dedications) for the required affordable units.

Submission Requirements:

Submit information describing how the residential development will comply with the requirements of Title 19, "Inclusionary Housing". <u>Use the Inclusionary Housing Worksheet</u> to calculate the number of required affordable units to be incorporated within the residential development, or the required fee in-lieu payment.

If the development will include affordable units, submit the following additional information:

- The number of market-rate and affordable for-sale and rental units to be constructed, including type of dwelling, number of bedrooms per unit, proposed pricing, and construction schedule, including anticipated timing of issuance of building permits and occupancy certificates.
- Documentation and plans regarding locations of affordable units and market-rate units, and their exterior appearance, materials, and finishes.

City Council Determination:

City Council will review the proposal to comply with the requirements of Title 19 "Inclusionary Housing" and will determine whether affordable units, fee in-lieu, or a mix of affordable units and fee in-lieu will be accepted.

INCLUSIONARY HOUSING WORKSHEET

Name of Development Date Submitted:	OLIVER-HOFFMAN RESUBDIVISION
Prepared by:	



Use this worksheet to determine the affordable unit requirement for the proposed development and to propose how the development will meet the Inclusionary Housing requirements of Title 19.

Calculate the number of affordable units required:

Unit Count Range	# of Units Proposed in Development		% of Affordable Units Required		# of Affordable Units Required
1 to 15 Units		X	5%	=	
More than 15 Units	129	X	10%	=	12.9

How will the Inclusionary Housing requirement be met?

	Provid	le on-site affordable units				
X	Pay a	fee in-lieu of providing affordable units (calculate fee in-lieu below)				
	Provide a mixture of affordable units and fee in-lieu					
	0	# of affordable units to be provided:				
	0	Amount of fee in-lieu to be paid (calculate below):				

Fee In-Lieu Payment Calculation

# of Affordable	# of Affordable Units Proposed to Pay the Fee-In- Lieu		Fee-In-Lieu Amount		Total Fee-In-Lie	
Units Required			Per Unit		Amount	
12.9	12.9	X	\$39,665.75	=	\$511,688.17	

SCHOOL LAND/CASH WORKSHEET

City of St. Charles. Illinois

Name of Development Date Submitted: Prepared by:

DLIVER-HOFFMAN RESUBDIVISION	



A credit for existing residential lots within the proposed subdivision shall be granted. Deduct one (1) unit per existing lot.

Total Dwelling Units w/ Deduction: 129

Estimated Student Yield by Grades:

Type of Dwelling	# of dwelling Units (DU)		mentary des K to			Aiddle des 6 to	8)		High les 9 to	12)
Detached Single Family	y									
3 Bedroom	37	DU x .369	=	13.653	DU x .173	=	6.401	DU x .184	=	6.808
4 Bedroom		DU x .530	=		DU x .298	=		DU x .360	=	
> 5 Bedroom		DU x .345	=		DU x .248	=		DU x .300	=	
Attached Single Family	1									
> 1 Bedroom		DU x .000	=		DU x .000	=		DU x .000	=	
2 Bedroom		DU x .088	=		DU x .048	=		DU x .038	=	
> 3 Bedroom	92	DU x .234	=	21.528	DU x .058	=	5.336	DU x .059	=	5.428
> 4 Bedroom		DU x .322	==		DU x .154	=		DU x .173	=	
Apartments										
> Efficiency		DU x .000	=		DU x .000	=		DU x .000	=	
> 1 Bedroom		DU x .002			DU x .001	=		DU x .001	=	
2 Bedroom		DU x .086	=		DU x .042			DU x .046	=	
> 3 Bedroom		DU x .234	=		DU x .123	=		DU x .118		
Totals	129 TDU		-	35.181 _{TE}			TM			12.236 TH
(With	n deduction, if appli	cable)								

School Site Requirements:

Туре	# of students	Acres per student	Site	e Acres
Elementary (TE)	35.181	x .025	=	0.880
Middle (TM)	11.737	x .0389	=	0.457
High (TH)	12.236	x .072	=	<u>0.881</u>
		Total Site Acres		2.218

Cash in lieu of requirements:

2.218	(Total Site Acres)	X	\$240,500 (Fair Market Value per Improved Land)	=	\$	533,429	
-------	--------------------	---	---	---	----	---------	--

^{*}If the proposed subdivision contains an existing dwelling unit, a credit is available calculated as a reduction of the estimated population for the dwelling. Please request a worksheet from the City.

PA	RK	LA	ND	/CA	SH W	VC	RKSHEET
						•	

City of St. Charles, Illinois

Name of Development Date Submitted: Prepared by:

DLIVER-HOFFMAN RESUBDIVISION	



Total Dwe	lling	Units:	129	

A credit for existing residential lots within the proposed subdivision shall be granted. Deduct one (1) unit per existing lot.

Estimated Population Yield:

Type of Dwelling	# Dwelling Units (DU)	Population Generation per Unit	E	Sstimated Population
Detached Single Fam	nily			
> 3 Bedroom	37	DU x 2.899	=	107.263
> 4 Bedroom		DU x 3.764	=	
> 5 Bedroom		DU x 3.770	=	
Attached Single Fam	ily			
> 1 Bedroom	•	DU x 1.193	=	
2 Bedroom		DU x 1.990	=	
> 3 Bedroom	92	DU x 2.392	=	220.064
> 4 Bedroom		DU x 3.145	=	
Apartments				
➢ Efficiency		DU x 1.294	=	
> 1 Bedroom		DU x 1.758	=	
2 Bedroom		DU x 1.914	=	
3 Bedroom		DU x 3.053	=	
Totals	129			327.327
	Total Dwelling Units (with deduction, if applic		Esti	mated Total Population

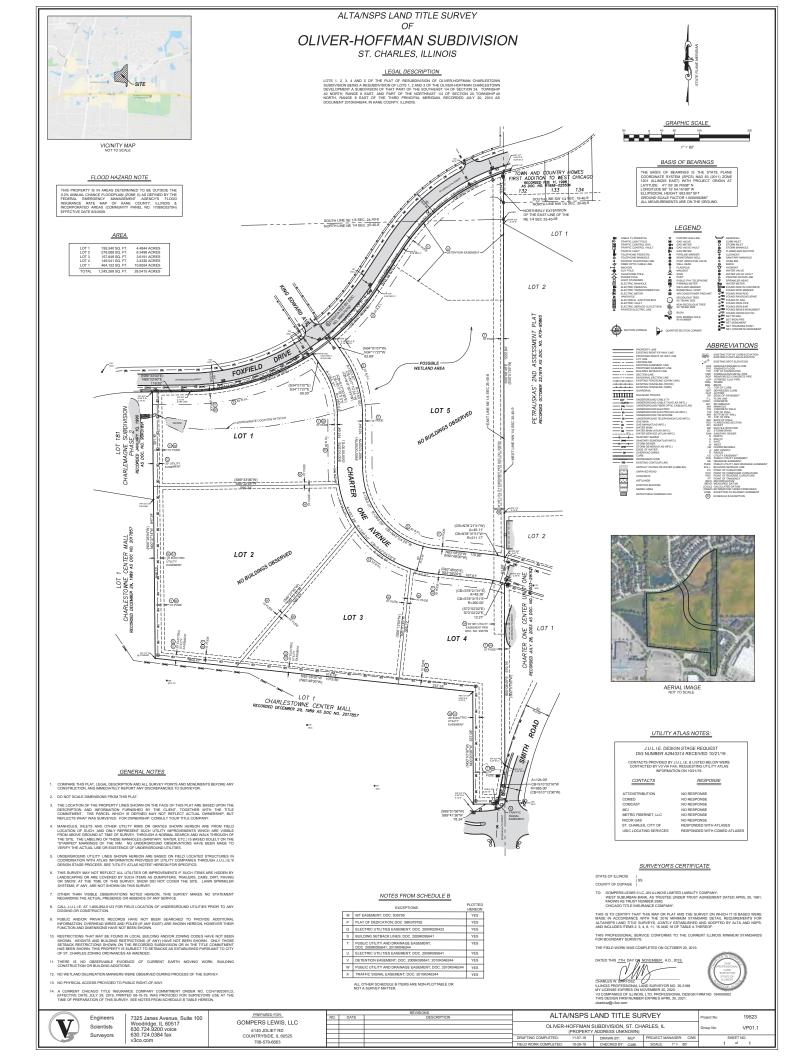
Park Site Requirements:

Estimated Total Population _ 327.327 ___ x .010 Acres per capita = ___ 3.273 ___ Acres

Cash in lieu of requirements:

Total Site Acres _____ x \$240,500 (Fair Market Value per Improved Land) = \$_____ 787,157

^{*}If the proposed subdivision contains an existing residential dwelling unit, a credit calculated as a reduction of the estimated population for the dwelling is available. Please request a worksheet from the City.







REVISIONS A

AND THE RECOMMENDS AND THE PROPER BRINGS INC. NO PART OF THIS WORK NO PARTIES OF PROPERTY PERMANENT

LINCOLN PROPERTY COMPANY
OLIVER-HOFFMAN
RESUBDIVISION
SYA, SE CORNER OF FOXFIELD DR & KING EDW.

G : SEK TE : 8/11/2020

SHEET TITLE

AERIAL MAP

C 1.0





REVISIONS A

AND THESE DRIVINGS ARE THE PROPE STRING, NO. NO PART OF THIS WORK NO SWITHOUT PROOF WITTEN PERMISSION STRING, NO.

LINCOLN PROPERTY COMPANY
OLIVER-HOFFMAN
RESUBDIVISION
SWEAS CORNER OF FOXFELD DR & KING EDWY

PROJ NO: 200130 ENG: SEK DATE: 8/11/2020

EXISTING CONDITIONS PLAN

SHEET NUMBER
C2.0
2 OF 3



REVISIONS A

OLIVER-HOFFMAN RESUBDIVISION

NG:SEK ATE: 8/11/2020

PROPOSED SITE PLAN

C3.0

















Fri 9/18/2020 11:08 AM

From: mark sawyer <sawyersam2001@yahoo.com>

Re: Oliver Hoffmann Resubdivision

Dear Mr. Colby,

My name is Mark Sawyer, we live at 3714 King George Lane, St Charles, IL 60174. Thank you, I received your public notice of the upcoming meeting for the subject property. My main concern is the proposed access road, charter one ave, that exits onto Fox Field Drive. I think that would cause a major traffic problem for that intersection and for the portion of Foxfield Drive that continues to Kirk Road. Perhaps a better location could be considered, such as at the southwest corner of the property where there is an alley located and that would access onto the service road going past the Jewel store entrance all the way to Foxfield Drive where there is already an intersection. I can see where that would require a re-positioning of one of the townhome buildings. Something like this would not only be better for us coming out on King Edward Drive to Foxfield, but it would also be better for the future residents of Oliver Hoffman subdivision, because then they also would have less traffic problems getting out to Highway 64 and Kirk Road. Thank you for your attention and consideration to this.

Sincerely, Mark Sawyer 630-327-2485 Sent from my iPhone From: Mark Hauser <markhauserinsurance@gmail.com>

Sent: Monday, September 21, 2020 11:59 AM

To: CD <cd@stcharlesil.gov>

Subject: Concept Plan for Oliver Hoffman Property

Does the plan include a 4-way stop at the intersection of Charter One Ave/King Edward Drive and Foxfield Dr.? If not, that should be part of the recommendation. With the number of cars that currently exceed the 25 MPH speed limit on Foxfield Drive (primarily Cornerstone Lakes residents), there are going to be numerous accidents if only the drivers coming off of Charter One or King Edward are required to stop. I would estimate that at least 75% of the vehicles driving down Foxfield Dr (school busses included) are exceeding the posted limit by 10-15 MPH. This is a huge concern!

Mark Hauser

IN THE CIRCUIT COURT OF THE SIXTEENTH JUDICIAL CIRCUIT KANE COUNTY, ILLINOIS

THE CITY OF ST. CHARLES, an Illinois municipal corporation,)	
Plaintiff,)	
vs.)	Case No. 07 ED 07
WEST SUBURBAN BANK, AS)	
TRUSTEE UNDER TRUST)	
AGREEMENT DATED APRIL 30, 1981,)	
AND KNOWN AS TRUST NO. 2580,)	Clerk of the Circuit Court
AND UNKNOWN OWNERS,)	Kane County, IL
)	
Defendants.)	. JUL - 8 2009
AMENDED CON	ISENT	DECREE-ILED 54

This cause coming on upon the agreement of the parties, and the Plaintiff and the Defendant having reached agreement on the matters set forth herein, and the Court being fully apprised in the premises.

Upon the agreement of the parties, the Court finds as follows:

- 1. Subject to the conditions hereinafter stipulated, the City of St. Charles (the "City") will agree to the voluntary dismissal of its Complaint for Condemnation to acquire by eminent domain a parcel of land legally described in the complaint filed by the City in this case (the "Eminent Domain Parcel").
- 2. The dismissal will be without prejudice.
- 3. In this litigation, the City sought to acquire the Eminent Domain Parcel for use as a right-of-way for the construction of an extension of Charter-One Drive (the "Charter-One Drive Extension").
- 4. The Eminent Domain Parcel is located entirely within a larger parcel of land owned by the West Suburban Bank as Trustee (the "Trust" and the "Trust Property").
- 5. The Trust Property was annexed into the City pursuant to an Annexation Agreement entered into by the City and the First Star Naper Bank, as Trustee and the Oliver-

- Hoffmann Corporation, as owners, on or about August 26, 1991, (the "Dunham Club Annexation Agreement").
- 6. The West Suburban Bank, as Trustee, is a successor title holder of record to the First Star Naper Bank. Hereinafter, the West Suburban Bank, as Trustee and the Oliver-Hoffmann Corporation shall be collectively referred to as the "Owners".
- 7. Pursuant to the Dunham Club Annexation Agreement, the City granted a Special Use as a Planned Unit Development by Ordinance 1991-Z-12 entitled "An Ordinance Granting a Special Use as a Planned Unit Development for the Dunham Club/Charlestowne PUD", further amended by Ordinance 1992-Z-1 entitled "An Ordinance amending Ordinance 1991-Z-12" (the "Existing Zoning Entitlement").
- 8. Attached hereto and incorporated herein as Exhibit "A" is a site plan of the Trust Property (the "Site Plan") prepared by Taurus Engineering, dated January 26, 2009 consisting of 1 page. As used herein, the terms "Site Plan" or, as hereinafter defined, "Alternative Site Plan" shall refer solely to the documents attached to this Amended Consent Decree and these documents are not, by virtue of the use of these terms as definitions herein, deemed to comply with any requirements of the St. Charles Municipal Code (the "City Code") which apply to the same or similar terms therein contained.

Based on the agreement of the parties and the findings contained herein, it is hereby ordered, adjudged and decreed as follows:

- 1. The City's complaint herein is voluntarily dismissed, said dismissal is without prejudice.
- 2. In consideration of the City's voluntary dismissal of this action, the Owners will, within twenty-eight (28) days of the entry of this Amended Consent Decree, file all proper and complete amended applications with the City requesting the initial zoning and subdivision relief to be granted pursuant to this Amended Consent Decree. Such relief shall be limited to classifying the various portions of the Trust Property as follows: Parcels 1 and 2 as depicted upon the Site Plan shall be rezoned and classified into the BC Community Business District and Parcel 3 shall be rezoned and classified RM-3 General Residential District, as hereinafter provided, and subdividing the three Parcels comprising the Trust Property into three zoning lots in accordance with the Site Plan heretofore incorporated herein as Exhibit A. Upon the filing of all proper and complete amended applications, the City will give notice and conduct all lawfully required public hearings necessary to enable the City to grant the relief hereinafter stipulated. Owners agree to pay to the City such fees and costs attendant thereto including, but not limited to, such fees and costs as may be specified in title 17 of the City Code.
 - a. An application to subdivide the Trust Property into three zoning lots as depicted upon the Site Plan and to rezone that portion of the Trust Property identified as Parcel 3 on

- the Site Plan heretofore incorporated herein as Exhibit A to the RM-3 General Residential District under the City's Zoning Ordinance (Section 17.12.010 M).
- b. Parcel 3 on the Site Plan consists of approximately 10.66 acres and is legally described on Exhibit "B" attached hereto and incorporated herein. Upon the subdivision of the Trust Property and the granting of the requested zoning referenced in subparagraph 2. a. above, Parcel 3 may be developed with a maximum density of no more than 17 dwelling units per acre plus a Density Bonus of no more than 3.4 units per acre pursuant to the provisions of Section 17.18.060 of the City Code. No structure constructed on Parcel 3 located North or Northwest of the demarcation line depicted on Exhibit A may exceed the height of either: (i) 40 feet (measured per City Code) or (ii) 802 feet above sea level (per City datum), whichever is less. No structure constructed on those portions of Parcel 3 located South or Southeast of the demarcation line depicted on Exhibit A may be constructed to a height greater than 40 feet (measured per City Code) or (ii) 810 feet above sea level (per City datum), whichever is less. No curb cut shall be allowed directly from Lot 3 onto Foxfield Drive. Owners shall develop Parcel 3 only with the uses authorized and permitted under the RM-3 General Residential District. Notwithstanding the granting of the zoning and subdivision entitlements referred to above and in paragraph 2. a., no development of Parcel 3 shall occur prior to the filing by Owners of all proper and complete applications for planned unit development approval in accordance with the City Code, particularly the City's Zoning Ordinance and the granting of planned unit development approval by the City. The City shall not be required to grant planned unit development approval unless the requested planned unit development is compliant with all of the City's requirements related to planned unit development approvals.
- In addition to the subdivision of the Trust Property in accordance with the Site Plan and the rezoning of Parcel 3 as hereinabove provided in paragraphs 2. a. and b., the Owners shall file applications to rezone those portions of the Trust Property identified as Parcels 1 and 2 on the Site Plan to the BC Community Business District (Section 17.14.010 B). Parcel 1 is legally described on Exhibit "C" which is attached hereto and incorporated herein by reference. Parcel 2 is legally described on Exhibit "D" which is attached hereto and incorporated herein by reference. Parcels 1 and 2 shall constitute two separate zoning lots. Owners shall develop Parcels 1 and 2 only with the uses authorized and permitted under the BC Community Business District (Section 17.14.010 B) except to the extent hereinafter limited by the provisions of paragraphs 3. and 4. of this Agreement. Notwithstanding the granting of the zoning and subdivision entitlements referred to in this paragraph above, no development of Parcel 1 or Parcel 2 shall occur prior to the filing by Owners of all proper and complete applications for planned unit development approval in accordance with the City Code, particularly the City's Zoning Ordinance, and the granting of planned unit development approval by the City. The City shall not be required to grant planned unit development approval unless the requested planned unit development is

compliant with all of the City's requirements related to planned unit development approvals.

In the event the Owners contract to sell Parcel 1 to the adjacent Charlestowne Mall ownership on or before January 1, 2010, the following provisions shall apply. In such event, the ownership of the Charlestowne Mall shall hereinafter be referred to as the "Contract Purchaser". Upon the execution of a contract for the sale of Parcel 1 to the Contract Purchaser, the Owners shall give prompt notice to the City of said contract. If the Contract Purchaser shall make prompt, proper and complete application for the zoning relief hereinafter set forth in this paragraph within six months of the date of the contract or on or before June 30, 2010, whichever date shall first occur, the following provisions of this paragraph shall apply. The Contract Purchasers may file applications for re-subdivision, zoning and other development entitlements requesting that Parcel 1 be incorporated into any existing zoning entitlement applicable to the Charlestowne Mall property. Upon receipt of a proper and complete application and the payment of all fees and costs attendant thereto from the Contract Purchaser, the City shall process such application in accordance with the provisions of the City Code, particularly the City's Subdivision and Zoning Ordinances, and paragraph 10. and 14. of this Amended Consent Decree including the giving of notice and conducting of such hearings as may be required by law and thereafter consider and, if acceptable, approve the resubdivision of the Charlestowne Mall property and adopt an ordinance granting the zoning relief requested therein provided that such requested relief is compliant with the City Code including the City's regulations related to the approval of subdivisions and the granting of zoning and planned unit development approvals and, further, that such relief is, in the judgment of the City, compatible with the zoning classification and planned unit development approval applicable to the existing Charlestowne Mall as of the date of the entry of this Amended Consent Thereafter, Parcel 1 shall be deemed to have been removed from the provisions of this Amended Consent Decree and Parcels 2 and 3 shall thereafter continue to be subject to the use limitations, terms and other restrictions and conditions set forth in this Amended Consent Decree.

If Parcel 1 is sold to any party other than the owners of the Charlestowne Mall, or if the Contract Purchaser shall acquire Parcel 1 but not request the zoning relief specified in the preceding paragraph within six months of the date of the contract or on or before June 30, 2010, whichever date shall first occur, then the zoning entitlements applicable to Parcel 1 will be those identified in the first paragraph of subparagraph c. and the terms of the immediately preceding paragraph will not apply. Further, if no contract for the sale of Lot 1 to the owners of the Charlestowne Mall is executed on or before January 1, 2010, then the subdivision and zoning entitlements applicable to the Trust Property pursuant to this order shall be as provided for in subparagraph d. below.

d. Attached hereto and incorporated herein as Exhibit "E" is an alternative site plan (hereinafter, the "Alternate Site Plan"). The Alternate Site Plan has been prepared by Taurus Engineering, dated January 26, 2009 consisting of 1 page. If no contract for the sale of Parcel 1 to the Charlestowne Mall ownership is executed on or before January 1, 2010, or if the Contract Purchaser, as hereinbefore defined, shall fail to make application for the zoning relief provided for in paragraph 2. c. and paragraph 10. of this Amended Consent Decree within six months of the date of the contract or on or before June 30, 2010, whichever date shall first occur, then the Owners shall make prompt and proper application for the re-subdivision of the Trust Property in accordance with the Alternate Site Plan. The Owners shall submit a "Plat of Resubdivision" subdividing the Trust Property into five lots as depicted upon the Alternate Site Plan. Lot 5 (previously Parcel 3 on the Site Plan) will continue to be zoned RM-3 General Residential District and may be developed subject to the provisions of the City's Code subject to the requirements, limitations, terms and conditions of this Amended Consent Decree particularly those set forth in paragraph 2. b. hereof. The Alternate Site Plan and the Plat of Resubdivision provides, and shall provide, that the balance of the Trust Property (Trust Property minus Parcel 3 - Lot 5) is to be re-subdivided into four (4) lots (Lots 1, 2, 3 and 4) which will continue to be zoned BC Community Business District and may be developed subject to the provisions of the City's Code in substantial conformance with Alternate Site Plan subject to the requirements, limitations, terms and conditions of this Amended Consent Decree. The Owners shall, upon the Plat of Re-subdivision, provide legal descriptions for Lots 1, 2, 3 and 4.

Should the balance of the Trust Property be re-subdivided so that Parcels 1 and 2 are subdivided into four (4) lots (Lots 1, 2, 3 and 4) as per the Alternative Site Plan, none of these lots (Lots 1, 2, 3 and 4) may be subdivided into a lot having a gross area greater than five (5) acres or a minimum lot area of less than one (1) acre unless the proposed use for said lot is a retail use which, in the sound discretion of the City, is complementary to the Charlestowne Mall planned unit development, such as an anchor/department store attached to the Mall, an electronics store (i.e. Best Buy), an appliance store, a home furnishings store, or a sit down restaurant.

Notwithstanding any of the foregoing provisions, should the Owners' be required to comply with the requirements of this subparagraph 2. d., no development of Lots 1, 2, 3, 4 or 5 shall occur prior to the filing by Owners of all proper and complete applications for planned unit development approval in accordance with the City Code, particularly the City's Zoning Ordinance, and the granting of planned unit development approval by the City. The City shall not be required to grant planned unit development approval unless the requested planned unit development is compliant with all of the City's requirements related to planned unit development approvals.

3. Owners agree that, notwithstanding the foregoing or anything contained in the City Code to the contrary, and further notwithstanding whether the planned unit development under which the Trust Property is eventually developed is approved incorporating the Site Plan or the Alternate Site Plan, the following uses which are otherwise permitted in the BC Community Business District shall be prohibited on Parcel 2 as depicted upon the Site Plan heretofore incorporated herein as Exhibit A (or on the Lots depicted on the Alternate Site Plan constituting part or all of Parcel 2):

PROHIBITED USES ON Parcel 2:

Outdoor Recreation Public Service facility School Bank Car wash Currency Exchange Gas station Heavy Retail and service Kennel Motor vehicle service and repair Motor vehicle sales and leasing Outdoor sales (permanent) Personal Services Establishments Tavern/Bar Veterinary Office/Animal Hospital Mini Warehouse Temporary Motor Vehicle Storage Communication Tower Utility, Community/Regional Transportation Operations Facility Places of worship

4. Owners agree that, notwithstanding the foregoing or anything contained in the City Code to the contrary, and further notwithstanding whether the planned unit development under which the Trust Property is eventually developed is approved incorporating the Site Plan or the Alternate Site Plan, the following uses which are otherwise permitted in the BC Community Business District shall be prohibited on Parcel 1 as depicted upon the Site Plan heretofore incorporated herein as Exhibit A (or on the Lots depicted on the Alternate Site Plan constituting part or all of Parcel 1):

PROHIBITED USES ON Parcel 1:

Outdoor Recreation Public Service facility School Car wash Currency Exchange
Gas station
Heavy Retail and service
Kennel
Motor vehicle service and repair
Outdoor sales (permanent)
Tavern/Bar
Veterinary Office/Animal Hospital
Mini Warehouse
Temporary Motor Vehicle Storage
Communication Tower
Utility, Community/Regional
Transportation Operations Facility
Places of worship

- 5. Notwithstanding the foregoing or anything contained in the City Code to the contrary, banks and pharmacies/drug stores and, upon the filing of an application for a special use, drive-thru facilities attendant thereto, will be permitted within Parcel 1 as depicted upon the Site Plan (or on the Lots depicted on the Alternate Site Plan contained within Parcel 1) provided, however, that the Trust must make application for preliminary plan approval under the City's planned unit development regulations (including an application for a special use if drive-thru facilities are to be requested) the approval of which shall be within the City's reasonable discretion including discretion as to the location and brightness of site lighting and traffic circulation for Parcel 1.
- 6. No curb cut shall be allowed directly from Parcel 2, as depicted upon the Site Plan, (or on the Lots depicted on the Alternate Site Plan constituting part or all of Parcel 2) onto Foxfield Drive.
- 7. Twenty-four (24) hour operations, including drive-thru uses, shall be prohibited on the Trust Property except for Parcel 1, as depicted upon the Site Plan, (or on the Lots depicted on the Alternate Site Plan contained within Parcel 1) subject to preliminary plan approval as provided for planned unit developments under the City Code.
- 8. Development of Parcels 1 and 2, as depicted upon the Site Plan, (or on the Lots depicted on the Alternate Site Plan contained within Parcels 1 and 2) shall comply with all bulk regulations of the BC Zoning District (Chapter 17.14 of Zoning Ordinance), except that a 40 foot landscape buffer yard shall be provided along the north property line of Parcel 2. The forty (40) foot buffer line is depicted on both the Site Plan and the Alternate Site Plan. No buildings or parking shall be allowed in such buffer yard.
- 9. None of the structures constructed on Parcels 1 and 2, as depicted upon the Site Plan (or on the Lots depicted on the Alternate Site Plan contained within Parcels 1 and 2) shall be

constructed to a height greater than either: (i) 40 feet or (ii) 802 feet above sea level, whichever is less.

- 10. If the Owner and the Contract Purchaser shall enter into a contract for the sale of Parcel 1, as depicted upon the Site Plan and as hereinbefore provided, then in addition to any other provision of this Amended Consent Decree, the provisions of this paragraph 10. shall apply. Should the Contract Purchaser make prompt, proper and complete application to add Parcel 1 to the existing Charlestowne Mall planned unit development and conditioned upon the Contract Purchaser making payment of all fees and costs attendant thereto, the City shall properly notice and cause to be conducted all legally required hearings necessary to permit the existing planned unit development for the Charlestowne Mall to be amended to include Parcel 1. The Contract Purchaser shall request the re-subdivision of the zoning lot comprising the Charlestowne Mall to provide for the inclusion of Parcel 1 within the Charlestowne Mall planned unit development and shall further request zoning relief necessary to permit the development of Parcel 1 in a manner consistent with the planned unit development currently applicable to the existing Charlestowne Mall. After the conduct of all required hearings and the receipt of all required recommendations, should the City's Corporate Authorities determine that the inclusion of Parcel 1 into the existing Charlestowne Mall planned unit development is consistent with the intent and purpose of that planned unit development and that such application is otherwise in compliance with the City Code, the City shall adopt an ordinance amending the existing Charlestowne Mall planned unit development adding Parcel 1 thereto upon such terms and conditions as the City may deem appropriate together with such other actions as may be necessary or appropriate. Any such ordinance adopted prior to the closing of the purchase of Parcel 1 from the Trust to the Contract Purchaser shall provide that the effectiveness of the ordinance shall be contingent upon the closing of said sale and the acquisition of Parcel 1 by the Contract Purchaser. Thereafter, and without regard to the provisions of paragraph 4 of this Amended Consent Decree, the uses permitted upon Parcel 1 shall be the uses provided for in the Charlestowne Mall planned unit development which may include uses such as a "big box" retail store of up to 175,000 square feet. Should Parcel 1 be conveyed to, and be developed as part of, the Charlestowne Mall planned unit development, no access from Parcel 1 to Charter-One Drive shall be permitted. Further, should Parcel 1 be conveyed to, and developed as part of, the Charlestowne Mall planned unit development, Parcel 2, as depicted upon the Site Plan, may be developed with up to a maximum of 75,000 square feet in a manner compliant with the other provisions of this Amended Consent Decree and the City Code. If, for any reason, the sale of Parcel 1 to the Contract Purchaser should not close, the provisions of this paragraph 10. and the second paragraph of subparagraph 2. c. shall not apply, and Owners development rights shall be determined in accordance with the balance of this Amended Consent Decree.
- 11. No direct or cross access shall be permitted between Parcels 1 and 2, as depicted upon the Site Plan. This prohibition shall apply regardless of whether Parcel 1 is eventually sold to and incorporated in the Charlestowne Mall planned unit development. Nonetheless,

this restriction shall not apply if the Trust Property shall be zoned, subdivided and developed in accordance with the Alternate Site Plan. In this case, cross access and cross access easement agreements may be required by the City to facilitate the location and functioning of a frontage road system to restrict and limit direct access to Charter-One Drive to appropriate intersections.

12. The Owners may request a deviation from the City Code as part of the any required planned unit development application which would allow for the erection of an Off-Premise Identification monument sign. This sign would be for the benefit of Parcels 2 (or on the Lots depicted on the Alternate Site Plan contained within Parcel 2) and Parcel 3 (Lot 5), as depicted upon the Site Plan (or, as the case may be, the Alternate Site Plan) and is to be located on the southeast portion of Parcel 1 where, and as depicted upon, the Site Plan heretofore incorporated herein as Exhibit "A". Such sign shall be requested by Owners and shall be subject to review and approval for conformance with the following standards:

Minimum setback from ROW: 10 feet; and a

Maximum sign area: 64 square feet per face for a double faced monument sign; and a

Maximum height: 14 feet; and

The sign may be externally illuminated only; no lighting on top of signage; illumination shall be designed, located, shielded and directed so as to prevent casting of glare or direct light upon adjacent streets or surrounding properties; and The base of the monument sign shall be decorative masonry or natural stone materials; the sign panel shall be flanked by two symmetrical masonry or stone posts; and

The sign base shall be landscaped in accordance with Section 17.26.100 of the City Code; and

The sign panels shall be of a durable material such as stone, metal or alumilite; and

The signage shall comply with all other applicable provisions of Chapter 17.28 of the City Code.

13. At such time as the City shall grant approval of the initial zoning entitlements and subdivision relief outlined in paragraph 2. and in subparagraphs 2. a., b. and the first paragraph of subparagraph 2. c. and paragraph 12 hereof, the Owners shall cause to be dedicated to the City a highway/road right-of-way for the Charter-One Drive Extension (the "Extension") as depicted upon the Site Plan heretofore incorporated herein as Exhibit "A" (the "Extension Dedication"). The legal description of the proposed Extension Dedication is contained on Exhibit F attached hereto and incorporated herein. The Owners shall cause the Extension Dedication to be made by way of a properly prepared and executed plat of subdivision, at no cost to the City.

- a. The Extension Dedication shall be eighty (80') feet in width intersecting with the existing right-of-ways for Charter One Avenue and King Edward Drive.
- b. Prior to the issuance of any building permit for the construction of any building or structure within Parcels 1, 2 or 3, as depicted upon the Site Plan (or on the Lots 1, 2, 3, 4 or 5 as depicted on the Alternate Site Plan), the Owners, or their successors and assigns, shall cause to be submitted to the City for its appropriate final engineering and construction plans and specifications for the construction of the Extension within the Extension Dedication as depicted upon the Site Plan heretofore incorporated herein as Exhibit "A". Upon the City's approval of the final engineering and construction plans and specifications for the construction of the Extension, the Owners' or, as appropriate, their successors and assigns, shall cause the Extension to be constructed pursuant to such approved plans and specifications. Such construction is to commence with all other required site work for any Parcel or Lot as to which a building permit has been issued and prosecuted diligently until completion and acceptance by the City. If the construction of the Extension is not commenced or prosecuted diligently to completion as herein required, the City may withhold the issuance of occupancy permits and may otherwise bar occupancy and use of the Trust Property. In the event Parcel 1 is conveyed to the Contract Purchaser, Parcel 1 and the Contract Purchaser shall be deemed exempt from this requirement and the obligation of the Owners described in this subparagraph shall then be triggered by an application for a building permit being made with respect any building or structure to be located within Parcels 2 or 3 as depicted upon the Site Plan (or on the Lots depicted on the Alternate Site Plan which constitute any part of Parcels 2 or 3 as depicted on the Site Plan).
- c. If no building permit has been drawn for the construction of any structure within Parcels 1, 2 or 3 as depicted upon the Site Plan, or within Parcels 2 or 3 as depicted upon the Site Plan per subparagraph 13. b. above (or on Lots 1, 2, 3, 4 or 5 as depicted on the Alternate Site Plan, or the Lots depicted on the Alternate Site Plan which constitute any part of Parcels 2 or 3 as depicted on the Site Plan per subparagraph 13. b. above) by January 1, 2024, the Owners, or their successors and assigns, shall cause the Extension to be constructed within nine (9) months of January 1, 2024.
- d. Notwithstanding the preceding subparagraph c., at any time after January 1, 2016, the City may determine that the construction of the Extension is required to serve the traffic management, flow and related needs of the City. Should the City make such a determination, the City shall request that the Owners waive their rights under subparagraph c. above and immediately proceed with the development of all necessary engineering plans and specifications for the construction of the Extension and, upon the approval of said engineering plans and specifications, promptly proceed with the construction of the Extension. Should the owners elect to waive their rights under the preceding subparagraph c., they shall immediately cause all such necessary

engineering plans and specifications to be prepared for review and approval by the City and, once approved, shall let appropriate contracts for the construction of the Extension. The construction of the Extension shall be diligently prosecuted until completion and acceptance by the City. If the City reasonably determines that the Owners are not proceeding diligently with the preparation of the necessary engineering plans and specifications or should the City determine that the construction of the Charter-One Drive Extension is being unreasonably delayed or not prosecuted diligently, the City may serve notice on the Owners and if, in the City's reasonable determination, the Owners do not promptly act to remedy the delay, the City may assume responsibility for the completion of the work necessary to complete the construction of the Extension project. In such event, the City shall have the rights hereinafter provided in the subsequent paragraph.

Upon the City's request that Owners proceed with the Extension project, should the Owners elect not to undertake the project or should the Owners initially elect to undertake the project but fail to diligently prosecute such work to completion, as provided in the preceding subparagraph, the City may assume responsibility for the construction of the project under the following terms and conditions. The City shall cause all necessary and proper engineering for the construction of the Extension to be prepared or completed, as the case may be, and shall then cause contracts to be let for the construction of the Extension in a manner consistent with the approved engineering plans and specifications. The engineering plans and specification shall encompass all facets of the project including, but not by way of limitation, all required signalization and detention. Should additional land be necessary to accommodate detention made necessary by reason of the construction of the Extension, the Owners shall dedicate, free of charge to the City, such additional land as may be necessary in an amount and location reasonably agreeable to the parties. The City shall advance the sums necessary to pay the cost of the engineering and the construction of the Extension. The City may do so from cash reserves on hand or from the proceeds of any bond issue or other public financing as deemed appropriate by the City or from any combination thereof. The Owners, their successors and assigns shall be liable to the City for the repayment of the entire cost of the Extension (engineering and construction) together with interest thereon at the interest rate being paid by the City on any public financing incurred by the City for the funds used to pay the cost of the Extension or at an interest rate equal to the interest rate then being quoted by the Wall Street Journal as the prime rate. Further, the City shall have a lien on the Trust Property to secure the repayment of the sums due the City (including the interest thereon). Said lien may be separately recorded in the chain of title to the Trust Property in which case Owners agree to cooperate to place said lien of record in the office of the Recorder of Deeds of Kane County, Illinois. Further, the parties agree that a memorandum of agreement reflecting this provision shall also be so recorded.

Thereafter, at such time as a building permit may be applied for as to all or any portion of the Trust Property, the entire outstanding balance of the costs of the Extension, together with the interest due thereon, shall become immediately due and payable to the City and the City shall have no obligation to approve or issue any such building permit until the Owners, or their successors or assigns, as the case may be, have fully satisfied this obligation.

- e. The Charter-One Drive Extension shall be constructed as a collector street at no cost to the City. The construction shall be accomplished pursuant to appropriate engineering and construction plans and specifications for the construction of the Charter-One Drive Extension in accordance with the requirements of the City Code and the City's standards for roadway construction as approved by the City.
- f. Storm water detention for the Charter-One Drive Extension shall be provided for on Parcel 3, as depicted upon the Site Plan (or on Lots 5 as depicted on the Alternative Site Plan), in compliance with the City Code and the City's standards for the construction of storm water detention facilities. Detention must be provided at the time of, and as part of, the construction of Charter-One Drive Extension.
- g. Curb cuts onto the Charter-One Drive Extension shall be limited to two four (4) way intersections located so as to provide a safe and adequate distance from adjacent public streets and private driveways. Curb cuts located on the east and west sides of the Charter-One Drive Extension shall be so located so as to form four-way intersections.
- h. Signalization and intersection improvements at the intersection of the existing Charter One Drive and Smith Road shall be constructed pursuant to one or more warrants for a signal being met in accordance with the Manual on Uniform Traffic Control Devices whenever any one or more warrants from the Manual is met. A financial guarantee in the form of a letter of credit shall be provided for these improvements at the time the Owners or, as appropriate, their successors and assigns, build the Charter-One Drive Extension. The nature and extent of these improvements shall be in accordance with Article 13 b. or 13 c. whichever may apply. The financial guarantee shall be maintained until the first to occur of the following: (i) the improvements are accepted by the City or (ii) seven (7) years have passed from the date of the initial guarantee. The signalization shall be coordinated (fiber connection) with the existing signal at the intersection of IL 64 and Smith Road. In the event Parcel 1, as depicted upon the Site Plan, is conveyed to the owners of the Charlestowne Mall planned unit development, Parcel 1 and the owners of Charlestowne Mall shall be deemed exempt from this requirement and the obligation of the Owners described in this subparagraph shall then be exclusively born by the Owners of Parcels 2 or 3, as depicted upon the Site Plan (or the Lots depicted on the Alternative Site Plan which constitute any part of Parcels 2 or 3 as depicted on the Site Plan) or, as appropriate, their successors and assigns.

- i. Additional roadway, signalization, and intersection improvements may be required due to increased traffic resulting from development on the Trust Property consistent with the development entitlements provided for herein. The City may require that, as a condition of processing any application for zoning and/or other development entitlements, that a traffic study be conducted in conjunction with the proposed zoning and/or other development entitlements. The traffic study shall be performed by the City prior to the commencement of the work being undertaken pursuant to subparagraphs 13. b. or 13. c., whichever subparagraph may apply. Additional roadway and signalization improvements consistent with the results and recommendations of the traffic study shall be constructed contemporaneously with the construction of the Charter One Drive Extension. No owner or developer of any parcel of the Trust Property shall be required to make or pay more than its proportionate share of any improvement in accordance with the following formula: projected traffic from Trust Property parcel expected to use improvement / total projected traffic expected to use improvement = proportionate share of Trust Property parcel.
- 14. Upon proper application as herein specified, the City shall cause proper notice to be given and shall hold all legally required public hearings on the Owners' zoning and subdivision applications and shall, to the extent such applications are consistent with the terms of this Amended Consent Decree, the City Code, ordinances, rules and regulations, approve same by ordinances duly adopted and approved by its Corporate Authorities within one hundred eighty (180) days of the filing of the complete and accurate applications. Should the Owners or, as appropriate, their successors or assigns, cause or request a delay in the processing of the applications or the conduct or any required hearing or meeting, the one hundred eighty (180) day period provided for herein shall be tolled by a number of days equal to the number of days attributable to the delay caused or requested.

Upon the passage and approval of the initial zoning and subdivision entitlements provided for in paragraph 2. and in subparagraphs 2. a., b. and the first paragraph of subparagraph 2. c. and paragraph 12 hereof, the parties will present to the Court and amendment to this Amended Consent Decree confirming that the parties have performed their respective obligations as herein set forth.

Notwithstanding anything to the contrary in this Amended Consent Decree, the City retains the sole and exclusive discretion to grant or deny the initial zoning and subdivision entitlements as set forth in paragraphs 2 and 2.a above, upon proper application and after all due notice and public hearings have been conducted. The City's grant of the initial zoning and subdivision entitlements is a condition precedent to the effectiveness of this Amended Consent Decree and the Owner's obligation to dedicate, construct and pay for the Extension as described in paragraph 13 and its subparagraphs. Any other zoning relief contemplated by this Amended Consent Decree will not be

granted by the City except upon proper application by Owner or Contractor Purchaser as described herein which is consistent with the terms of the Amended Consent Decree, the City Code, and all applicable ordinances, rules and regulations including the issuance of required public notice(s) and public hearing(s).

The ordinances providing for the granting of the zoning entitlements and subdivision applicable to the Trust Property, once approved by the City's Corporate Authorities, shall remain in effect for not less than 20 years, unless modified by the agreement of both the City and the Owners or, as appropriate, the Owners' successors or assigns. Such modifications shall require, in each instance, the filing of proper, accurate and complete applications for zoning or other relief which shall be processed in the same manner and which shall be subject to the same codes, ordinances, rules and regulations as would otherwise apply to other similar applications in the St. Charles' City Code. Should the City elect to grant the requested relief, the City and the Owners or, as appropriate, their successors or assigns, shall cause a further amendment to the Amended Consent Decree to be presented to the court for its approval and, upon such approval, such further Amended Consent Decree shall control the development of the Trust Property or so much of the Trust Property as was properly the subject of the applications requesting the modifications of the development entitlements provided for herein.

In the event that the Owners, or their successors or assigns file proper, accurate and complete applications for the initial zoning, subdivision or other relief as herein contemplated and should the City shall deny the Owners' applications or fail to act on the Owners' applications within one hundred eighty (180) days from the date of the filing of proper, accurate and complete applications, or within such extended period of time as may be attributable to a delay caused or requested by the Owners or, as appropriate, their successors or assigns, as hereinbefore provided, the Owners, or as appropriate, their successors or assigns may petition the court to enforce the terms of this Consent Decree by conducting a hearing as to whether the City's delay or denial of the Owners' applications violates the terms of the Amended Consent Decree.

Upon the conclusion of such hearing, should the Court determine that the City's delay or denial violates the terms of this Amended Consent Decree, the sole relief available to the Owners, or as appropriate, their successors or assigns, is to request the Court to vacate this Amended Consent Decree.

In the case of each application for zoning, subdivision or other relief or entitlement filed by Owners pursuant to the provisions of this Consent Decree, and particularly paragraph 14 hereof, the Owners, or as appropriate, their successors or assigns, shall pay to the City all fees and costs attendant thereto as the City may, from time to time provide for the form of relief or entitlement being requested.

15. That each party shall bear its own costs and attorney's fees in this matter.

- 16. From and after the City's adoption and approval of the initial ordinances or resolutions granting of the zoning entitlements and subdivision relief contemplated herein, the Dunham Club Annexation Agreement and the Existing Zoning Entitlements shall be null, void and of no further force and effect and shall, in each respect, be replaced and governed by the terms of this Amended Consent Decree and the City Code, its ordinances, rules and regulations to the extent not in conflict with this Decree.
- 17. This Amended Consent Decree shall remain in full force and effect for a period of twenty years and the Court shall retain jurisdiction of this matter to either enter a final order upon performance of the terms of this Amended Consent Decree, or to vacate this Amended Consent Decree as set forth herein.

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One of the Attorneys for Defendant, West Suburban Bank, as Trustee under Trust Agreement Dated April 30, 1981, and Known as Trust No. 2580, the Oliver-Hoffmann Corporation and Unknown Owners

JUDGE_______ JUL 8 2009

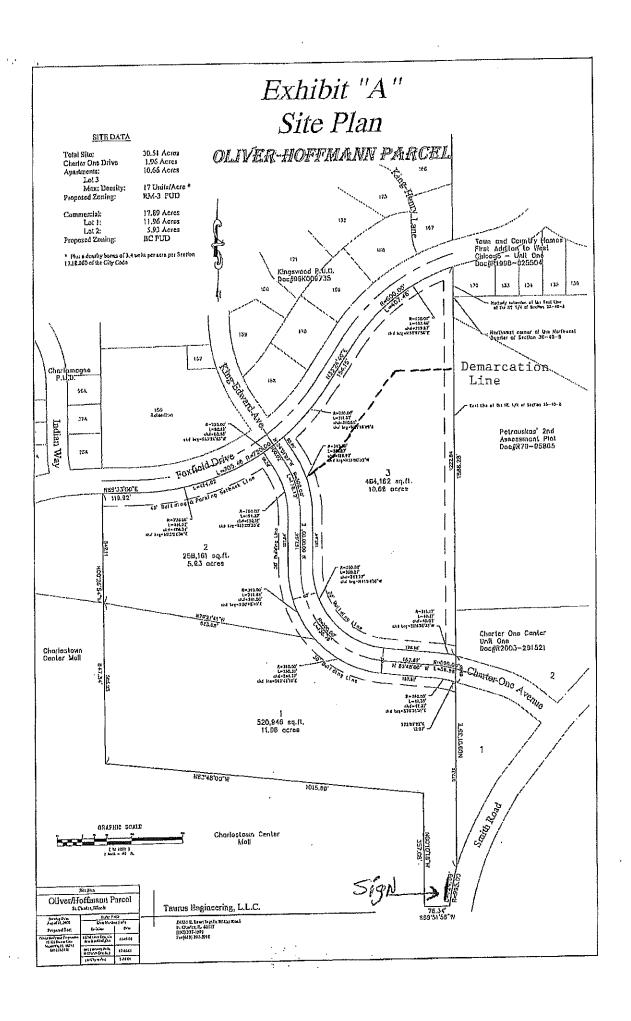


EXHIBIT "B"

Parcel 3

That Part of the Southeast Quarter of Section 24, Township 40 North Range 8 East and part of the Northeast Quarter of Section 25, Township 40 North, Range 8 East of the Third Principal Meridian described as follows:

Beginning at the Southwest corner of Lot 2 in Charter One Center Unit One Subdivision, being a subdivision in the Northwest Quarter of Section 30, Township 40 North, Range 9 East of the Third Principal Meridian in DuPage County, Illinois, said point being the northwest corner of the Right of Way of Charter One Avenue as dedicated in said Charter One Center Unit One Subdivision, thence westerly along the arc of curve concave to the south having a radius of 211.17 feet, a chord bearing of North 78°21'31" West and a chord length of 40.05 feet, 40.11 feet; thence North 83°48'00" West 179.88 feet; thence northerly along the arc of curve concave to the northeast having a radius of 260 feet, a chord bearing of North 41°54'00" West and a chord length of 347.27 feet, 380.27 feet; thence North 0°00'00" East 167.64 feet; thence northerly along the arc of a curve concave to the west having a radius of 340 feet, a chord bearing of North 17°00'33" West and a chord length of 198.92 feet, 201.87 feet; thence North 34°01'07" West 62.89 feet to the southerly Right of Way line of Foxfield Drive as dedicated by document 98K079762 in Kane County, Illinois; thence easterly along said southerly Right of Way of Foxfield Drive being the arc of curve concave to the northwest with a curve radius of 720 feet, a chord bearing of North 41°49'09" East and a chord length of 210.56 feet, 211.32 feet; thence North 33°24'40". East along the southerly Right of Way line of said Foxfield Drive, 154.15 feet; thence continuing easterly along said southerly Right of Way line said line being the arc of a curve concave to the south having a radius of 600 feet, a chord bearing of North 52°51'56" East and a chord length of 399.67 feet, 407.46 feet to the Northwest corner of Lot 132 in Town and Country Homes First Addition to West Chicago Unit 1 recorded as document #R1998-025504 in DuPage County, Illinois; thence South 00°01'35" West along the east line of the Northeast Quarter of Section 25, Township 40 North, Range 8 East of the Third Principal Meridian and the northerly extension thereof, 1222.84 feet to the point of beginning, containing therein 10.66 acres more or less all in Kane County, Illinois.

EXHIBIT "C"

Parcel 1

That Part of the Northeast Quarter of Section 25, Township 40 North, Range 8 East of the Third Principal Meridian described as follows:

Beginning at the Northwest corner of Lot 1 in Charter One Center Unit One Subdivision, being a subdivision in the Northwest Quarter of Section 30, Township 40 North, Range 9 East of the Third Principal Meridian in DuPage County, Illinois, said point being the southwest comer of the Right of Way of Charter One Avenue as dedicated in said Charter One Ceuter Unit One Subdivision, thence North 72°55'02" West 12.27 feet; thence continuing westerly along the arc of curve concave to the south having a radius of 260 feet, a chord bearing of North 78°21'31" West and a chord length of 49.31 feet, 49.39 feet; thence North 83°48'00" West 167.61 feet; thence northwesterly along the arc of curve concave to the northeast having a radius of 340 feet, a chord bearing of North 62°42'10" West and a chord length of 244.77 feet, 250.39 feet; thence North 79°21'41" West 683.08 feet; thence South 00°26'54" East 505.23 feet; thence South 83°48'00" East 1015.80 feet; thence South 00°10'18" East 357.08 feet; thence North 89°51'56" East 78.34 feet; thence northerly along the arc of a curve concave to the east having a radius of 995 feet, a chord bearing of North 10°13'36"East and a chord length of 124.01 feet, 124.09 feet to the east line of the Northeast Quarter of said Section 25; thence North 00°01'35" East along said east line 579.76 feet to the point of beginning, containing therein 11.96 more or less acres all in Kane County, Illinois.

EXHIBIT "D"

Parcel 2

That Part of the Northeast Quarter of Section 25, Township 40 North, Range 8 East of the Third Principal Meridian described as follows:

Commencing at the Northwest corner of Lot 1 in Charter One Center Unit One Subdivision, being a subdivision in the Northwest Quarter of Section 30, Township 40 North, Range 9 East of the Third Principal Meridian in DuPage County, Illinois, said point being the southwest corner of the Right of Way of Charter One Avenue as dedicated in said Charter One Center Unit One Subdivision, thence North 72°55'02" West 12.27 feet; thence continuing westerly along the arc of curve concave to the south having a radius of 260 feet, a chord bearing of North 78°21'31" West and a chord length of 49.31 feet, 49.39 feet; thence North 83°48'00" West 167.61 feet; thence northwesterly along the arc of curve concave to the northeast having a radius of 340 feet, a chord bearing of North 62°42'10" West and a chord length of 244.77 feet, 250.39 feet to the Point of Beginning; thence North 79°21'41" West 683.08 feet; thence North 00°26'54" West 342.11 feet to the southerly Right of Way line of Foxfield Drive as dedicated by document 98K079762 in Kane County, Illinois; thence North 89°33'00" East along the southerly Right of Way line of said Foxfield Drive, 119.92 feet; thence continuing easterly along said southerly Right of Way line of said Foxfield Drive being the arc of a curve concave to the north having a radius of 720 feet, a chord bearing of North 73°04'36" East and a chord length of 408.34 feet, 414.02 feet; thence South 34°01'07" East 59.30 feet; thence southerly along the arc of a curve concave to the west having a radius of 260 feet, a chord bearing of South 17°00'33" East and a chord length of 152.11 feet, 154.37 feet; thence South 0°00'00" West 167.64 feet; thence southeasterly along the arc of curve concave to the northeast having a radius of 340 feet, a chord bearing of South 20°48'10" East and a chord length of 241.50 feet, 246.89 feet to the point of beginning, containing therein 5.93 acres more or less all in Kane County, Illinois.

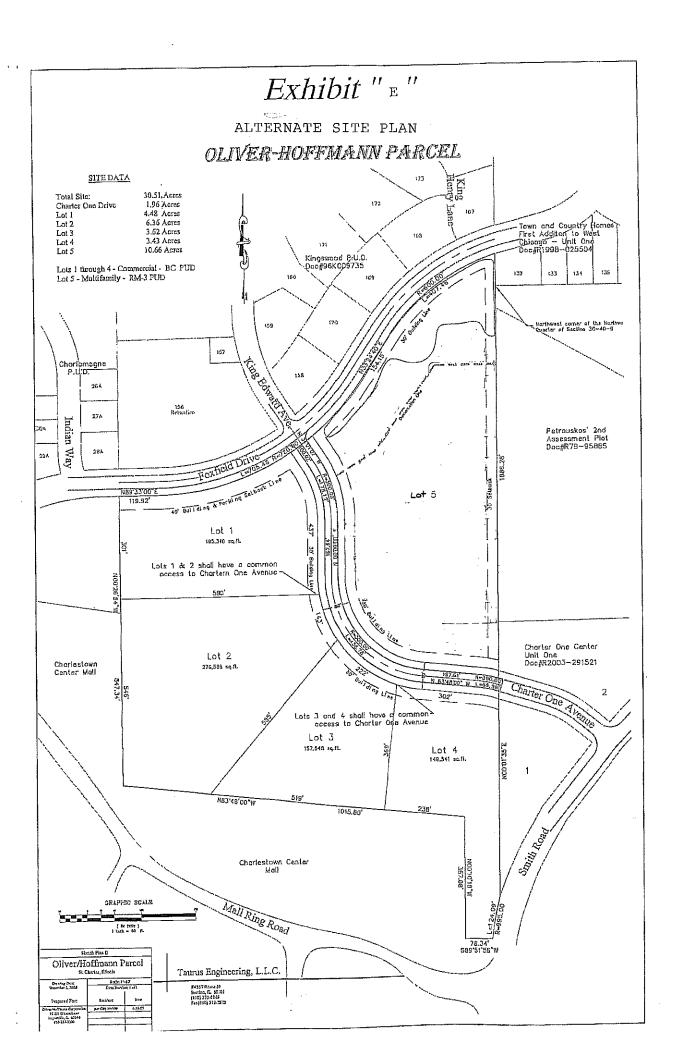


EXHIBIT "F"

CHARTER ONE AVENUE

That Part of the Northeast Quarter of Section 25, Township 40 North, Range 8 East of the Third Principal Meridian described as follows:

Beginning at the Southwest corner of Lot 2 in Charter One Center Unit One Subdivision, being a subdivision in the Northwest Quarter of Section 30, Township 40 North, Range 9 East of the Third Principal Meridian in DuPage County, Illinois, said point being the northwest corner of the Right of Way of Charter One Avenue as dedicated in said Charter One Center Unit One Subdivision, thence westerly along the arc of curve concave to the south having a radius of 211.17 feet, a chord bearing of North 78°21'31" West and a chord length of 40.05 feet, 40.11 feet; thence North 83°48'00" West 179.88 feet; thence northerly along the arc of curve concave to the northeast having a radius of 260 feet, a chord bearing of North 41°54'00" West and a chord length of 347.27 feet, 380.27 feet; thence North 0°00'00" East 167.64 feet; thence northerly along the arc of a curve concave to the west having a radius of 340 feet, a chord bearing of North 17°00'33" West and a chord length of 198.92 feet, 201.87 feet; thence North 34°01'07" West 62.89 feet to the southerly Right of Way line of Foxfield Drive as dedicated by document 98K079762 in Kane County, Illinois; thence westerly along said southerly Right of Way of Foxfield Drive being the arc of curve concave to the north with a curve radius of 720 feet, a chord bearing of South 53°24'55" West and a chord length of 80.08 feet, 80.12 feet; thence South 34°01'07" East 59.30 feet; thence southerly along the arc of a curve concave to the west having a radius of 260 feet, a chord bearing of South 17°00'33" East and a chord length of 152.11 feet, 154.37 feet; thence South 0°00'00" West 167.64 feet; thence southeasterly along the arc of curve concave to the northeast having a radius of 340 feet, a chord bearing of South 41°54'00" East and a chord length of 454.13 feet, 497.28 feet; thence South 83°48'00" East 167.61 feet; thence easterly along the arc of a curve concave to the south having a radius of 260 feet, a chord bearing of South 78°21'31"East and a chord length of 49.31 feet, 49.39 feet; thence South 72°55'02"East 12.27 feet to the southwest corner of the Right of Way of Charter One Avenue as dedicated in said Charter One Center Unit One Subdivision; thence North 00°01'35" East along the westerly Right of Way line of said Charter One Avenue as dedicated in said Charter One Center Unit One Subdivision, 83.68 feet to the point of beginning, containing therein 1.96 acres more or less all in Kane County, Illinois.

	AGENDA	A ITEM EX	XECUTIVE SUMMARY	Agenda Item Number: 4b	
CITY OF	Title:	Presenta	tion of a Concept Plan fo	or 1023 W. Main St.	
ST. CHARLES ILLINOIS • 1834	Presenter:	Ellen John	nson		
Meeting: Planning	& Developm	ent Commi	ttee Date: Octob	ber 12, 2020	
Proposed Cost: \$		Bud	geted Amount: \$	Not Budgeted:	
			2777 2 5 1 2 1 2 1 2	4 th ~	_

The subject property is located at the southeast corner of W. Main St. and S. 11th St. A gas station has operated on the property since as early as the 1920s. The property is zoned residential. Its nonconforming status necessitates a zoning change and/or PUD approval to redevelop the site for continued use as a gas station.

Mohammed Shahid Ali, property owner, is seeking feedback on a Concept Plan to redevelop a gas station. Details of the proposal are as follows:

- Rezone the property to the BL Local Business District and establish a Planned Unit Development (PUD).
- Retain existing site access points to Main St. and 11th St.
- Construct a new 1,434 sf convenience store with a 1-bedroom upper level dwelling unit in the general location of the existing convenience store.
- Construct a gas station canopy with three fuel pump stations.

The Comprehensive Plan land use designation for the property is Neighborhood Commercial with residential character.

Plan Commission Review

Plan Commission reviewed the Concept Plan on 10/6/20. Comments are summarized as follows:

- Support for continued gas station use with upper level residential unit.
- Several changes to the plans were suggested in order to improve the function and appearance of the redevelopment:
 - o Close the western Main St. access point due to safety concerns.
 - Add landscaping where possible, such as along Main St. in place of the western access, around the freestanding sign, along the building between the convenience store and residential unit entrances, add planters between gas pumps, etc.
 - o Move the freestanding sign to the NW corner.
 - o Improve the design of the proposed building. High-quality materials should be used. The design should blend with the neighborhood. Look into reducing the building size to allow for a larger rear setback and more landscaping.

Neighboring property owners provided feedback via email (attached). The comments were generally supportive of the gas station use. Concern was expressed for the addition of the residential unit.

Attachments (please list):

Concept Plan Application, Plans, Correspondence from neighboring property owners

Recommendation/Suggested Action (briefly explain):

Provide comments on the Concept Plan. Staff is recommending the Committee provide comments on the following topics:

- Gas station & residential uses.
- Proposed BL zoning designation.
- Site layout and access.
- Building design.
- Whether a PUD would be warranted for this development.

Community & Economic Development Community Development Division

Phone: (630) 377-4443 Fax: (630) 377-4062



Staff Report

TO: Chairman Rita Payleitner

And Members of the Planning & Development Committee

FROM: Ellen Johnson, Planner

RE: 1023 W. Main St. Concept Plan

DATE: October 8, 2020

I. APPLICATION INFORMATION:

Project Name: 1023 W. Main St.

Applicant: Mohammed Shahid Ali

Purpose: Obtain feedback on a Concept Plan for redevelopment of a gas station

General Information:

Site Information		
Location	Southeast corner of W. Main St. and S. 11 th St.	
Acres 7,557 sf / .1735 acres		

Application:	Concept Plan
Applicable City Code	Ch. 17.06 – Design Review Standards & Guidelines Ch. 17.14 – Business & Mixed Use Districts
Sections	Ch. 17.20 – Use Standards Ch. 17.26 – Landscaping & Screening

Existing Conditions		
Land Use	Gas Station (currently not operational)	
Zoning RT-2 Traditional Single Family Residential		

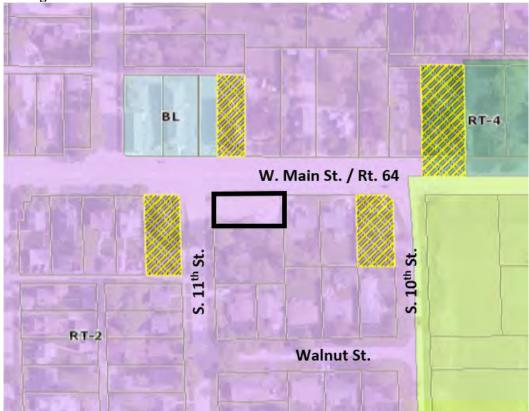
Zoning Summary			
North	BL Local Business; RT-2 Traditional Single Family Residential w/ BT Transitional	Prairie State Legal Services; single-family home	
	Business Overlay	single-family nome	
East	RT-2 Traditional Single Family Residential	Single-family home	
South	RT-2 Traditional Single Family Residential	Single-family home	
West	RT-2 Traditional Single Family Residential w/	State Farm insurance office	
	BT Transitional Business Overlay		

	Comprehensive Plan Designation
Neighborhood Commercial	

Aerial



Zoning



II. OVERVIEW

A. BACKGROUND / HISTORY

The subject property is located at the southeast corner of W. Main St. and S. 11th St.

A gas station has operated on the property for several decades. Sanborn Insurance Maps from the 1920s and aerial imagery from the 1930s appear to show a service station on the site. Based on this information, it can be assumed the gas station use was established prior to adoption of the City's 1960 Zoning Ordinance. City zoning records prior to 1960 are incomplete. In 1960, the property was zoned R2, a single-family residential district, at which point the gas station use was classified as nonconforming.

The property's zoning has remained single-family residential to present day. In 1980, a petition to rezone the property to a commercial district in order to allow expansion of the gas station was denied due to concerns about intensifying the use by adding a canopy. In 1986, a variance was requested to allow a canopy, which was denied due to the property's nonconforming status and the provision that a nonconforming use cannot be expanded.

Earlier this year, the current property owner had three underground storage tanks removed from the property. The gas pumps were also removed. A 448 sf convince store building remains on the property but is not currently operating.

B. PROPOSAL

Mohammed Shahid Ali, officer of the trust that owns the property, is seeking feedback on a Concept Plan to redevelop a gas station on the subject property. As part of the Concept Plan, the applicant is proposing rezoning to a commercial district and establishment of a Planned Unit Development (PUD) to address the issue of the nonconforming use and several zoning deviations, many of which currently exist.

Details of the proposal are as follows:

- Rezone the property to the BL Local Business District and establish a Planned Unit Development (PUD).
- Retain existing site access points to Main St. and 11th St.
- Construct a new 1,434 sf convenience store with an upper level dwelling unit in the general location of existing convenience store.
- Construct a gas station canopy with three fuel pump stations.

C. CONCEPT PLAN REVIEW PROCESS

The purpose of the Concept Plan review is to enable the applicant to obtain informal input on a concept prior to spending considerable time and expense in the preparation of detailed plans and architectural drawings. The Concept Plan process also serves as a forum for citizens and owners of neighboring property to ask questions and express their concerns and views regarding the potential development. Following the conclusion of the Concept Plan review, the developer can decide whether to formally pursue the project.

III. ANALYSIS

A. COMPREHENSIVE PLAN

The Land Use Plan adopted as part of the 2013 Comprehensive Plan identifies the subject property as "Neighborhood Commercial", along with the remainder of the Main St. frontage on the block and the block across Main St.

The Neighborhood Commercial land use category is described in the plan as follows (p. 46):

Areas designated as neighborhood commercial are



intended toward smaller-scale retail and service commercial areas geared toward providing for the daily shopping, service, and convenience needs of surrounding neighborhoods. Uses in the neighborhood commercial areas should be of a scale and intensity to be considered generally compatible with adjacent and nearby residential uses. Grocery stores, gasoline service stations, pharmacies, personal and financial services, smaller office uses, convenience and specialty retailers, and more are appropriate...Many neighborhood commercial properties, especially those along Main Street are relatively shallow and present challenges for redevelopment. Because many of the properties along this corridor are adjacent to residential areas, buffering, screening, and setbacks should be used to protect adjacent residential neighborhoods. Hours of operation and intensity of use may also become an important issue in some neighborhood commercial areas.

The Commercial Areas Framework Plan on p.51 provides additional guidance for the different commercial areas by designating appropriate business and commercial types and activity levels along key corridors within the City. The subject property is noted as Neighborhood Commercial with residential character, to promote development that fits with the adjacent residential properties along that portion of Main St.

The Plan provides the following Commercial Area land use policies which are relevant to the proposed gas station redevelopment: (p. 48-50):

Promote a mix of attractive commercial uses along the Main Street Corridor that provide a range of goods and services to the St. Charles community. A wide range of commercial uses exist along the Main Street corridor, providing a variety of goods and services to residents. As a primary east-west route through the City, Main Street contributes to the overall character, image, and appearance of St. Charles. In general, some commercial areas are newer, well maintained, well occupied, provide a desirable mix of uses, and are generally considered attractive. Others however, are older/dated, suffer from deferred maintenance and obsolescence, and suffer from a less desirable mix of uses and higher vacancy rates. The City should continue to promote reinvestment along this key commercial corridor and maintain Main Street as a unique commercial corridor that can accommodate a wide array of business types to cater to the diverse needs of the St. Charles community.

To the extent possible, mitigate the negative effects of commercial and industrial uses on adjacent and nearby residential properties through use of setbacks, screening, buffers, orientation of activity, and more. The composition of the City's commercial districts along corridors that transect the City means that there are many areas where commercial uses abut residential properties and neighborhoods. The use of horizontal and vertical buffering and screening, including berms, fencing, and landscaping, should be promoted to protect neighborhoods from abutting commercial or industrial land uses. The City should identify areas where land use conflicts are problematic and explore solutions to mitigate the conflicts, including buffering and screening.

Improve access management along the City's commercial corridors. As a community approaching full build out, the commercial areas of St. Charles are well defined—located along the City's arterial corridors. In some areas, incremental commercial development has resulted in poor access management. Along Main Street and Randall Road, many individual businesses have established one or more driveways located within close proximity to one another. This can be problematic with regards to traffic and pedestrian safety and traffic flow. The City should work with IDOT and KDOT, as well as property owners, to improve access management within corridor commercial areas in order to improve traffic flow and safety. Along these commercial corridors, the City should work to minimize curb cuts, consolidate the access points, and facilitate cross-access easements and shared parking agreements between adjacent properties. These improvements would serve to increase safety for motorists, pedestrians, and bicyclists by minimizing points of conflict and creating predictability for the location and frequency of ingress and egress.

B. **ZONING REVIEW**

Gas Station Use – Gas Station is defined in Ch. 17.30 as follows:

An establishment offering for sale at retail to the public, fuels, oils and accessories for motor vehicles, which may also offer convenience goods such as food, beverages, and other items typically found in a convenience market.

The subject property is zoned RT-2 Traditional Single-Family Residential. Gas Station is not a permitted use in the RT-2 District. As described in the Background section on page 3, it appears the gas station use was established on the property prior to adoption of the 1960 Zoning Ordinance, perhaps as early as the 1920s. The property was zoned R2 Single-Family Residence District in 1960, followed by rezoning to RT-2 Traditional Single-Family Residential when the current Zoning Ordinance was adopted in 2006.

Because the use was established prior to zoning regulations, it was grandfathered in as a legal, nonconforming use. Under Ch. 17.08 "Nonconformities", a use which existed lawfully prior to adoption of the Zoning Ordinance but became nonconforming upon adoption of the Zoning Ordinance, may continue subject to certain restrictions. Nonconforming uses cannot be expanded, enlarged or increased in intensity. This provision has prevented a canopy from being constructed on the subject property, as has been requested by previous property owners; the addition of a canopy would be considered an intensification of the use.

As previously mentioned, the underground fuel storage tanks and fuel pumps were removed earlier this year. When a nonconforming use becomes vacant and remains unoccupied for 180 days (6 months) or more, the nonconforming use shall be deemed to be abandoned, and cannot be reestablished. Any subsequent use of the property must comply with zoning regulations. The City is not aware of the exact date the business ceased operations. However, given the fact that the gas station facilities were removed and the gas station has not been

operational for a number of months, it is unlikely the City would be able to allow the use to be reestablished without further zoning approval.

<u>Proposed Zoning</u> – In order to permit redevelopment of a gas station on the property, approval of either a Map Amendment to rezone the property to a zoning district in which Gas Station is a permitted use and/or a Planned Unit Development is required.

The Concept Plan contemplates rezoning to the BL Local Business District. The purpose of the BL District provided in the Zoning Ordinance is as follows:

To provide locations for small-scale service and retail uses that primarily serve the convenience needs of St. Charles neighborhoods. The BL District permits a mix of uses, but care must be taken to ensure that adequate access, parking and screening is provided so as not to negatively impact adjoining residential neighborhoods.

The BL District is generally located along Main St. between downtown and larger scale commercial properties further east and west of downtown. Property zoned BL is located across Main St. from the subject property. The BL District is also compatible with the Neighborhood Commercial land use category identified for the property in the Comprehensive Plan. However, Gas Station is not a permitted or special use in the BL District (although gas station is called out as appropriate for Neighborhood Commercial areas in the Comprehensive Plan). Gas Station is only permitted in the BC Community Business and BR Regional Business districts. However, given the location of the property, its Comprehensive Plan land use designation, and adjacent zoning, BC and BR zoning would not be appropriate.

The Concept Plan also proposes a Planned Unit Development (PUD) for the property in order to establish unique zoning standards for the site, including permitting a Gas Station in the BL District and approving deviations from the bulk standards of the BL District. Many aspects of the physical development of the site, as well as the gas station use itself, are nonconforming, including building and paving setbacks and lack of landscape buffer yard. A PUD ordinance would document approval of these deviations; the physical layout of the site and the gas station use would no longer be considered nonconforming.

Also proposed is to construct a new convenience store with a second-story dwelling unit. An Upper-Level Dwelling is a permitted use in the BL District.

<u>Bulk Standards</u> – The table below compares the proposed BL District zoning standards with the Concept Plan. A number of zoning deviations, denoted in *bold italics*, would need to be approved through a PUD in order to allow the development as proposed.

	BL District	Concept Plan
Min. Lot Area	Upper-Level Dwelling: 3,000 sf/unit Other uses: no min. lot area	7,557 sf
Min. Lot Width	None	60 ft. (11 th St.)
Max. Building Coverage	60%	19%
Max. Gross Floor Area per Building	10,000 sf	2,868 sf
Max. Building Height	30 ft.	30 ft.

	T 11 11 200	
	Building: 20 ft.	Building: 49 ft.
Min. Front Yard	Parking: 10 ft.	Parking: 0 ft.
	(11 th St.)	Furking. Oji.
Min. Exterior Side	Building: 20 ft.	Building: 40 ft.
Yard	Parking: 10 ft.	
raru	(Main St.)	Parking: none
Min. Interior Side	Building: 5 ft.	Puilding, 1 ft
Vard	Parking: none	Building: 1 ft.
Y aru	(south lot line)	Parking: none
	Building: 20 ft.	Puilding, 2.6
Min. Rear Yard	Parking: none	Building: 2 ft.
	(east lot line)	Parking: none
	10 ft. along lot lines abutting/across a	
Landscape Buffer	street from residential zoning	None
Yard	(applies to all sides, excluding portion	None
	of north lot line across from BL zoning)	
	Gas Station: 4 spaces per 1,000 sf of	
	GFA, reduced by # of pumps	
Off Stuast Banking	(3 spaces required - 6 pumps = 0	3 spaces
Off-Street Parking	required spaces for gas station)	
	Dwelling Unit: 1 space for 1 bedroom;	
	2 spaces for 2 bedrooms	
Refused Dumpster	20 ft. from 11 th St.	Approx. 1 ft. from 11 th
Setback	3 ft. from south lot line	St. and south lot line

C. USE STANDARDS

Gas Stations are subject to the use standards contained in Section 17.20.030, listed below. Comments on whether the Concept Plan complies with each standard are noted in italics. Standards for fuel pump and canopy setbacks are not met.

- 1. Restaurants in gas stations shall be required to meet the parking requirements for restaurants in addition to those for gas stations.
 - N/A; a restaurant has not been identified as part of the convenience store.
- 2. Fuel pumps shall be located no closer than 20 feet from any lot line and shall be located so that a vehicle using the fuel pump does not encroach into the public right of way or onto adjoining property
 - Fuel pumps are located 16 ft. from the Main St. lot line and 26 ft. from the 11th St. lot line.
- 3. Gas station canopies shall be subject to the lighting standards of Section 17.22.040 (Site Lighting). Gas station canopies shall also meet all applicable setback requirements for the principal building.
 - The proposed canopy lighting will need to meet the standards of Section 17.22.040.
 - The canopy is set back 6 ft. from the Main St. lot line; a 10 ft. setback is required.
- 4. The provisions hereof relating to Outdoor Sales shall apply if Outdoor Sales are included.
 - Outdoor sales have not been identified. However, per Section 17.20.030, outdoor sales accessory to gas stations are permitted provided the sales area is limited to 30 sf multiplied by the number of pumps on the lot. The sales area(s) can only be located within the pump islands or on a sidewalk adjoining the building.

D. LANDSCAPING

A landscape plan will be required with a future PUD Preliminary Plan. Currently, no landscaping exists on the property. Based on the site plan, no landscaping is proposed to be added and none of the applicable standards of Ch. 17.26 "Landscaping & Screening" are met. PUD deviations from these standards would need to be granted to allow the development as proposed.

Category	Zoning Ordinance Standard	Concept Plan
Overall Landscape Area	15%	None
Public Street Frontage Landscaping	75% of street frontage 1 tree / 50 ft. of street frontage (Main St: 3 trees 11 th St.: 1 tree)	None No trees
Parking Lot Screening	50% of parking lot to height of 30"	None
Building Foundation Land	scaping	
Foundation Planting Beds	50% of total building walls; 50% of walls facing a public street (Main St. & 11 th St.)	None
Foundation Plantings	5 ft. wide planting beds 20 shrubs/bushes/perennials per 50 ft. of planting bed	None
Foundation Trees	2 trees per 50 ft. of planting bed	None
Monument Sign Landscaping	3 ft. around sign	None
Retaining Wall	For retaining walls over 4 ft. high, terrace to provide planting area	Height of wall TBD
Refuse Dumpster Screening	Enclosed and screened on all sides when visible from public street	TBD
	10 ft. along all lot lines (excluding portion of north lot line across from BL zoning):	None
Landscape Buffer Yard	 Opaque, year-round screening via berming, landscaping, or fencing to a height of 6 ft. 1 shade tree or 2 evergreen trees per 400 sf of required buffer area + variety of other plantings 	8 ft. fence proposed along south lot line, however 10 ft. buffer not provided

Staff Comments

• Opportunities for incorporating landscaping on the site plan are limited due to the size of the property. However, effort should be made to add landscaping along the street frontages if at all possible. Increasing landscaping from current conditions could be considered a benefit of approving a PUD for the site. It may be possible to incorporate landscaping if the Main St. curb cuts are reduced in number and/or width. Planters could be incorporated along the building walls, if the accessibility code allows. Landscaping could also be added around the freestanding sign, and perhaps at the northwest corner of the property.

D. BUILDING ARCHITECTURE

New buildings in the BL District are subject to the Design Review requirements of Section 17.06.030 Standards and Guidelines – BL, BC, BR, & O/R Districts. A rendering of the

convenience store has been submitted. The design, particularly for the second floor, has a residential-like scale and appearance which promotes compatibility with surrounding residential structures. It appears that the building design has the ability to comply with applicable design standards.

E. SIGNAGE

Signage is not shown on the building rendering. Two wall signs are permitted, one for each street frontage. The site plan indicates a 15 ft. freestanding sign at the northeast side of the property, utilizing the existing sign base. A 10 ft. ROW setback is required. The existing sign base is set back approx. 7 ft. from the ROW. Maximum permitted sign area is 100 sf and maximum sign height is 15 ft.

F. SITE ACCESS

The subject property contains three access points: two from Main St. and one from 11th St. The width of these access points exceeds current standards for two-way traffic, which is 24 ft.

Staff Comments:

- In connection with the site's redevelopment, it would be preferred if one of the Main St. access points were closed. The western Main St. driveway is very close to the corner. The fire truck and fuel truck turning exhibits included in the Concept Plan do not depict that driveway as necessary for circulation. Removing that driveway would likely improve vehicular safety and improve the appearance of the site, perhaps allowing for some incorporation of street frontage landscaping.
- Driveway width should be reduced if possible.
- As part of a future PUD application, a Traffic Impact Study analyzing the access points and internal vehicle circulation should be provided.
- An IDOT permit will be required for any work within the Main St. (State Rt. 64) right-of-way. There is potential that IDOT could require changes to the driveways as a condition of a permit.

G. ENGINEERING REVIEW

The Concept Plan has been reviewed on a preliminary basis to identify any significant issues concerning feasibility of the project. No "red flags" have been identified.

IV. FUTURE APPROVAL PROCESS

If the applicant chooses to move forward with the proposed development at the conclusion of the Concept Plan process, the following zoning/subdivision approvals would be necessary:

- 1. Zoning Map Amendment: To rezone the property from RT-2 to BL.
- 2. Special Use for PUD: To establish a PUD ordinance with unique zoning standards to accommodate the proposal.
- 3. PUD Preliminary Plan: To approve the physical development of the property, including site, engineering, and landscape plans.

V. SUGGESTED ACTION

Review the Concept Plan and provide comments to the applicant. Staff recommends the Committee provide feedback on the following:

- ✓ Whether continued use of the property as a gas station and addition of residential unit are appropriate.
- ✓ Proposed BL zoning designation.
- ✓ Site layout and access.
- ✓ Building design.
- ✓ Whether a PUD appropriate for this project. Would a PUD advance one or more of the purposes of the PUD procedure?
 - 1. To promote a creative approach to site improvements and building design that results in a distinctive, attractive development that has a strong sense of place, yet becomes an integral part of the community.
 - 2. To create places oriented to the pedestrian that promote physical activity and social interaction, including but not limited to walkable neighborhoods, usable open space and recreational facilities for the enjoyment of all.
 - 3. To encourage a harmonious mix of land uses and a variety of housing types and prices.
 - 4. To preserve native vegetation, topographic and geological features, and environmentally sensitive areas.
 - 5. To promote the economical development and efficient use of land, utilities, street improvements, drainage facilities, structures and other facilities.
 - 6. To encourage redevelopment of sites containing obsolete or inappropriate buildings or uses.
 - 7. To encourage a collaborative process among developers, neighboring property owners and residents, governmental bodies and the community
- ✓ Would the identified PUD deviations be warranted? To grant PUD deviations, the City Council will need to find that:
 - a. Conforming to the requirements would inhibit creative design that serves community goals; OR
 - b. Conforming to the requirements would be impractical and the proposed PUD will provide benefits that outweigh those that would have been realized by conforming to the applicable requirements.

Factors to be considered in this determination shall include, but are not limited to the following:

- 1. The PUD will provide community amenities beyond those required by ordinance, such as recreational facilities, public plazas, gardens, public art, pedestrian and transit facilities.
- 2. The PUD will preserve open space, natural beauty and critical environmental areas in excess of what is required by ordinance or other regulation.
- 3. The PUD will provide superior landscaping, buffering or screening.
- 4. The buildings within the PUD offer high quality architectural design.
- 5. The PUD provides for energy efficient building and site design.
- 6. The PUD provides for the use of innovative stormwater management techniques.
- 7. The PUD provides accessible dwelling units in numbers or with features beyond what is required by the Americans with Disabilities Act (ADA) or other applicable codes.
- 8. The PUD provides affordable dwelling units in conformance with, or in excess of, City policies and ordinances.
- 9. The PUD preserves historic buildings, sites or neighborhoods.

VI. ATTACHMENTS

- Concept Plan Application; received 9/3/2020
- Concept Plan
- Letters from Neighboring Property Owners

CITY OF ST. CHARLES

TWO EAST MAIN STREET ST. CHARLES, ILLINOIS 60174-1984



COMMUNITY DEVELOPMENT DIVISION

PHONE: (630) 377-4443 EMAIL: cd@stcharlesil.gov

CONCEPT PLAN APPLICATION

CITYVIEW

Project Name:

1023 W. Main St.

Project Number:

2020 -PR- 013

Cityview Project Number:

PLCPDUDO 00044

Received Date

RECEIVED St. Charles, IL

SEP 0 3 2020

--cdb

Planning Division

To request review of a Concept Plan for a property, complete this application and submit it with all required attachments to the Planning Division.

When the application is complete and has been reviewed by City staff, we will schedule a Plan Commission review, as well as a review by the Planning and Development Committee of the City Council. While these are not formal public hearings, property owners within 250 ft. of the property are invited to attend and offer comments.

The information you provide must be complete and accurate. If you have a question please call the Planning Division and we will be happy to assist you.

1.	Property Information:	Location: 1023 West Main st, St. Charles IL 60174	
AND THE PROPERTY OF THE PROPER		Parcel Number (s): 09-33-203-001	
		Proposed Project Name: 1023 W. Main St Redevelop	oment
2.	Applicant Information:	Name Mohammed Shahid Ali	Phone 708-997-6799
		Address 201 Lilac st, Bolingbrook IL- 60490	Fax
		201 Eliao St, Bollingbrook IE Go 400	Email mshahidali@hotmail.com
3.	Record Owner	Name Hamza Jehangir Ali 2014 Trust	Phone 708-997-6799
	Information:	Address	Fax
		201 Lilac st, Bolingbrook IL- 60490	Email mshahidali@hotmail.com

Please check the type of application:

PUD Concept Plan:	Proposed Name:	1023 W. Main St. Redevelopment
☐ Subdivision Concept Plan	Proposed Name:	
Other Concept Plan		
Zoning and Use Information:		
Current zoning of the property:	RT-2 Traditional Single I	⁼ amily
Is the property a designated Landmark	or in a Historic District? No	
Current use of the property:	Gas Station and Conviniend	ce store
Proposed zoning of the property:	BL Local Business	PUD? Yes
Proposed use of the property:	Gas Station and Convinie	ence store w/ Office use above
Comprehensive Plan Designation:	Local Business District/C	Commercial

Attachment Checklist

REIMBURSEMENT OF FEES AGREEMENT:

An original, executed Reimbursement of Fees Agreement and deposit of funds in escrow with the City, as provided by Appendix B of the Zoning Ordinance.

REIMBURSEMENT OF FEES INITIAL DEPOSIT:

Deposit of funds in escrow with the City. Required deposit is based on review items (number of applications filed) and the size of the site:

Number of Review Items	Under 5 Acres	5-15 Acres	16-75 Acres	Over 75 Acres
1	\$1,000	\$2,000	\$3,000	\$4,000
2 or 3	\$2,000	\$4,000	\$5,000	\$7,000
4 or more	\$3,000	\$5,000	\$7,000	\$10,000

PROOF OF OWNERSHIP and DISCLOSURE:

- a) a current title policy report; or
- b) a deed and a current title search.

If the owner is not the applicant, an original letter of authorization from the owner permitting the applicant to act on his/her behalf is required. If the owner or applicant is a Trust, a disclosure of all beneficiaries; if the owner or applicant is a Partnership, a disclosure of all partners; if the owner or applicant is a Corporation, a disclosure of all owners with an interest of at least ten percent (10%).

NOTE: Private covenants and deed restrictions can limit private property rights with respect to the use of land even though the City's Zoning Ordinance may authorize the use or a less restrictive use. We strongly advise that

you perform a title search on the property to determine if there any private covenants containing use restrictions or other deed restrictions. As those private covenants and deed restrictions may conflict with the City's Zoning Ordinance, it is further recommended that you consult with an attorney to obtain an opinion with respect to whether your intended use is compatible with those restrictions.

LEGAL DESCRIPTION: For entire subject property, on 8 ½ x 11 inch paper

⋈ PLAT OF SURVEY:

A current plat of survey for the Subject Realty showing all existing improvements on the property, prepared by a registered Illinois Professional Land Surveyor.

AERIAL PHOTOGRAPH:

Aerial photograph of the site and surrounding property at a scale of not less than 1"=400', preferably at the same scale as the concept plan.

PLANS:

All required plans shall be drawn on sheets no larger than 24" x 36", unless the Director of Community Development permits a larger size when necessary to show a more comprehensive view of the project. All required plans shall show north arrow and scale, and shall be drawn at the same scale (except that a different scale may be used to show details or specific features). All plans shall include the name of the project, developer or owner of site, person or firm preparing the plan, and the date of plan preparation and all revisions. A pdf document file or files of all plans shall be required with each submittal. The number of paper plans required shall be as determined by the Director of Community Development, based upon the number of copies needed for review.

Copies of Plans:

Initial Submittal - Ten (10) full size copies for non-residential projects OR Twelve (12) full size copies for residential projects; Three (3) 11" by 17"; and a PDF electronic file (On a CD-ROM or may be emailed to the Project Manager). For subsequent submittals, please contact the Project Manager to determine how many copies are required.

Concept Plans shall show:

1. Existing Features:

- Name of project, north arrow, scale, date
- Boundaries of property with approximate dimensions and acreage
- Existing streets on and adjacent to the tract
- Natural features including topography, high and low points, wooded areas, wetlands, other vegetative cover, streams, and drainage ways.
- General utility locations or brief explanation providing information on existing sanitary sewer, storm sewer, water, and other utilities necessary to service the development.

2. Proposed Features:

- Name of project, north arrow, scale, date
- Boundaries of property with approximate dimensions and acreage
- Site plan showing proposed buildings, pedestrian and vehicular circulation, proposed overall land use pattern, open space, parking, and other major features.
- Architectural elevations showing building design, color and materials (if available)
- General utility locations or brief explanation providing information on existing sanitary sewer, storm sewer, water, and other utilities necessary to service the development

▼ SUMMARY OF DEVELOPMENT:

Written information including:

- List of the proposed types and quantities of land use, number and types of residential units, building
 coverage, floor area for nonresidential uses and height of proposed buildings, in feet and number of
 stories.
- Statement of the planning objectives to be achieved and public purposes to be served by the development, including the rationale behind the assumptions and choices of the applicant
- List of anticipated exceptions or departures from zoning and subdivision requirements, if any

PARK AND SCHOOL LAND/CASH WORKSHEETS Not applicable

For residential developments, Park and School land/eash worksheets in accordance with Title 16 of the St. Charles Municipal Code with population projections establishing anticipated population and student yields.

INCLUSIONARY HOUSING SUMMARY: For residential developments, submit information describing how
the development will comply with the requirements of Title 19, "Inclusionary Housing" of the St. Charles
Municipal Code. Not applicable

X LIST OF PROPERTY OWNERS WITHIN 250 FT.

Fill out the attached form or submit on a separate sheet. The form or the list must be signed and notarized.

I (we) certify that this application and the	documents submitted with it are true and	correct to the best of my (our)
knowledge and belief.		
I (we) certify that this application and the knowledge and belief.	08/20/2020	
Record Owner	Date	
Applicant or Authorized Agent	Date	

OWNERSHIP DISCLOSURE FORM LAND TRUST

STATE OF ILLINOIS) NIL) SS. KANE COUNTY)
I, Mohammed Shahid Ali , being first duly sworn on oath depose and say that I am Trust Officer of Hamza Jehangir Ali 2014 Trust , and that the following
persons are all of the beneficiaries of Land Trust No:
Hamza Jehangir Ali
Hali Tali 8/20/2020 By: Mohammed Shahid Ali, Trust Officer
Subscribed and Sworn before me this day of
Motary Public
JORGE E MARTINEZ Official Seal Notary Public - State of Illinois

City of St. Charles Ownership Disclosure Forms

My Commission Expires Nov 20, 2021

<u>Summary for Development</u> for Gas Station Project at 1023 West Main Street, St. Charles, IL 60174

To: City of St. Charles c/o Ellen Johnson (City Planner) 2 East Main Street St. Charles, IL 60174

To Whom It May Concern:

On behalf of the Applicant, Mohammed Shahid Ali, and the Owner, Hamza Jehangir Ali 2014 Trust, please accept this request for a Concept Plan Review regarding a change to the existing Clark gas station property at 1023 West Main Street. The existing site is 7,557 SF (0.1735 acres). The property is currently zoned RT-2 and located at the southeast corner of South 11th Street and West Main Street (IL-64).

The existing gas station abuts a residence to the east and south of the property. It is across the street from commercial uses to the west and north. There is also a residence across the street to the north. The existing gas station has been in the ownership of Mohammed Shahid Ali and Hamza Jehangir Ali established since 2015.

The scope of the new project includes demolishing the existing 470 SF convenience store. The previous two gas pumps and underground tanks have recently been removed following State guidelines. A new 2-story 2,868 SF building (30'-0" tall maximum building height) with convenience store on the first level and a single family residential unit on the second floor is proposed to be constructed in the same general area as the previous building. In addition, three gas pumps, a new underground storage tank, canopy and parking area will be constructed on the existing site. Lastly, a new freestanding electronic pricing sign will be installed in the same northeast corner location as the existing non-illuminated pricing sign. The hours of operation will be 5am – 11pm and the brand for the gas station will continue to be Clark.

The work will require a new PUD approval with rezoning from RT-2 to BL district. The BL district is "to provide locations for small-scale service and retail uses" in neighborhoods. As this is already a gas station, the Owner believes it is most advantageous to maintain the gas station use and improve the amenities for the end user with a store that is more than two times larger, has an additional pump and has a new canopy that provides protection from weather with better lighting. The location off IL-64 continues to make this a prime location for a gas station and C-store in an area that is largely residential. Consequently, the larger convenience store will better serve the public, especially the local neighborhood.

The new building will cover 16.8% of the property. 60% maximum building coverage is allowed. The amount of parking for a gas station and office use will be compliant with the zoning ordinance. The requirements for parking with proposed uses and square footages are five for the gas station (which can include parking at the pump) and three for the dwelling unit. There will be six spaces at the pumps

and four spaces at the southwest corner of the property. The lighting requirements for gas station canopies will be met as the Owner is sensitive to the effects of lighting on the residential neighbors. The traffic flow will remain the same as the former operating station – one way in from 11th Street and Main Street at existing curb cuts, one way out to Main Street at the existing east curb depression.

There are several exceptions, however, that are requested as part of the new PUD that are itemized in the Exhibits. These exceptions include:

- Request to add GAS STATION use as a permitted use in BL district.
- Request for various setback revisions from the standard BL district requirements such as building setbacks, fuel pump setback, parking setback and canopy setback.
- Request to eliminate all landscaping requirements such as amount of landscaping, location of landscaping along building and street frontages and landscaping buffers.
- Request to locate new sign in the same location which may be too close to the right of way per BL zoning district ordinance requirements.

We look forward to discussing this project further with staff. Thank you for your time.

Sincerely,

Eric Carlson

President, ECA Architects & Planners

Cc: Mohammed Shahid Ali – Applicant

APPLICATION FOR PUD for Gas Station Project at 1023 West Main Street, St. Charles, IL 60174

EXHIBIT A

To: City of St. Charles c/o Ellen Johnson (City Planner) 2 East Main Street St. Charles, IL 60174

Subject Property Address:

1023 West Main Street

Legal Description:

THE NORTH 60 FEET OF LOTS 4 AND 9 IN BLOCK 5 OF MORRISON'S ADDITION TO ST. CHARLES; IN THE CITY OF ST. CHARLES, KANE COUNTY, ILLINOIS.

Parcel No:

09-33-203-001

Lot Size:

0.1735 Acres / 7,557 SF

Current Zoning District:

RT-2

Proposed Zoning District:

BL

Gas Station Preliminary Site Plan

1023 W. Main Street

St. Charles, IL 60174

IF PRINTED TO SCALE, BOTH THESE

DRAWN BY: SBD/ERC DATE: 08-21-2020 REV 1: REV 2: REV 3:

Existing Aerial Site Photo SHEET

EX-0.1

EXISTING AERIAL SITE PHOTO SCALE: N.T.S.







BUILDING FOOTPRINT: NEW 1434 SF / EXISTING 470 SF (Table 17.14-2) MAX BUILDING COVERAGE = 60% PROPOSED 1434 SF/7557 SF = 19.0% NEW SECOND FLOOR RESIDENTIAL USE: 1290 SF (Table 17.14-2) HEIGHT OF BUILDING TO BE 30'-0" MAX (Table 17.24-3) PARKING SPACES FOR GAS STATION & OFFICE USES: 1 PER SERVICE BAY + 4 PER 1000 SF GEA (REDUCE BY # FUEL PUMPS) OFFICE 3 PER 1000 SF GFA REQUIRED: 5 (GAS STATION) + 3 (OFFICE) PROVIDED: 6 @ PUMP + 4 SPOTS (1 ADA)

(Table 17.28-2) SIGNAGE:

- REPLACE EXISTING FREESTANDING SIGN IN NORTHEAST CORNER W/ NEW FREESTANDING ELECTRONIC PRICING SIGN IN SAME LOCATION
- MAXIMUM HEIGHT 15' TO TOP OF SIGN
- 1.5 SF PER LINEAR FRONTAGE OF THE BUILDING OR 100 SF. WHICHEVER IS LESS

FRONTAGE OF BUILDING = 70 LINEAR FEET 70 x 1.5 = 105 SF THEREFORE, 100 SF ALLOWED ZONING NOTES FOR CONCEPT PLAN SUBMITTAL

NON-CONFORMING ITEMS OF EXIST. PROPERTY AS BL DISTRICT:

- (Section 17.20.030-I) FUEL PUMP DOES NOT MEET FRONT SETBACK REQ'MT
- (Table 17.14-2) BUILDING ENCROACHES SETBACKS
- (Table 17.14-2) PARKING ENCROACHES INTERIOR SIDE SETBACK
- VARIOUS LANDSCAPING SETBACKS/REQ'MTS ARE NON-COMPLIANT

(Section 17.26.060) MINIMUM 15% OF LOT TO HAVE LANDSCAPING FOR PROPERTIES W/ OFF-SITE STORMWATER STORAGE (15%x7557 = 1133 SF, CURRENT MULCH BED IS 950 SF)

(Section 17.26.070 &Table 17.14-2) 10' LANDSCAPE BUFFER YARD REQ'D ALONG ANY LOT LINE THAT ABUTS OR IS ACROSS THE STREET FROM RT DISTRICT (PARTIAL COMPLIANCE ON SOUTH RESIDENTIAL USE, NO COMPLIANCE ON EAST RESIDENTIAL USE, NO COMPLIANCE TO NORTH COMMERCIAL & RESIDENTIAL USE, NO COMPLIANCE TO WEST COMMERCIAL USES)

(Section 17.26.080) NO BUILDING FOUNDATION LANDSCAPING, WALKWAY/CONC ALL SIDES OF BUILDING

(Section 17.26.110) AMOUNT OF SIGN LANDSCAPING IS DEFICIENT, NOT 3' FROM ALL SIDES

Main St. IL-64 (width varies) ∠EXIST. DEPRESSED CURB EXIST. DEPRESSED CURB -SCN is 2.00'N & 2.00'W S89°53'42"E 126.00 -SRN is at corner EXIST.-GRASS PKÍNG ⊲STOP SIGN SIDE PKING STDE EXIST. 20.0 (60)SIGN ASPHALT APRONS TO 11TH ST BLDG & PUMP EXIST. SANITARY-SIDE TO BLDG SIDE -EXIST. 60.00 NOT"38'07"W FLAGPOLE St 00.09 -ÉXIST. RAISED ń CONC. 20.0 S01°39'29 EXIST. GAS-Lot 9 " Lot 4 ∠EXIST, WATER 11thTO BLDG TO[△]BLDG EXIST. CURB Concrete is 1.14"NEO.93"W MULCH EMBANKMENT FPm is 0.16'N & 3.72'W The South line of the North 60 feet lḋing is 1.24'N N89°53'42"W -SCN on curb is 13.50'W & On line extended Air Conditioner overhang is on line 126.02' (126') Fence is 0.15'N MORRISON'S ADDITION -EXIST. RETAINING WALL TO ST. CHARLES -EXIST. OVERHEAD ELECT. Block 5 7,557 Sq. Ft. 0.1735 Acres \pm Ν SCALE: 1" = 20

24 N BENNETT ST. GENEVA, IL 60134 PHONE 630 608 0500 FAX 630 839 8875



PROFESSIONAL I NO. 184 (

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Charles, I

St.

1023 W. Main Street

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DRAWN BY: SBD/ERC
DATE: 08-21-2020
REV 1:
REV 2:
REV 3:
REV 4:

Existing Plan SHEET

EX-1

ZONING NOTES FOR CONCEPT PLAN SUBMITTAL

BUILDING FOOTPRINT: NEW 1434 SF / EXISTING 470 SF (Table 17.14-2) MAX BUILDING COVERAGE = 60% PROPOSED 1434 SF/7557 SF = 19.0% NEW SECOND FLOOR RESIDENTIAL USE: 1434 SF (Table 17.14-2) HEIGHT OF BUILDING TO BE 30'-0" MAX

PARKING SPACES FOR GAS STATION & RES. USES: (Table 17.24-3) 1 PER SERVICE BAY + 4 PER 1000 SF GFA (REDUCE BY # FUEL PUMPS)

> RESIDENTIAL (UPPER LEVEL DWELLING UNIT) 1 PER D.U.

5 (GAS STATION) + 1 (UPPER LEVEL DWELLING) PROVIDED: 6 @ PUMP + 3 SPOTS (1 ADA) (Table 17.28-2) SIGNAGE:

REPLACE EXISTING FREESTANDING SIGN IN NORTHEAST CORNER W/ NEW FREESTANDING ELECTRONIC PRICING SIGN IN SAME LOCATION

MAXIMUM HEIGHT 15' TO TOP OF SIGN

1.5 SF PER LINEAR FRONTAGE OF THE BUILDING OR 100 SF, WHICHEVER IS LESS

FRONTAGE OF BUILDING = 70 LINEAR FEET 70 x 1.5 = 105 SF THEREFORE, 100 SF ALLOWED NEW PUD REQUEST FOR: 7,557 Sq. Ft. 0.1735 Acres ±

MORRISON'S ADDITION TO ST. CHARLES Block 5

PROPO	SED NON-CONFORMING ITEMS:	REQUIRED	PROPOSED PROPOSED
(Section	on 17.20.030-I)		
_	FUEL PUMPS WILL NOT MEET FRONT SETBACK REQ'MT	20'-0"	16'-0"
-	CANOPY ENCROACHES FRONT SETBACK REQ'MT	20'-0"	6'-1"
(Table	17.14-2)		
` –	BUILDING SETBACKS		
	FRONT	20'-0"	40'-0"
	INTERIOR SIDE (ENCROACHES)	5'-0"	2'-1"
	REAR (ENCROACHES)	20'-0"	1'-1"
	EXTERIOR SIDE	20'-0"	49'-0"
-	PARKING ENCROACHES EXTERIOR SIDE PARKING SETBACK	10'-0"	1'-0"
(Table	17.28-2)		
` _	NEW FREE STANDING SIGN LOCATION FROM ROW	10'-0" E	EXIST. LOCATION
		1	NON CONFORMING
(Table	17.14-1)		

REQUEST TO ADD GAS STATION AS PERMITTED USE

PROPOSED NON-CONFORMING LANDSCAPE ITEMS:

VARIOUS LANDSCAPING SETBACKS/REQ'MTS ARE NON-COMPLIANT

- (Section 17.26.060) MINIMUM 15% OF LOT TO HAVE LANDSCAPING FOR PROPERTIES W/ OFF-SITE STORMWATER STORAGE (15%x7557 = 1133 SF) REVISION: EXISTING MULCH BED HAS BEEN REMOVED

- (Section 17.26.070 &Table 17.14-2) 10' LANDSCAPE BUFFER YARD REQ'D ALONG ANY LOT LINE THAT ABUTS OR IS ACROSS THE STREET FROM RT DISTRICT (REVISION: NO COMPLIANCE ON SOUTH RESIDENTIAL USE, SAME: NO COMPLIANCE ON EAST RESIDENTIAL USE, NO COMPLIANCE TO NORTH COMMERCIAL & RESIDENTIAL USE, NO COMPLIANCE TO WEST COMMERCIAL USES)
- (Section 17.26.080) NO BLDG FOUNDATION LANDSCAPING (WALKWAY/CONC ALL
- (Section 17.26.090) REVISION: PUBLIC STREET FRONTAGE LANDSCAPING REQ'D IN FRONT AND EXTERIOR SIDE YARDS ADJOINING R.O.W. (REQ'D IN BL DISTRICTS, NON-COMPLIANT)
- (Section 17.26.110) AMOUNT OF SIGN LANDSCAPING IS DEFICIENT (NOT 3' FROM ALL SIDES)
- (Section 17.26.120) RETAINING WALL AT SOUTH EDGE OF PROPERTY MAY BE OVER FOUR FEET TALL. THERE IS NO ROOM FOR A TERRACE/STEPPED RETAINING WALL WITH LANDSCAPING, WILL BE NON-COMPLIANT

Main St. IL-64 (width varies) REMOVE EXISTING 4EXIST. DEPRESSED CURB · LIGHT POLE. EXIST. DEPRESSED CURB 52 4 30" MAX. HT₄ IN -126 00' S89°53'42"E 20' CORNER SITE⁴ -NFW SIGN ON TRIANGLE EXISTING BASE. 10.0 Lot 9 Lot 4 FXIST. GRASS PKING TOP/SIGN AT 15' "STOP SIGN SIDE -VACCUUMS W/ 5.0 SIDE BLDG BOLLARDS (60,) SIDE 60.00' S01°39'29"E 20.0 NO1° 38°07" 60.00° 24.0 ASPHACT TO 111 BLDG & PUMP SIDE St. NEW UNDERGROUND TANK th. 9.0 52.0 22.0 2.5 18.1 1434 SF 20.0 BLDG & PUMP REAR **FOOTPRINT** 49.0 N89°53'42"W 126.02' (126') - NEW CONC. PARKING AREA (612 SF) L NEW 2-STORY BLDG (GAS STATION W/ - NEW RETAINING WALL ALONG SOUTH APARTMENT ABOVE) PROPERTY IN FRONT OF 8' TALL FENCE - REMOVE EXISTING LIGHT POLE. M



24 N BENNETT ST. GENEVA, IL 60134 PHONE 630 608 0500 FAX 630 839 8875

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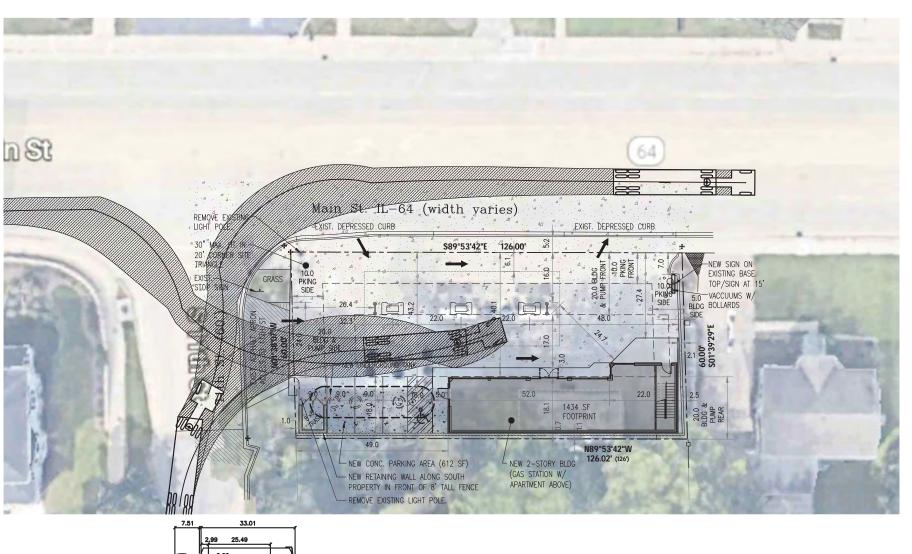
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Statio 60174 1023 W. Main Street = Charles, I S ω St. ۲η

Preliminary Site Plan

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Proposed Site Plan SHEET EX-2



Gas Station Preliminary Site Plan 1023 W. Main Street St. Charles, IL 60174

PROFESSIONAL DESIGN FIRM NO. 184 005766

IF PRINTED TO SCALE, BOTH

24 N BENNETT ST. GENEVA, IL 60134 PHONE 630 608 0500 FAX 630 839 8875

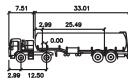
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> Circulation Plan 1 SHEET

> > EX-3

FUEL TRUCK TURNING EXHIBIT SCALE: 1" = 20'

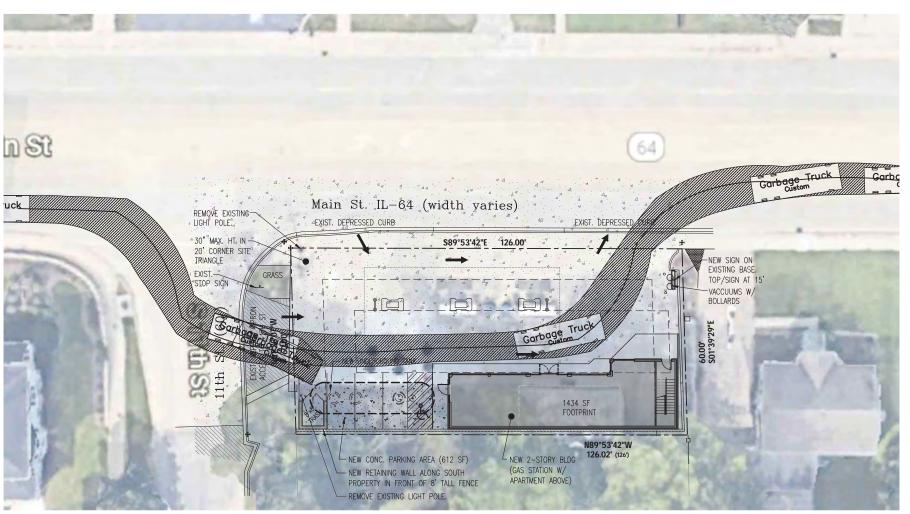
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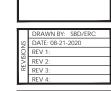


Fuel Artic

Tractor Width Trailer Width Tractor Track Trailer Track

: 6.0 : 20.3 : 70.0





Preliminary Site Plan

Gas Station

1023 W. Main Street St. Charles, IL 60174

PROFESSIONAL DESIGN FIRM NO. 184 005766

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24 N BENNETT ST. GENEVA, IL 60134 PHONE 630 608 0500 FAX 630 839 8875

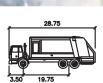
Circulation Plan	2
SHEET	

EX-4

GARBAGE TRUCK TURNING EXHIBIT

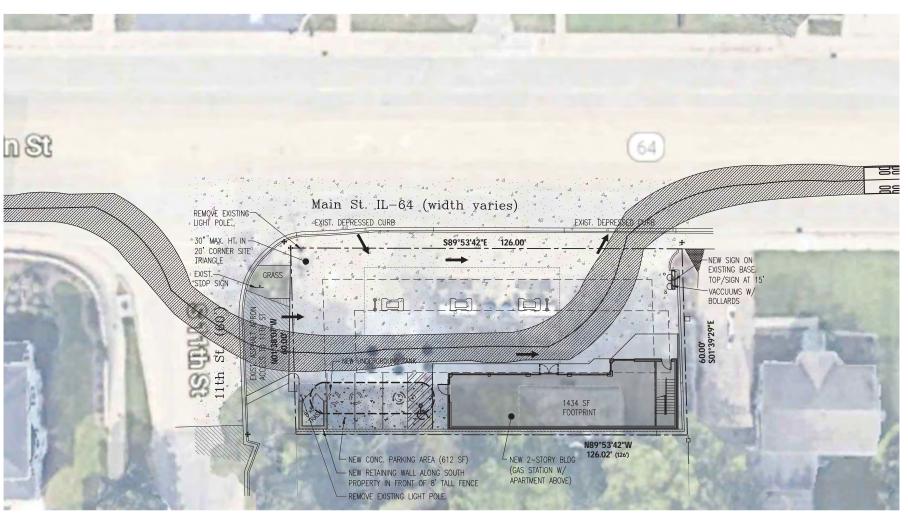
SCALE: 1" = 20'

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Garbage Truck

Width : 8.5 Track : 8.5 Lock to Lock Time : 6.0 Steering Angle : 45.0





PROFESSIONAL DESIGN FIRM NO. 184 005766

IF PRINTED TO SCALE, BOTH

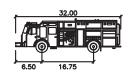
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Circulation Plan	2
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EX-5





Fire Pumper Width Track Lock to Lock Time Steering Angle : 8.50 : 8.50 : 6.0 : 45.0

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IF PRINTED TO SCALE, BOTH

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IF PRINTED TO SCALE, BOTH THESE BARS WILL MEASURE

St. Charles, IL 60174

Gas Station Preliminary Site Plan 1023 W. Main Street

DRAWN BY: SBD/ERC DATE: 08-21-2020 REV 1:

Main Street Perspective SHEET

EX-7

REV 2: REV 3: REV 4:



24 N BENNETT ST. GENEVA, IL 60134 PHONE 630 608 0500 FAX 630 839 8875

1023 W. Main Street St. Charles, IL 60174

Gas Station

Preliminary Site Plan

IF PRINTED TO SCALE, BOTH THESE BARS WILL MEASURE

DRAWN BY: SBD/ERC DATE: 08-21-2020 REV 1: REV 2: REV 3: REV 4:

Main & 11th Perspective

SHEET EX-8

From: Kathy Foulkes kffoulkes@yahoo.com Sent: Thursday, October 1, 2020 3:43 PM

To: CD <cd@stcharlesil.gov>

Subject: Project Name 1023 West Main Street

To Whom It May Concern:

We received in the mail a Notice of Public Meeting regarding the property at 1023 West Main Street. The proposal calls for the redevelopment of the property with a new gas station and convenience store with second floor residential unit.

We have no problem with a new gas station and convenience store, however, we are opposed to the second floor residential unit. We have lived in this neighborhood for 46 years and there has always been a gas station located there. We feel the second story residential unit will cause a burden in the neighborhood affecting parking. We just don't feel the second story is necessary!!!

Thank you for your consideration with this matter.

Sincerely, Michael and Kathleen Foulkes 1117 West Main Street St. Charles

Please let us know that you have received our email. Thank you.

From: Christine Farley <chrissypryor@icloud.com>

Sent: Thursday, October 1, 2020 9:42 PM

To: CD <cd@stcharlesil.gov>

Subject: No apartments on 11th and Rt 64

Hello, I'm Christine Farley, home owner off 11th Street and route 64 St. Charles. I received the notice in the mail and thank you for communicating with us neighbors about the up coming development on 11th Street and Route 64 empty lot in st. Charles. I wanted to let you know as a neighbor I agree with The gas station and store; However I disagree about the apartments. Apartments on top of a gas station store will be an eye sore for sure, I do not agree how that will fit into our community, it will also cause crowding on 11th Street due to parking.

Thank you Chrissy

Sent from my iPhone

From: Bobbi Daly <dalyb46@gmail.com> **Sent:** Friday, October 2, 2020 9:49 PM

To: CD <cd@stcharlesil.gov>

Subject: PROPOSED GAS STATION, CONVENIENCE STORE & APARTMENTS AT CORNER OF MAIN STREET

(ROUTE 64) & 11TH sT.

I received a letter regarding the proposal for the above. I am not opposed to the gas station & convenience store since those have been on this site for many years but I am opposed to constructing apartments at that location. I believe it would be too much for the surrounding neighborhood to have apartments located in that small space with very little parking availability for tenants except on the surrounding city streets. I feel that the tenants would be crammed into the apartments and they would not have the best living conditions by a gas station & convenience store.

Thank you for the letter & for the chance to participate in expressing my opinion about this proposal.

Barbara Daly 26 South 12th Street St. Charles, IL 60174 630-715-1915 dalyb46@gmail.com **From:** Linda Brink <lj.brink@yahoo.com> **Sent:** Tuesday, October 6, 2020 6:34 AM

To: CD <cd@stcharlesil.gov>

Subject: Meeting for 1023 W Main St. Project.

Commission Members,

I am unsure if I will be available to attend the meeting. I would greatly appreciate it if my statement below could be read.

My name is Linda Brink. I own the property south of the Gas Station. I am strongly opposed to the new plan. I have lived at this property for over 30 years, and have had to deal with the fumes, and extreme noise from the previous station. The noise from customer's cars was often so loud that it rattled my windows. Adding an additional pump will just make the situation worse. The fuel truck that came to fill the tanks would arrive after midnight waking everyone in my household. If this proposal goes forward, it would mean that the front of my house would face the back of someone's apartment and an 8 foot fence. Today at least, I can see some of Route 64. If the developer would like to consider purchasing my property, I would be happy to discuss it.

Thank you for you consideration, Linda Brink 630 377-8658 Sent from Mail for Windows 10 From: Lynne Ellberg < lellberg 020@gmail.com>

Sent: Monday, October 5, 2020 4:53 PM

To: CD <cd@stcharlesil.gov> Subject: 1023 W Main St

I am writing in response to the letter I was sent to me about the 1023 W Main St proposal. I am for the gas station and convenience store but not for the apartment upstairs. Is this apartment for the people running the gas station and how many bedrooms is it? Please let me know that you have received this.

Sincerely, Lynne Ellberg 1111W Main St. lellberg020@gmail.com From: Heather Gebhardt hgebhardt@comcast.net

Sent: Tuesday, October 6, 2020 3:56 PM

To: CD <cd@stcharlesil.gov>

Subject: 1023 W. Main Street Project

Dear Planning Commission;

We received the notice of Public Meeting regarding the Project: 1023 W. Main Street located on the Southeast corner of W. Main St. and S. 11th St. We are not in favor of having a gas station and convenience store in that mentioned location and we are very much object to having a second floor residential unit attached to it. The previous gas station drew foot traffic through the neighborhood along with some unsavory looking individuals. To back that up, we are of aware of at least one robbery occurring where the gas station attendant was assaulted over the synthetic marijuana (spice) that was being sold there. Along with the synthetic marijuana, the gas station also sold glass pipes. Furthermore, there is an insufficient amount of parking space at the gas station to accommodate a residence. We strongly object to having a residence above the gas station for this reason. There have been many instances of vehicles being parked on our street associated with a mechanic shop across 64. We are concerned that having a residence associated with a gas station would lead to more vehicles parked along our street; thus, impacting on our property value.

Please take our concerns into consideration.

Thank you,

Heather Gebhardt

	AGENDA	A ITEM EXECUTIVE SUMMARY	Agenda Item Number: 4c	
	Title:	St. Charles Initiative Update and Request to Approve Concept Design and Services Agreement with Serena Sturm		
CITY OF ST. CHARLES	Presenter:	Lora Vitek and the St. Charles Initiative Committee		
Meeting: Planning	g & Developm	ent Committee Date: Octol	ber 12, 2020	
Proposed Cost: \$166,729 Budgeted Amount: \$1,000,000 (construction/engineering) Not Budgeted:			Not Budgeted: □	
Executive Summary (if not budgeted please explain):				
The St. Charles Initiative is an independent advisory committee, under the umbrella of the Community Foundation of the Fox River Valley, comprised of St. Charles community leaders and residents and was formed				
to provide additional support for public projects in St. Charles. This public-private partnership generates funding				

On August 10, 2020, the St. Charles Initiative presented to the Committee a concept for the First Street Plaza expansion. The Committee provided feedback, and the St Charles Initiative would like to take this opportunity to respond to that feedback. The St Charles Initiative is also requesting a positive recommendation of approval for the concept and to advance with schematic design, the next phase of project architecture/engineering. With the completion of this phase of architecture/engineering the St Charles Initiative will be better able to undertake fundraising.

through charitable donations as a complement and/or replacement of tax-based sources.

The St Charles Initiative recommends that the City enter agreement with Serena Sturm for the Schematic Design phase, at a cost of \$166,729.

Further considerations by the Planning & Development Committee and City Council will be part of the usual development process and follow the completion of the schematic design.

Attachments (please list):

Project details and cost information Design Concept Serena Sturm proposal for design services

Recommendation/Suggested Action (briefly explain):

The St. Charles Initiative seeks approval from the Committee on the First Street Plaza design concept and for approval of the Schematic Design project phase agreement with Serena Sturm.





October 9, 2020

First Street Plaza project information for presentation to the Planning & Development Committee Meeting on Oct 12, 2020.

I. Cost estimate/Projected Revenue

A. Cost estimate for plaza expansion

\$1 million- One West Main St real estate purchase \$2.4 million- Construction estimate (1st Street Plaza expansion) \$500,000- Professional fees

Total ~ \$4 million

Note: additional pedestrian bridge estimated cost not included

B. Revenue

\$600,000 Exelon donation

\$400,000 City contribution for One West Main St purchase

\$1 million- City budget FY2020-2021 for building a river wall and filling the hole

\$94,000- County Riverboat grant from the Community Foundation

\$12,000- Concept design donation

\$25,000 - Other donations received

Total available ~\$2.31 million

Shortfall-\$1.69 million – The St Charles Initiative expects to cover this gap with donations and sponsorships.

II. Professional Services phased approach

- Schematic Design (SD) 8-10 weeks
- B. Design Development (DD)
- C. Construction documents (CD)
- D. Bidding (BD)
- E. Construction (CA)—summer 2021 with phase 1 construction

Next expenditure – Schematic Design phase includes collection of basic site data, project feasibility and lay the foundation for future phases. Proposal value-\$166,729.

III. Response to feedback from the August 10 P&D meeting

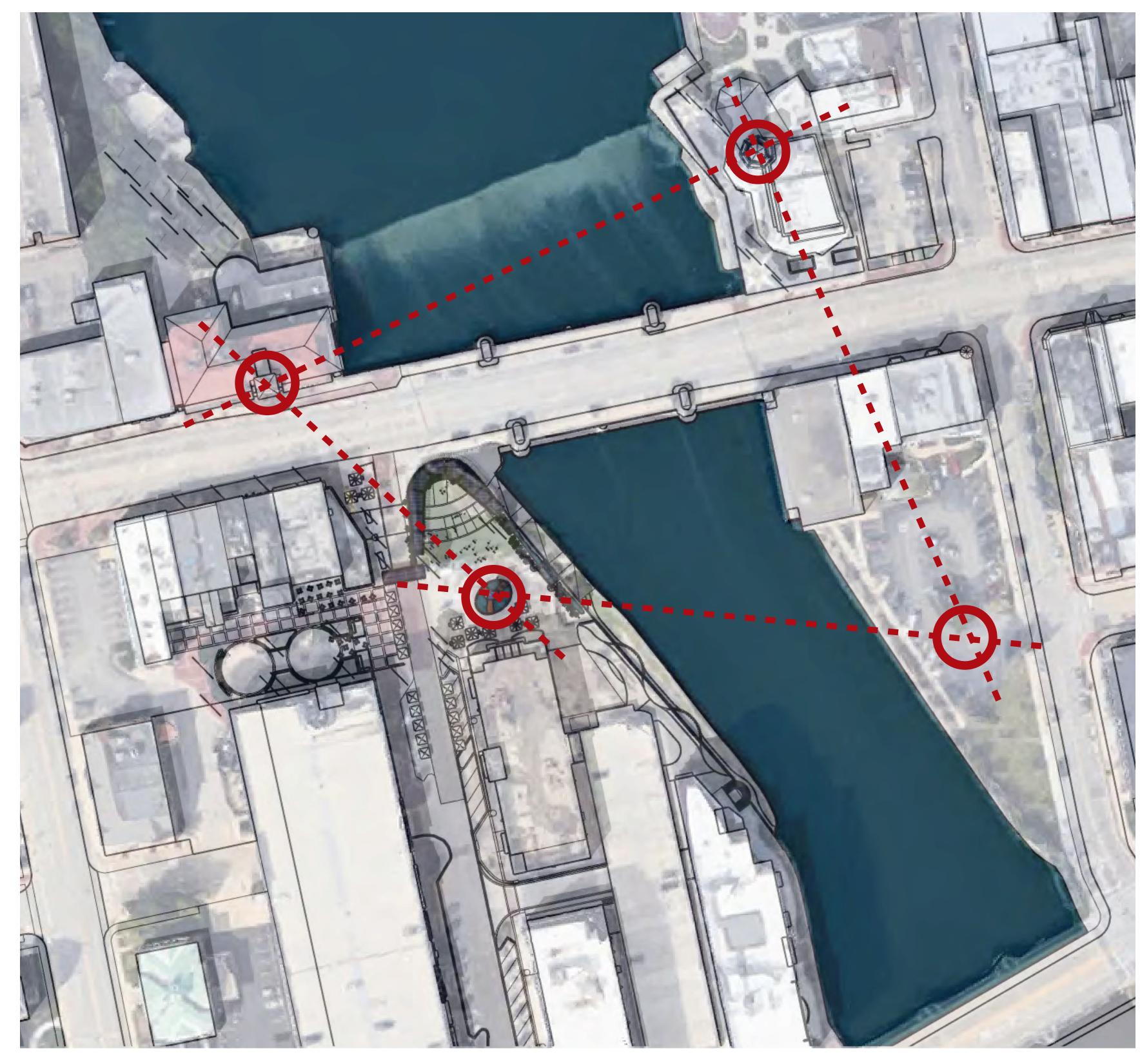
- A. **Request for added green space** the St Charles Initiative is interested in further study of this request. In particular the addition of green, perhaps lawn, as the surface within the area defined with the solar paneled canopies and shade trees (see the attached "Preliminary Plaza Concepts- Ecology/Green Space" drawing).
- B. **Splash park/children activity concept** the St. Charles Initiative appreciates this idea, as it is a shared intention to provide a space for the community, including families with

children. Pottawatomie Park, Lincoln Park, Hazeltine Park (newly renovated), and Mt. St. Mary Park are all very near the First Street area, and provide space for running, ball games, and many other outdoor activities that require significant space for activity. The St Charles Initiative sees this location as a more of a plaza than a park. Conversation will be initiated with the Park District to explore the possibility of adding this feature to what is now the 1st Street area between the south line of the plaza and the entrance to the parking deck in the proposed closure of the street.

- C. **Project cost** see above
- D. Public feedback
 - i. Received feedback over the last two months and continue to accept this.
 - ii. Social media and print media efforts provided opportunities to comment and feedback was received.
 - iii. Future small group meeting as a part of the Initiative process some have already occurred.
 - iv. Public signage and email address where to send feedback.
 - v. Public process to update the development process managed by the Community and Economic Development Department.
- IV. Request for approval of the St Charles Initiative 1st Street Plaza concept plan
- V. Request for approval of the Schematic Design project phase

"A unique gathering place that celebrates the natural beauty and heritage of St. Charles"

First Street Plaza emphasizes cultural continuity, urban regeneration, environmental responsibility and community engagement.



Vision and Urban Analysis - Elevated Places

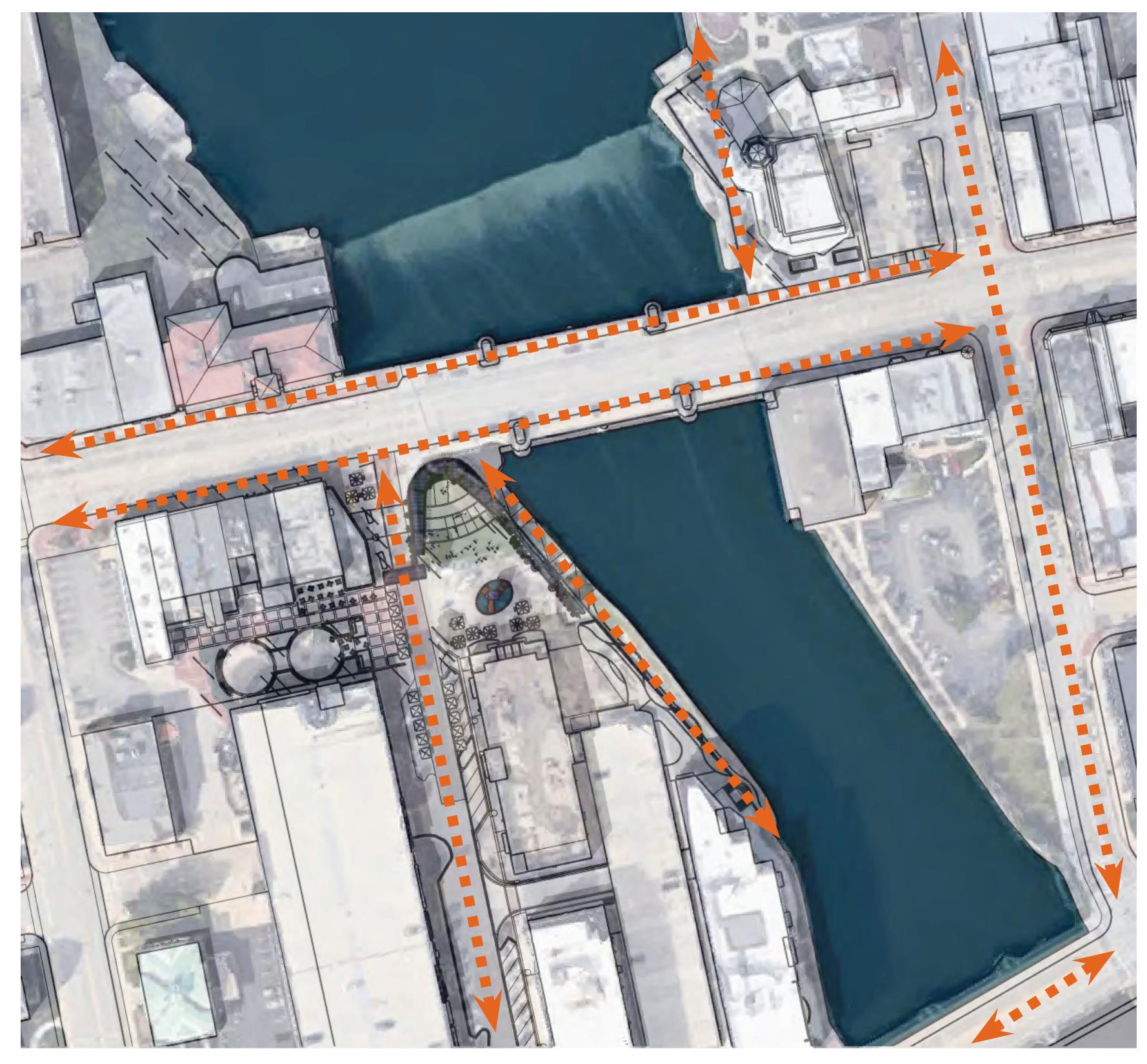


Vision and Urban Analysis - Gathering Places

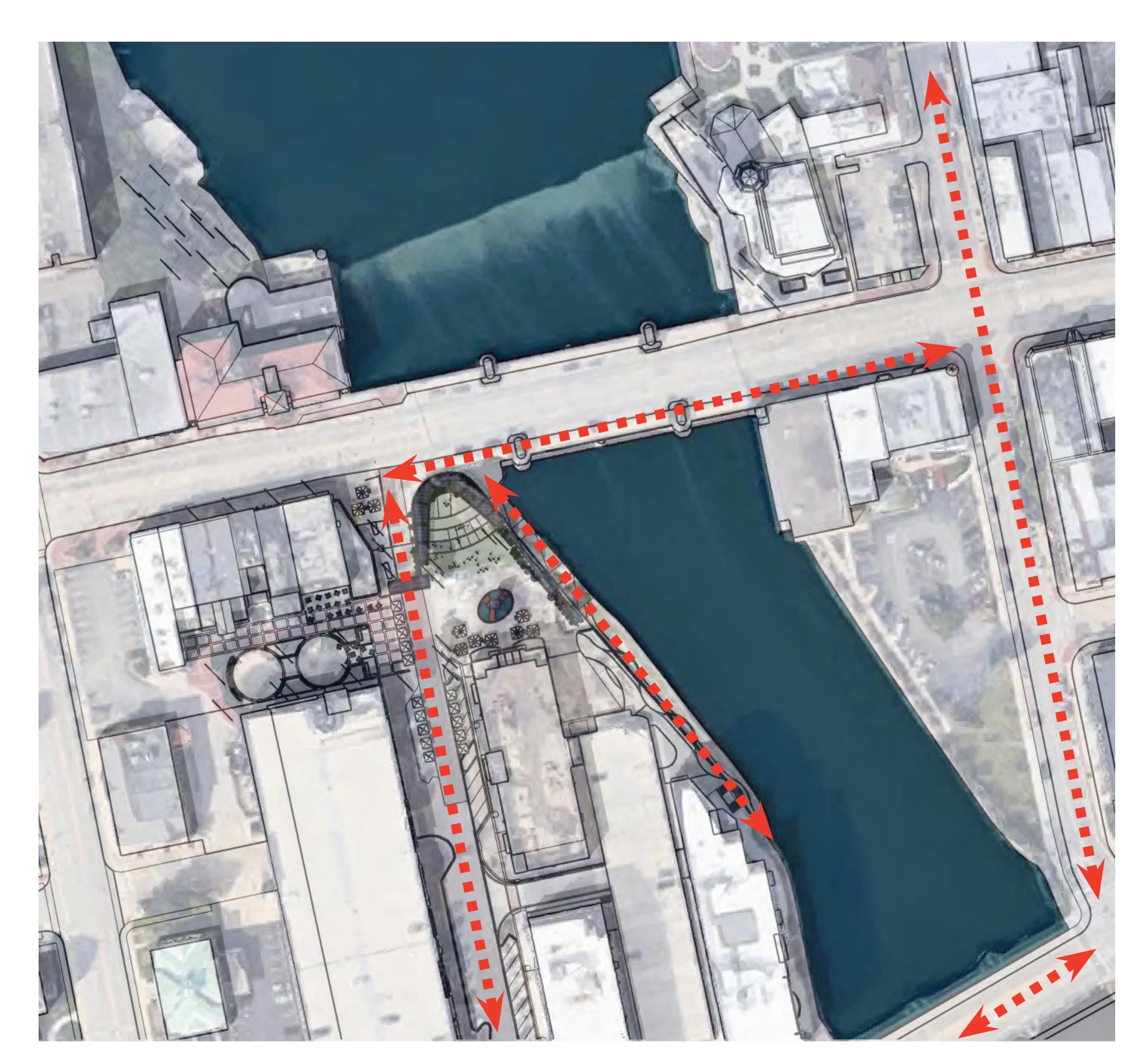


"A unique gathering place that celebrates the natural beauty and heritage of St. Charles"

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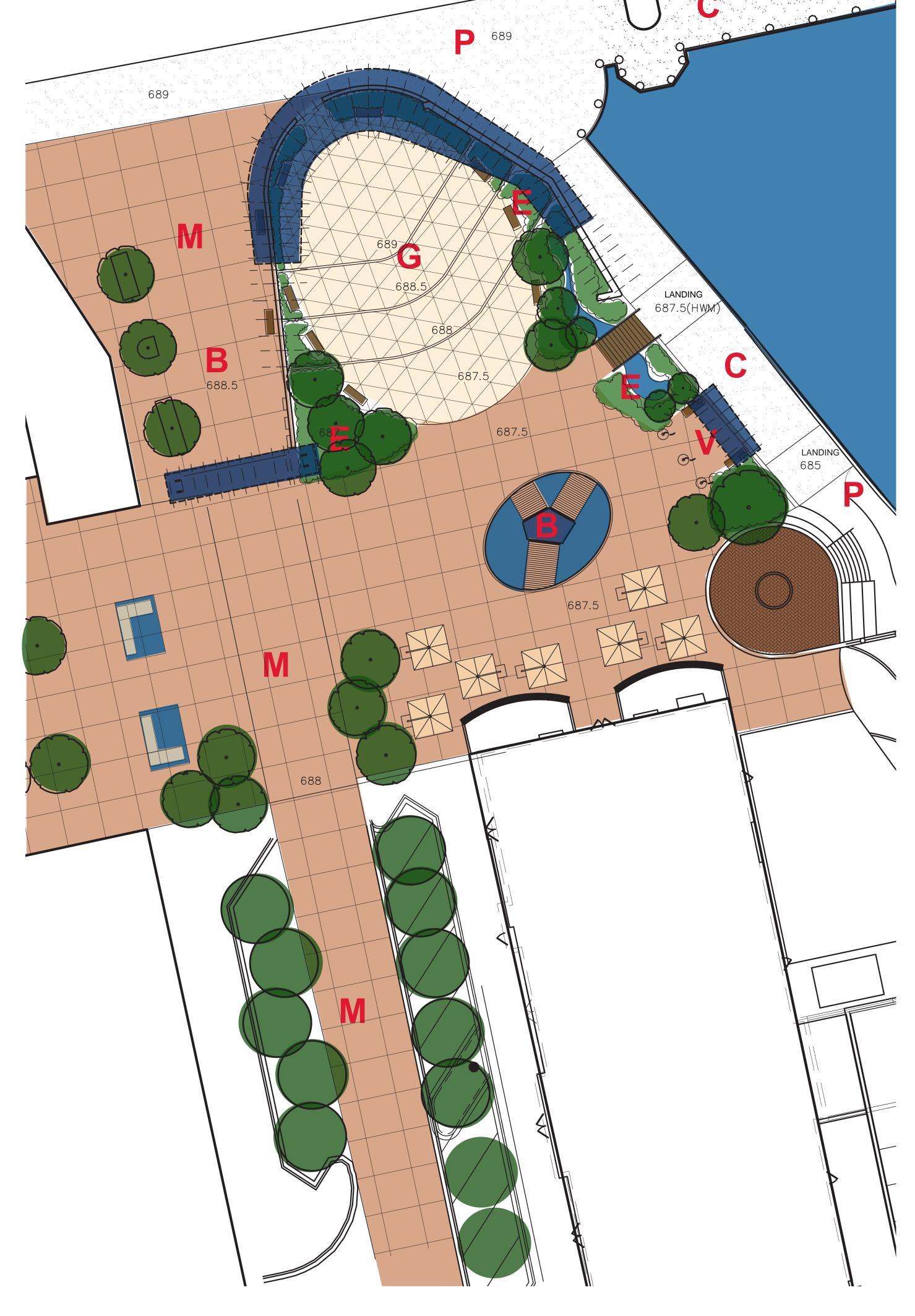


Vision and Urban Analysis - Pedestrian Connectors



Vision and Urban Analysis - Cycling Connectors

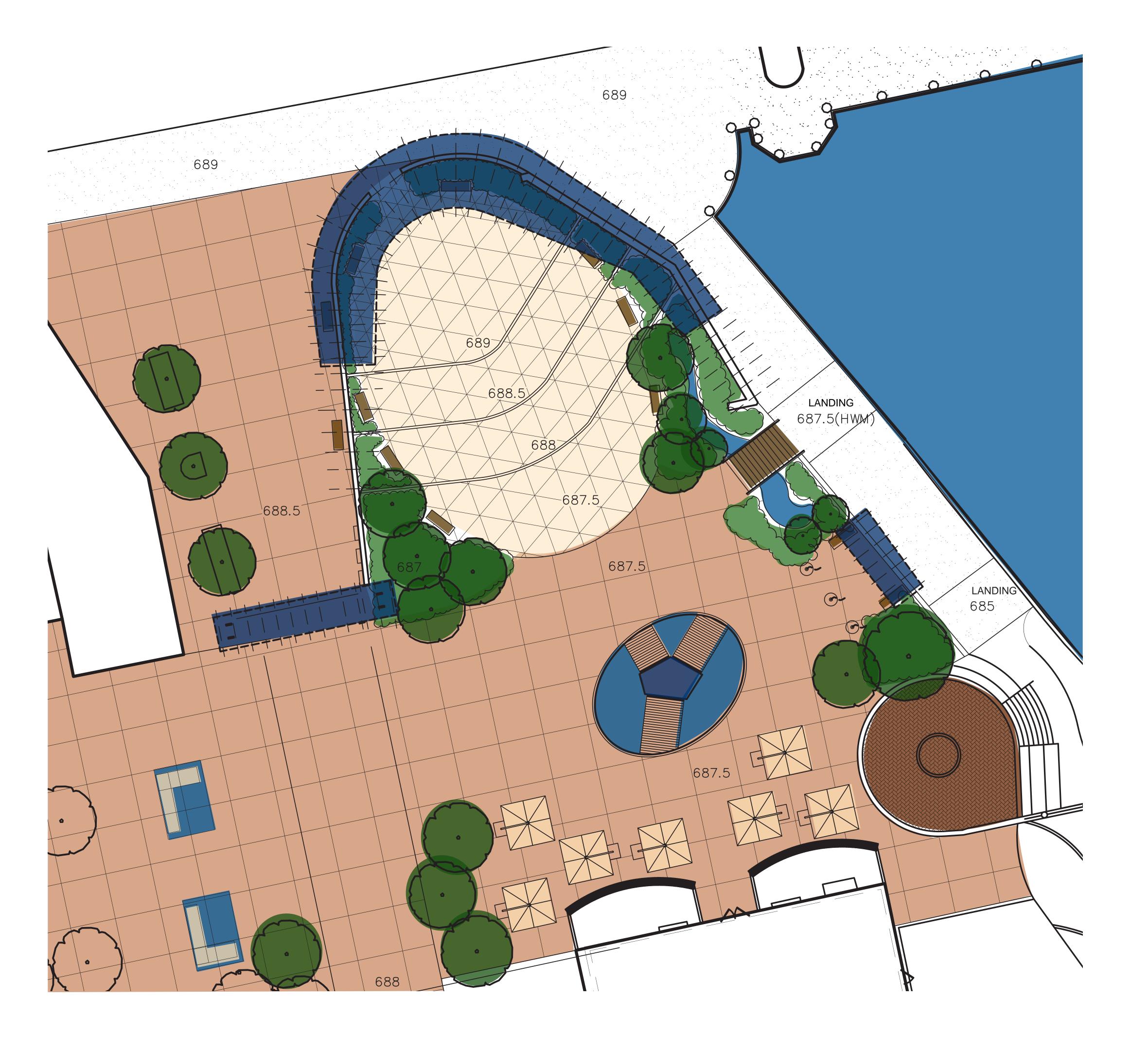




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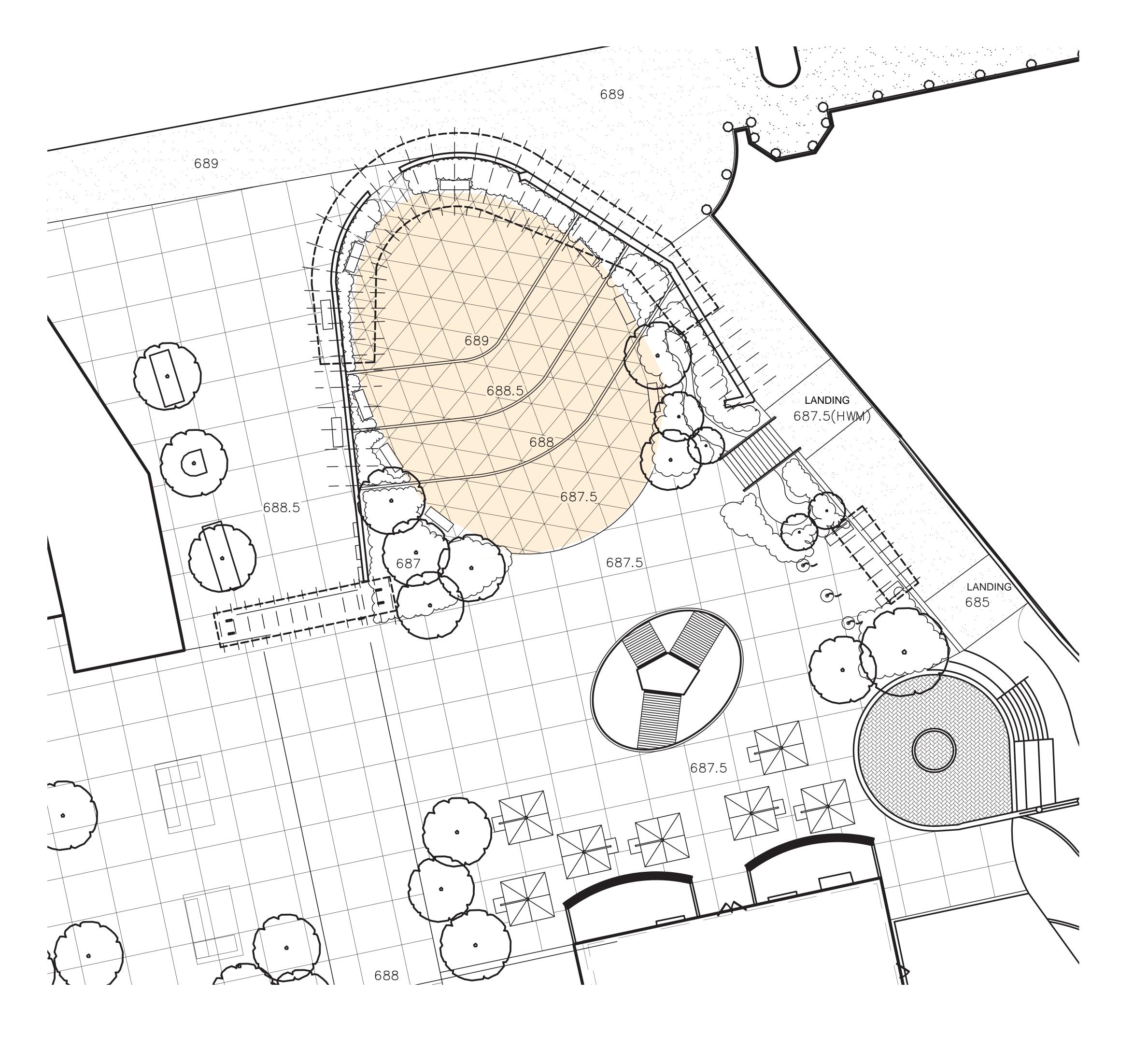
- E Enhance quality of local ecology
- G Supports various forms of gathering
- **B** Reinforcement of city heritage and beauty
- C Connectivity between river and bridge
- C Connectivity between each side of the river
- C Connectivity of north/south sides of Main St
- V Visual connectivity of multiple high value sites
- P Encourage pedestrian and cyclist traffic
- M Encourage street markets and activity





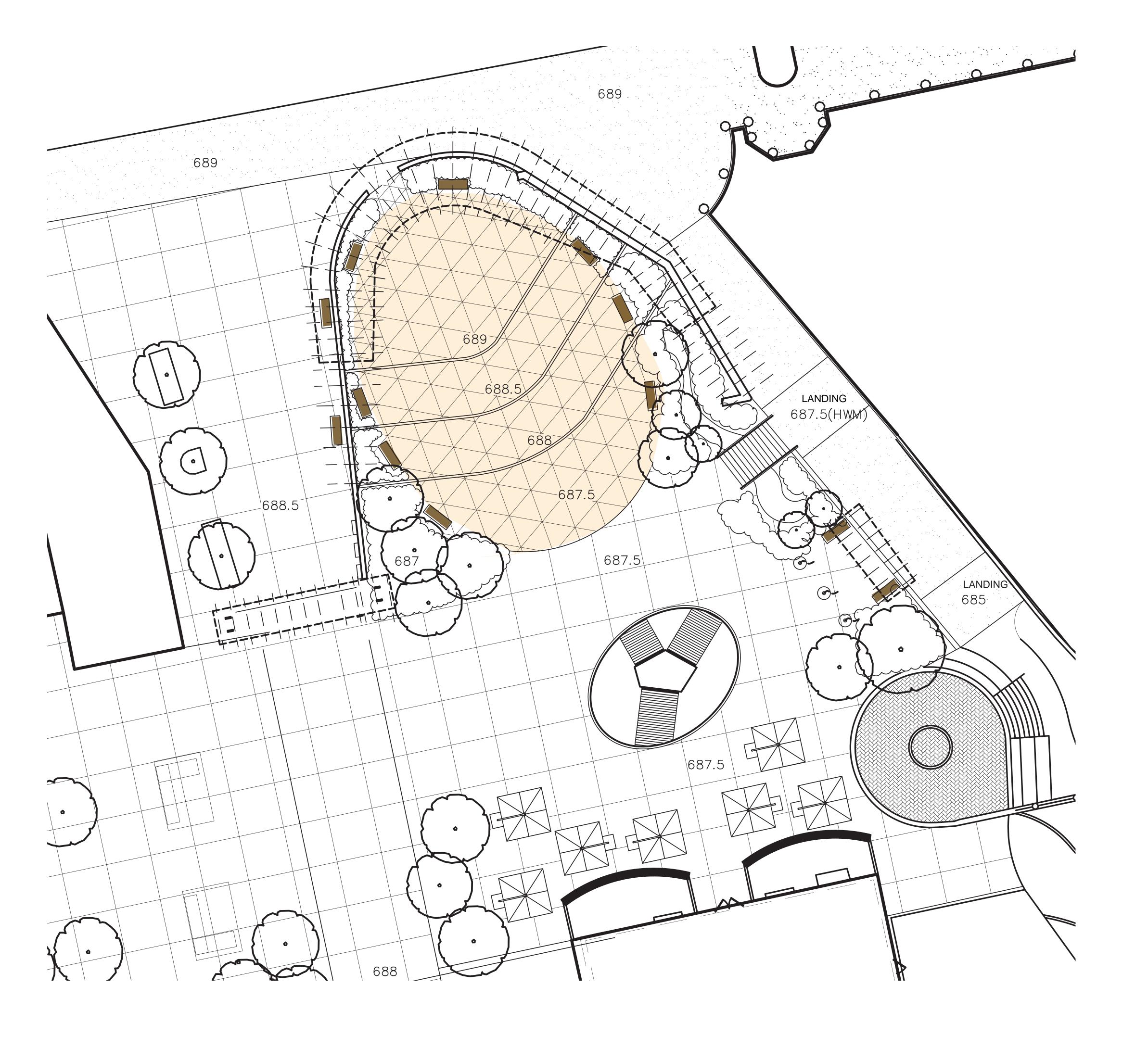
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- . **Expanded bridge** Walkway improves pedestrian and cycling connectivity to East Side of River
- . Ramp and bridge walkway expansion provide expanded pedestrian platform at Plaza and Main Street
- . Retains the history of the Manor Restaurant by retaining it's wall holding the new park in place
- . Graceful sweeping terrace provide space for gathering and community events
- . Raingardens and mini-swales provide stormwater collection and cleansing opportunities
- . Solar panel canopies and shade trees provide comfortable seating and local energy
- . **Entry element** located at the First Street/Main Street marks the place and welcomes the community and it's visitors.
- . A strong focal statement **reflects the elevated places** of the Community City Hall and the Heritage Baker Hotel
- . **Extension of plaza** First Street from Main to Walnut envisioned as a vehicle-free place making way for leisure and community activities.





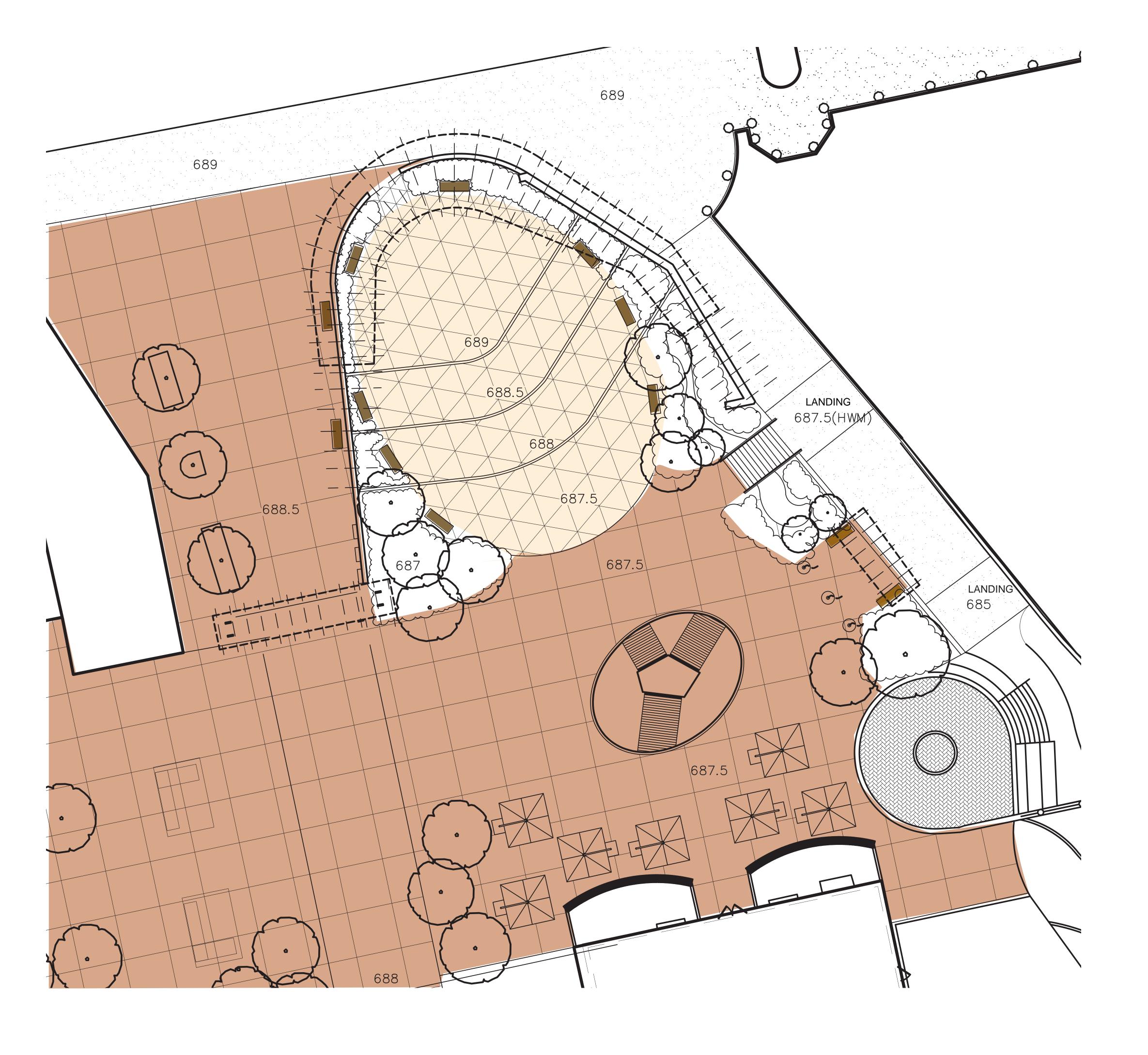
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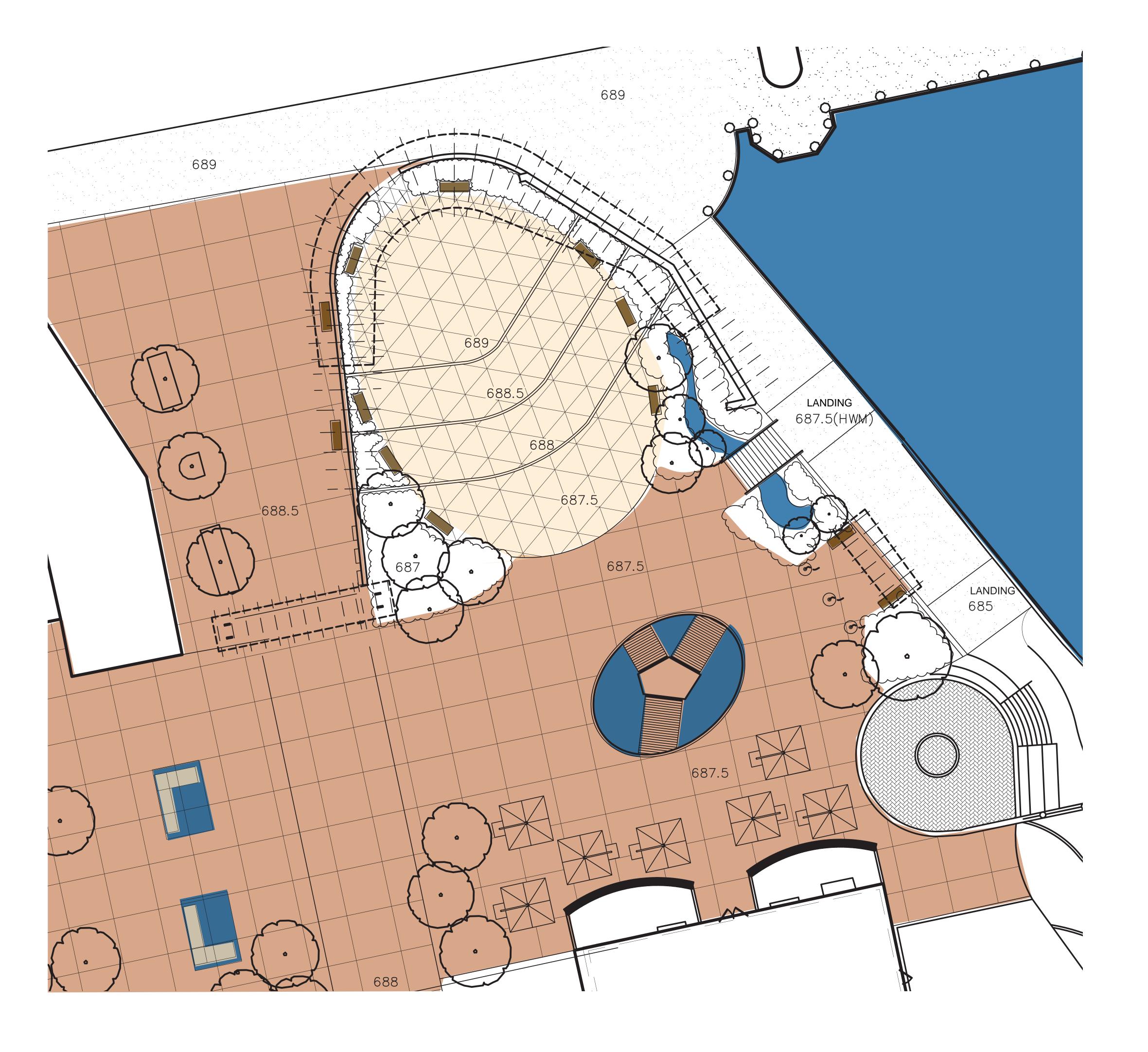
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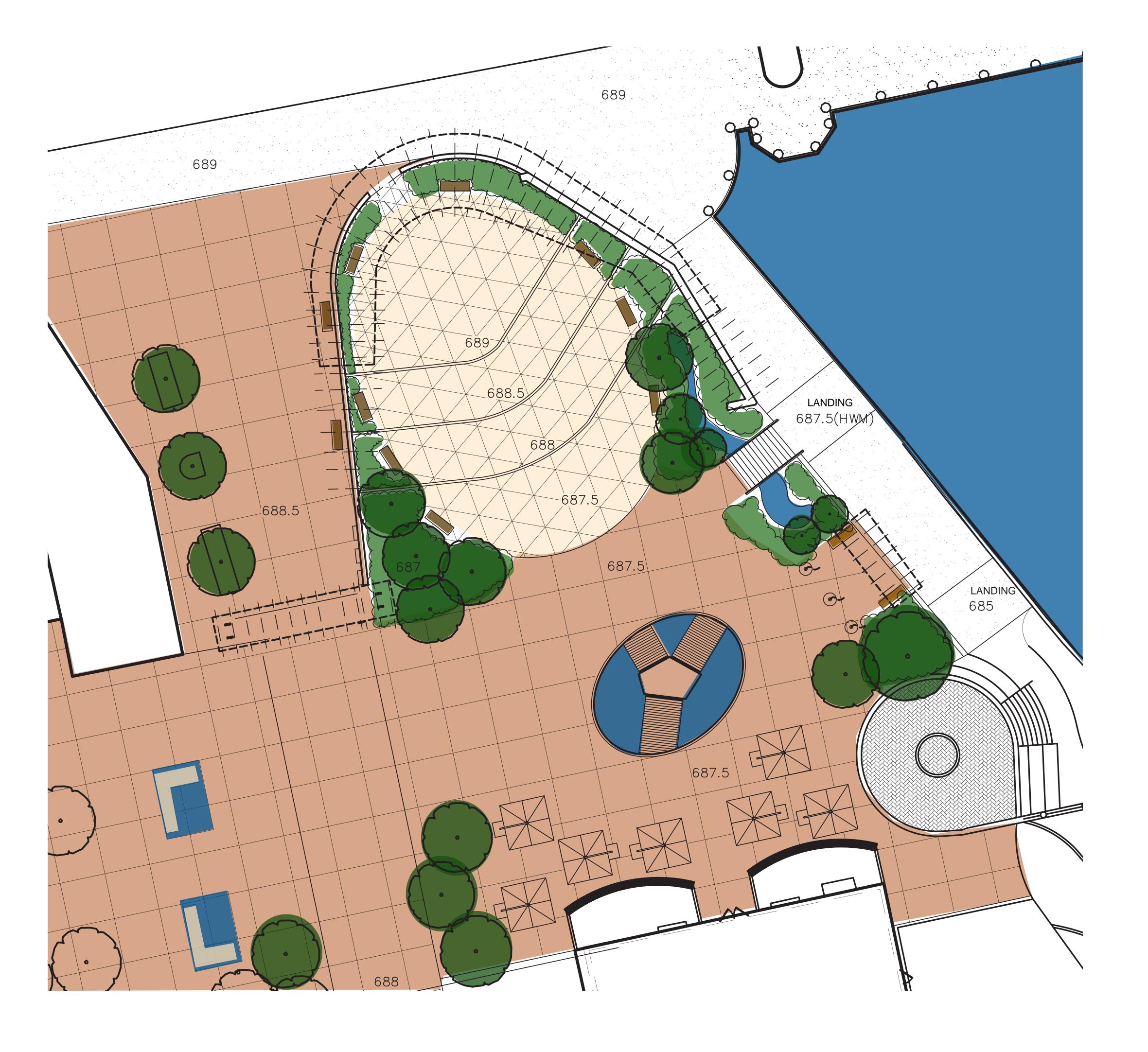
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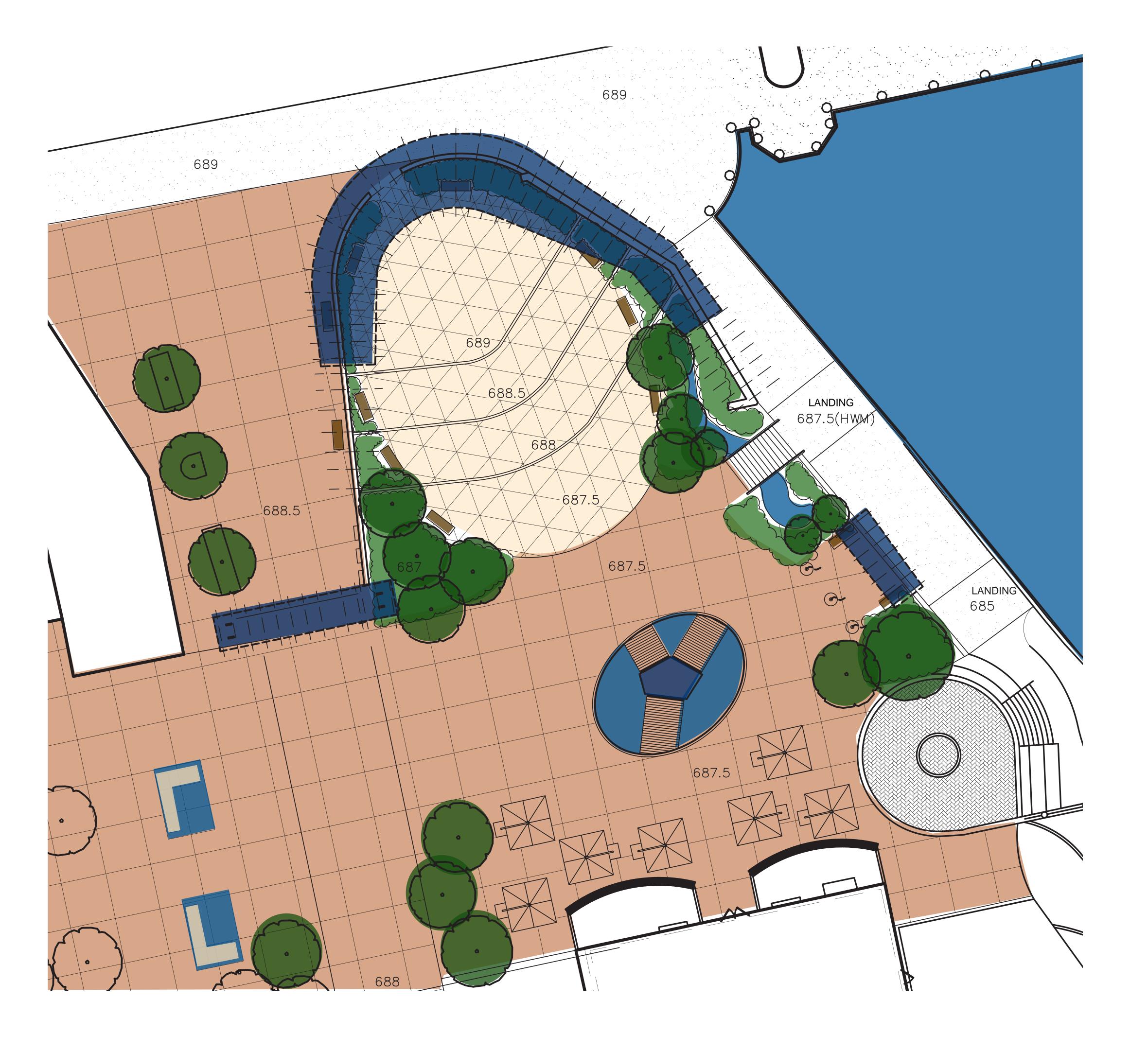




Synthesis Concept

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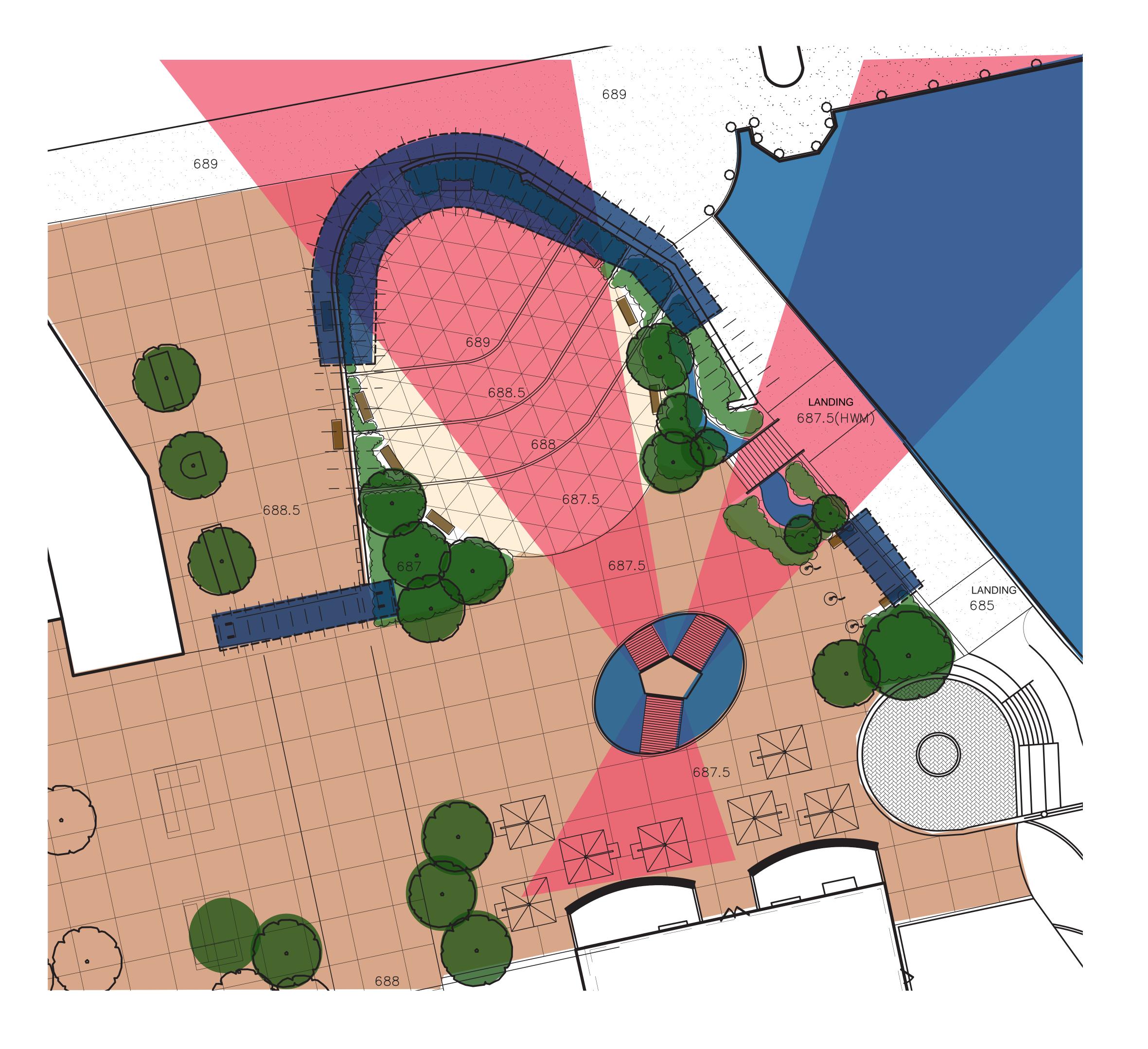




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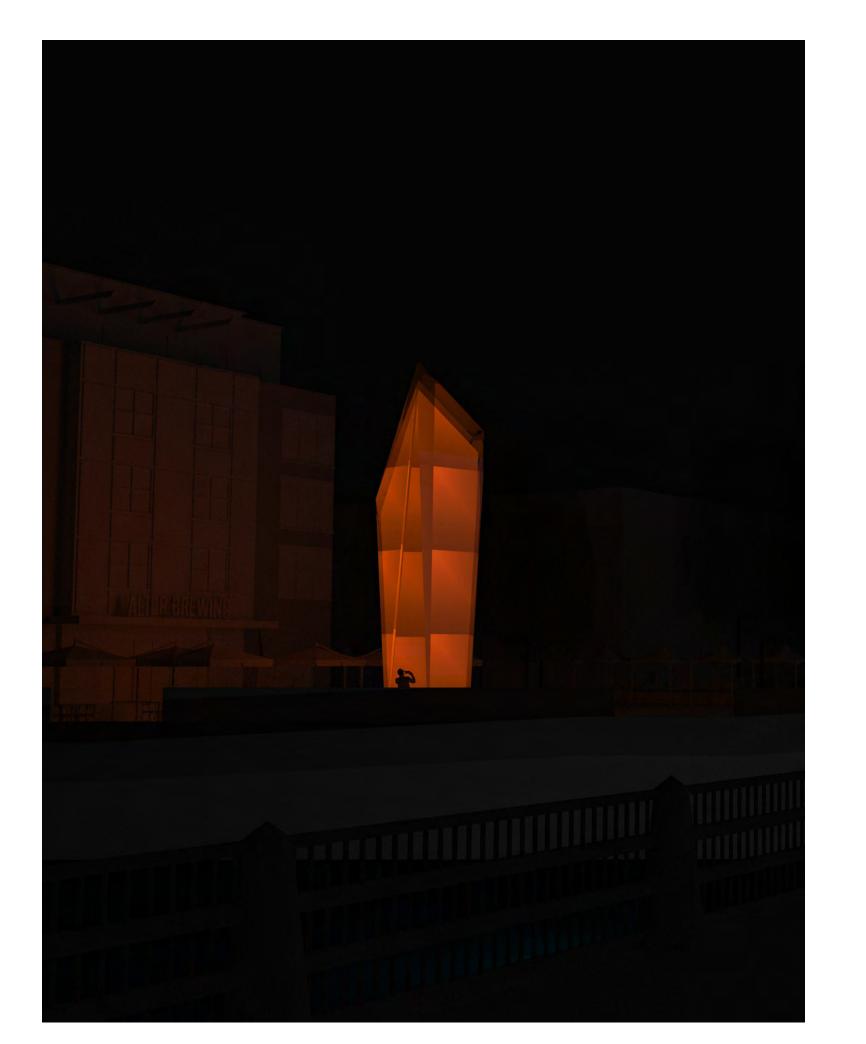


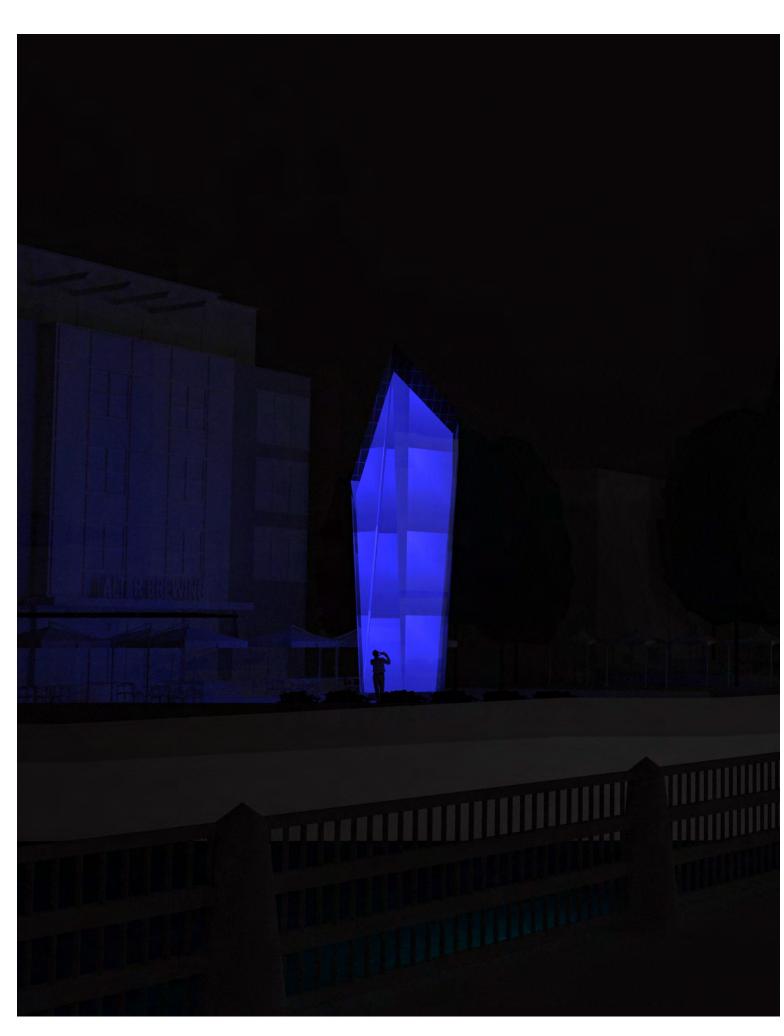
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Honoring Community and Heritage by Creating an "Elevated Place"

Drawing inspiration from the Vietnam memorial in DC, and Cloud Gate (the "Bean") in Chicago:

The unique idea behind this iconic glass sculpture is the combination of reflectivity during the da glowing flame at night.

During the day, one can simultaneously see the old and the new. During the night, the tower illuminates the plaza with festive color to commemorate special activities and holidays.

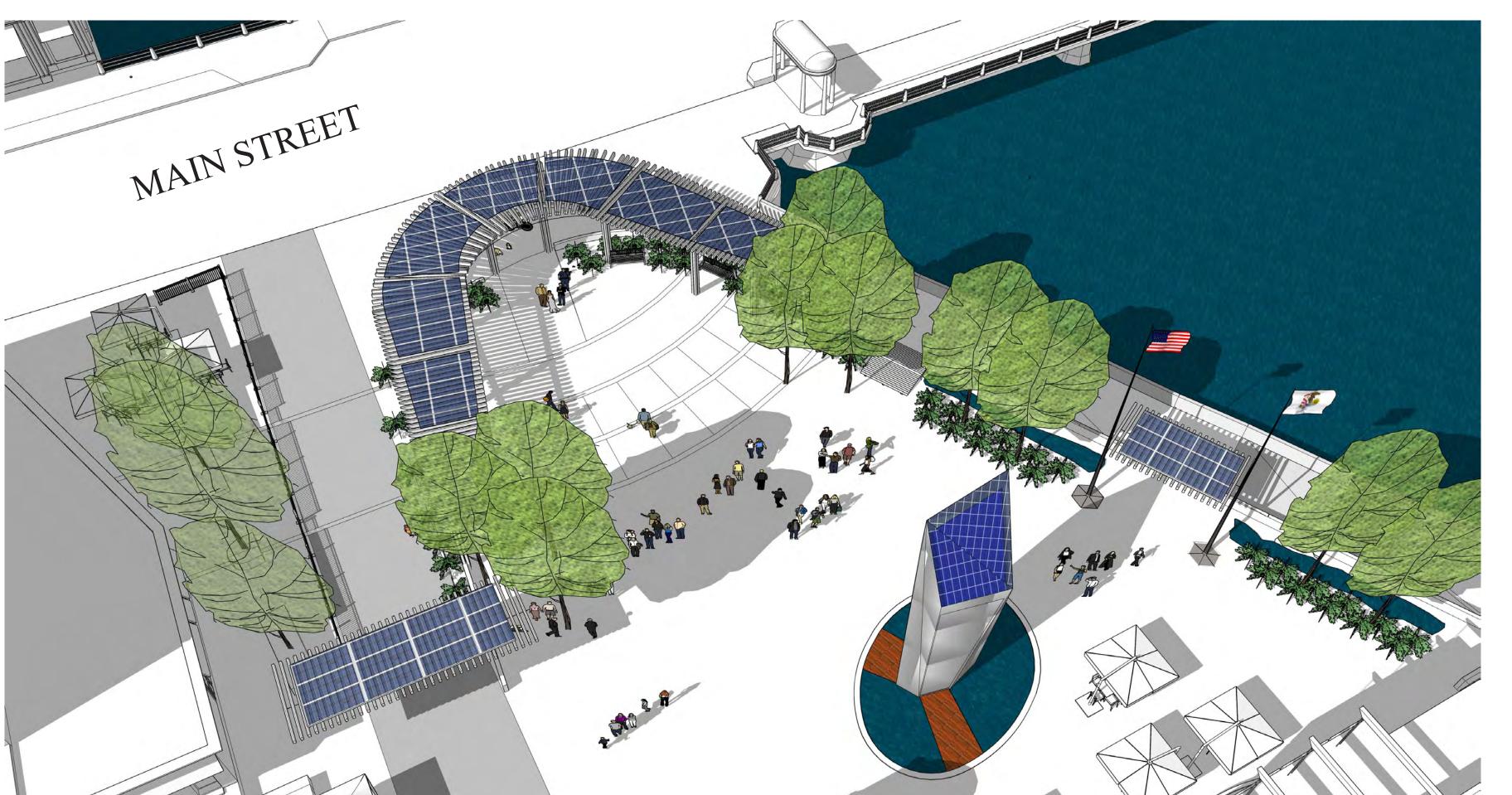






Preliminary Plaza Concepts - Integrating Past and Present

"A unique gathering place that celebrates the natural beauty and heritage of St. Charles"







First Street Plaza emphasizes cultural continuity, urban regeneration, environmental responsibility and community engagement.





September 23, 2020

City of St. Charles Mayor and Council St. Charles, IL 60174

Re: Proposal for Design Services

First Street Plaza Concept

St Charles, Illinois

Dear Mayor and Council,

Serena Sturm Architects, Ltd. (SSA) is pleased to submit the following response to your request for Proposal. Our team of professionals would be honored to be awarded the commission for Design Services for the First Street Plaza Improvements. It has been a pleasure and wonderful opportunity to work with the Community Foundation and the City of St Charles on this special project. We recognize the significance of this project in the pursuit of the Community Foundations vision for this project and wholly appreciate the considerable effect it will have on present and future generations. We feel our extensive expertise in design which requires a detailed research into the project potential, site history, a comprehensive assessment of present conditions, and creative design for a vibrant future, would be an essential contribution to the project.

The project site is defined by Main Street on the north, Fox River on the east, Alter Brewing building on the south and the entire First Street ROW from Main Street to Walnut Street. The design approach is based on the preliminary concept presented to the City on August 10, 2020 The overall gathering space is intended to support a wide variety of community events. No work is intended for the existing West Plaza.

We understand that this Proposal is for the implementation of Phase 2 Schematic Design consistent in intent to accomplish the Work Scope per the completed Phase 1 effort. The Phase 1 Work scope included the development of the program and scope of the improvements, a preliminary design that includes visuals and narrative to communicate with Stakeholders and the Community. In the Phase 2 work we will perform the detailed design and development, construction documentation and implementation of the project.

This agreement assumes that you will provide Miscellaneous Owners Expense items including but not limited to those noted on the attached Project Contract Budget which include Geotechnical an updated Site Survey of the entire project Area that documents the rights-of-way, restrictions, easements, encroachments, zoning, deed restrictions, boundaries, building setbacks and easements, physical characteristics (trees, topography, etc), utility locations and etc. for the site of the Project and a written legal description for the site.

BASIC SERVICES (Architect, Structural, Electrical and Plumbing)

Phase II - Project Implementation

Schematic Design

Develop Stage 1 conclusions into further detail including preparation of scaled architectural and engineering drawings.

Develop Preliminary Budget Outline

Develop Project Schedule

Owner Review – Staff meeting with stakeholders to review our proposed design solutions and gain feedback and insights.

Presentation to Stakeholders as appropriate/required.

Design Development

Prepare and present for approval by the stakeholders documents consisting of drawings and other exhibits to describe the size and character of the project's site, architectural, structural, HVAC, plumbing and electrical systems, materials and such other elements as may be appropriate.

Prepare statement of probable cost

Sign-off of Design Development or revised Design Development

Construction Documents

Prepare, for approval by stakeholders, documents consisting of Drawings and Specifications setting forth in detail the requirements for the Construction of the Project.

Issue bid documents as required for competitive bidding or negotiated contract.

Bidding or Negotiating

Assist the client in pre-bid meeting, obtaining bids or negotiated proposals, and assist in awarding and preparing contracts for construction.

Construction Administration

Observe the Site at appropriate intervals, to become familiar with the progress and quality of the work and to determine in general if the work is proceeding in accordance with the Contract Documents.

Review all shop and engineering drawings and sample submittals for compliance with contract documents.

Provide project status reports as required, informing Client of all relevant events, i.e., substantive discussion with contractors, building owners, etc., or any changes to the construction drawings or schedule.

Determine the amounts owing to the Contractors based on observations at the Project premises and on evaluations of the Contractors' Applications for Payment. Issue Certificates for Payment in such amounts as provided in the Contract Documents

Punch List

COMPENSATION

Projected Overall Compensation for Architectural services for Phase 2 of your project will vary resulting from the ultimate size, our scope of Work, regulatory agency review requirements and project delivery method (i.e. Design-Bid-Build, Pre-Selected Contractor or Design/Build approach to name the most common). Following is an outline of the Projected Overall Compensation for Phase 2 Project Fees based on the approved Phase 1 Scope of Work and the Preliminary Project Cost Estimate.

Basic Services

SCHEMATIC DESIGN DESIGN DEVELOPMENT CONSTRUCTION DOCUMENTS BIDDING CONSTRUCTION		15% 20% 40% 5% 20%		\$31,059 \$41,412 \$82,824 \$10,353 \$41,412	
ТО	TAL BASIC SERVICES FEE				\$207,060
<u>Requi</u>	red Supplemental Services				
0.01	EXISTING DOCUMENTATION			\$10,000	
0.02	SPECIAL CONSULTANTS Civil Engineer Landscape Architect Planting Soils Consulting Engineer Lighting Consultant Allowance)	\$136,600 \$50,000 \$9,000 \$20,000	1.10 1.10 1.10 1.10	\$150,260 \$55,000 \$9,900 \$22,000	
0.03	AGENCY REVIEW Civil Engineer (Hourly Allowance)			\$10,000	
0.04	COST ESTIMATES Civil Architectural, Structural, Electrical, Plumbing	\$13,700	1.10	\$15,070 \$6,100	
0.04	MODEL/PRESENTATION MATERIALS Architect (Allowance)			\$10,000	
0.05	MAINSTREET BRIDGE EXPANSION FEASIBILITY Architect (Hourly Allowance) Civil Engineer (Hourly Allowance)			\$2,500 <u>\$10,000</u>	
TO	OTAL REQUIRED SUPPPLEMENTARY SERVICES				\$ 298,330
Micellaneous Owners Expense \$42,2					\$42,200
Reimb	Reimburseable Expense (Allowance)\$6,900				
TOTA	L COST (with Allowances)				<u>\$554,490</u>

Projected Compensation for the implementation of Phase 2 Schematic Design:

Basic Services

SCHEMATIC DESIGN		\$31,059
Required Supplemental Services		
0.01 EXISTING DOCUMENTAT	ION \$10,000	
0.02 SPECIAL CONSULTANTS Civil Engineer Landscape Architect Planting Soils Consulting Lighting Consultant (Allow	\$35,420 \$22,000 Engineer 0	
0.03 AGENCY REVIEW Civil Engineer (Hourly Alle	owance) \$10,000	
0.04 COST ESTIMATES Civil Architectural, Structural, E	\$5,170 Electrical, Plumbing \$3,000	
0.04 MODEL/PRESENTATION Architect (Allowance)	MATERIALS \$10,000	
0.05 MAINSTREET BRIDGE EX Architect (Hourly Allowand Civil Engineer (Hourly Allo	se) \$2,500	
TOTAL (Basic Services and Requ	uired Supplemental Services)	\$122,090
Miscellaneous Owners Expense	Allowance (coordinated by Architect paid by Owner)	
Survey		\$8,200
Geotechnical Report (Allowance	e)	\$4,000
Reimburseables Allowance	_	\$1,380
TOTAL COST (with Allowances)		\$166,729

Serena Sturm Architects HOURLY FEE SCHEDULE

Serena Starm Architects Hooker Fee SomeDole	
<u>Position</u>	Base Hourly Rates
Principal Project Architect Intern Project Services Coordinator.	\$ 90.00/Hour \$ 65.00/Hour
Reimbursable expense items such as special delivery, printing at the fees outlined for professional services. The costs will be invo Itemized billings will occur on monthly intervals.	
Terms and Conditions of this proposal are in accordance with the Edition. Upon acceptance of this proposal SSA will prepare a Co specifications and other documents, including those in electronic consultants are Instruments of Service owned by the Architect ar Upon execution of this agreement the Architect grants the Owner Instruments of Service solely for the purposes of constructing, us	ntract Draft for Owner review. Drawings, form prepared by the Architect and the Architect's and are for use solely with respect to this Project.
We believe that our diverse architectural background, our collab- this scope of architectural work and our sensitive sustainable successfully complete the project goals. We are excited to subn initial requirements for this project. We appreciate your consid- forward to the opportunity to work with you on it. Please call if yo	e designs qualify Serena Sturm Architects, Ltd. to nit this proposal and trust that it is responsive to your eration of our firm for this special project and look
Respectfully,	
Maite of hum = 54,24d	
Marty Serena, Principal Serena Sturm Architects, Ltd.	
ACCEPTANCE OF PROPOSAL	
This proposal shall be signified by signing the endorsement below	w and returning one original to our office.
This proposal's acceptance may be terminated for convenience a seven (7) days written notice. In the event of termination not the compensated for services performed and reimbursable expenses	fault of the Architect, the Architect shall be
By:Mayor Raymond P. Rogina	Date:

	AGENDA	A ITE	M EXECUTIVE SUMM	ARY	Agen	da Item Number:	4d-1
CITY OF	Title:	Plan Commission recommendation to approve a Map Amendment, Special Use for Planned Unit Development, and PUD Preliminary Plan for Munhall Glen.			nt,		
ST. CHARLES ILLINOIS • 1834	Presenter:	Ellen Johnson/Russell Colby					
Meeting: Planning & Development Committee Date: September 14, 2020							
Proposed Cost: \$			Budgeted Amount: \$			Not Budgeted:	
Executive Summary (if not hudgeted please explain):							

Executive Summary (if not budgeted please explain):

The subject property is comprised of five parcels totaling 15 acres west of S. Tyler Rd. at Munhall Ave. The property contains one single-family home with the remainder in agriculture.

Court Airhart of Airhart Construction Corp. has filed zoning applications requesting approval of a singlefamily subdivision on the subject property. The plan is similar to a Concept Plan reviewed by the City in June of this year. Proposed is the following:

- Rezoning to the RS-4 District (6,600 sf minimum lot size).
- 50 single-family home lots with varying lot sizes and widths accommodating a variety of single- and two-story home models.
- Access from Munhall Ave. with emergency-only connection to South Ave.
- Internal looped public street configuration with sidewalks.
- Two stormwater detention areas with adjacent "pocket parks".

The applicant is requesting PUD approval to allow for deviations from certain bulk standards of the RS-4 District. A PUD Preliminary Plan for the development has been provided which includes preliminary engineering, landscape plan, and plat of subdivision.

Plan Commission Recommendation

Plan Commission held a public hearing on Sept. 9, 2020. No members of the public spoke during the hearing. Much of the discussion centered around the proposed emergency-only connection to South Ave. Commissioners also expressed interest in preservation of existing trees along Munhall Ave., if possible. Plan Commission voted 9-0 to recommend approval, subject to the following:

- 1. The South Ave. connection shall be a full public access.
- 2. Resolution of all staff comments prior to City Council action.

Attachments (please list):

Staff Memo, Applicant Letter on Outstanding Items, Plan Commission Resolution, Staff Report, Fee Request Letter, Emails from Residents, Applications, Plans, Traffic Study/Review Memo

Recommendation/Suggested Action (briefly explain):

Along with a recommendation on the overall project, the applicant is also seeking direction on the following outstanding items- these are discussed further in the attached staff memo:

- a) South Avenue connection: Emergency access only vs. full public access
- b) South Avenue off-site improvement: Improvement matching the street condition to the west is proposed. City reimbursement is requested for any further improvement, such as curbs, storm sewer, sidewalk, parkway, etc.
- c) Sanitary Sewer Trunk line: City reimbursement for the developer replacing/rerouting an existing trunk line sanitary sewer, with the reimbursement based on "oversizing" of the sewer.
- d) Impact Fees: Allowing School/Park/Housing Impact fees to be paid per unit through the PUD.

Community & Economic Development



Staff Memo

TO: Chair Rita Payleitner

And Members of the Planning & Development Committee

FROM: Russell Colby, Assistant Director of Community & Economic Development

RE: Munhall Glen PUD – **Outstanding Review Items**

DATE: October 7, 2020

In the attached letter dated 9/28/2020, the applicant identified certain unresolved review comments and requests that impact the overall project cost. Locations of these items within the project are shown on the attached plan mark-up for reference.

Prior to moving forward, the applicant is seeking further direction. Staff and the applicant have discussed the items as outlined below.

1. South Avenue connection

a. Emergency access vs. open street:

- Fire Code requires an emergency access connection. The developer is agreeable to providing the emergency access connection, but is not in favor of an open public street connection.
- Plan Commission recommended the street be open to all traffic.
- Police and Fire Departments have commented that their preference is for an open public street to avoid snow removal issues, improve public safety response times, and provide additional points of access.

b. Off-site street improvement:

- The adjacent approximately 360 ft. of South Avenue off-site to the west of the property is unimproved (up to the entrance of the City Public Works yard).
- The developer is willing to improve the street similar to the asphalt road section that exists to the west (without curb, storm sewer, sidewalk, parkway etc.). This level of improvement is sufficient for an emergency access and as an interim condition if open as a public street. (Upgrades to the street could be made later upon future redevelopment of adjacent industrial properties on South Ave.)
- If there is an interest in full improvement of the street, the applicant is requesting the City assist with this cost (estimated at \$115,000 to \$145,000).

2. Sanitary Sewer Trunk Line:

- A major Sanitary Sewer Trunk Line that serves the east side passes along the north and west boundaries of the site. The City has identified this trunk line for replacement at some point in the future, based on capacity needs for future development and redevelopment on the east side. In order to allow for future replacement of the sewer in its current location, a significant width of easement would need to be preserved along the perimeter of the property, which would conflict with the development plan.
- Alternately, this trunk sewer could be replaced now and re-routed through the site along the streets in a more accessible location. Staff supports this concept, as it would be more cost effective to complete now and would prevent future inconvenience to residents of the development when the sewer is replaced.
- The City would reimburse the developer for this improvement. The amount would be estimated later once the sewer has been engineered. The developer suggested an estimate of \$200,000, but staff believes the cost could be higher, up to the \$500,000 range.
- The City would typically reimburse for "oversizing" of a utility line, given that the sewer may replace some portion of on-site sewer that is required to serve the development. Staff would request the developer provide a cost estimates to substantiate the amount that the City would reimburse.

3. Tree Preservation/Removal:

- Staff and Plan Commission commented that preserving the mature trees on Munhall Ave. would be desirable.
- The applicant indicated that there will be a substantial replacement of undesirable trees with higher quality trees on the site overall, but that the trees along Munhall could not be preserved due to the site grading and stormwater detention requirements.
- Staff believes this response is sufficient and no further information is needed on this item.

4. Railroad Culvert:

- The stormwater report for the project identified that the site drains directly to a "dilapidated" drainage culvert under the railroad right-of-way. The culvert is located off-site of the property to the northwest, about 40 ft. from the property line. This culvert appears to be on Union Pacific railroad property.
- Staff had commented in the engineering review that this culvert may require replacement in order to ensure it continues to function and support the development.
- Staff is requesting the applicant's engineer demonstrate that the existing culvert will not be further negatively affected by the drainage resulting from the development of the property. This requires further discussion with the developer and their engineer.
- The City will contact the property owner (UP Railroad) regarding the condition of the culvert.

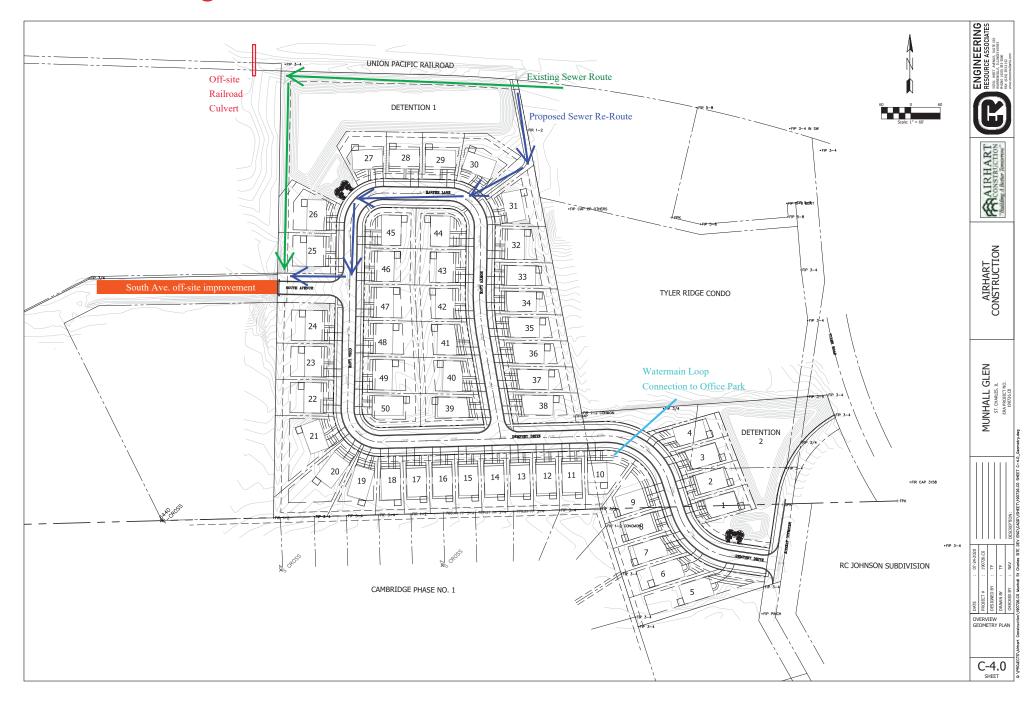
5. Watermain loop to office park

- Staff identified a watermain connection to the adjacent office park (525 S. Tyler Road).
- The loop connection may provide some benefit for water looping with the development. A water modeling study will be completed to assess this.
- The developer indicated that this connection does not serve the project and is more appropriate as a City expense (\$7,000 to \$10,000). This will be reviewed further based on the water modeling.

6. Impact Fees to be paid per unit (School, Park, Inclusionary Housing)

- The developer is requesting that the school/park land-cash fees and the Inclusionary Housing fee be paid on a per-unit basis.
- City Code requires these fees to be paid in full prior to issuance of the first building permit for each phase (this project will be developed in a single phase).
- A deviation from this requirement could be included in the PUD approval if the Committee is agreeable. (An alternative could be to require the fees to be paid for groups of units as opposed to on an individual basis; this may be simpler from an administrative standpoint.)
- In 2011, the City approved a similar request for the Regency Estates PUD; in that case, the School fees were due prior to issuance of a certificate of occupancy for each house.

Outstanding Review Items



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September 28th, 2020

Russell Colby
City Planner
Community & Economic Development
City of St. Charles
2 E. Main Street
St. Charles, IL 60174

Re: Preliminary engineering requests and request for direction

Dear Russell,

Thank you for your time on the phone on Friday regarding the preliminary staff review and requests from staff. As we discussed, we are enthusiastic about Munhall Glen and the opportunity to bring this new community to St. Charles. However, we are very concerned about some of the infrastructure requests and the cost implications to the project. We feel in order for this project to be successful we have to be able to market this neighborhood with prices starting in the low to mid \$400,000.

We already shared our concerns about the school, park and affordable housing donations. Those fees alone will increase the costs to the project by approximately \$840,000.00 or \$16,800 per home, 4% of the sales price. After receiving the preliminary review and discussion with the staff development team, we felt it was important to dig down into the cost requests to see if the project was still viable.

Below are the major concerns we have. We need some direction on how these will be evaluated and who will bear the costs. We believe many of these requests are not project specific to Munhall Glen. Some seem to be requests by staff that would be nice, but not necessary, and others seem to be more fairly attributable to St. Charles as infrastructure projects. At this point, we feel we need direction before going further with updating the engineering documents for preliminary approval.

Our concerns are as follows:

- 1) South Ave We continue to believe the best format for this is an emergency only entrance. The traffic from the industrial side is very different from the residential side and we are concerned the neighborhood would become a cut through from the industrial area. I have been on South Ave many, many times and there is constant vehicle and boat movement that temporarily blocks the road. In addition, there often is temporary vehicle storage in the right of way.
 - a. We are willing to put in a full road improvement on our side of the property line. This way in the future if there is ever a change to usage in the industrial area it can easily become full access.



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- b. We believe that there should be signage installed for emergency and municipal vehicle usage only.
- c. We believe an asphalt road similar to what currently exists on South Ave. is appropriate and can be easily maintained by Public Works.
- d. The request from staff for a full width improvement for street, curb and sidewalks will also involve landscaping and some level of storm water improvements. The cost, depending on the final design requirements, will be in the \$115-\$145,000 range.
- e. This request for full improvement seems to be a municipal improvement project that the cost should not be put on the developer.
- 2) **Sewer Main/Trunk line** This trunk line is located along the North and North West end of the property and it eventually ties in at South Ave.
 - a. Easement Expansion Request (for future replacement of sanitary trunk line)
 - i. Public Works have said they want an additional 10-20' of easement width granted and that area has to be flat. Part of the reason for this is that the trunk line is installed almost on/over the south side of the existing 20' easement.
 - ii. This would push our whole detention area south about 40' which would cause us to lose 4 lots.
 - iii. The loss of 4 lots will make Munhall Glen no longer feasible to develop.
 - b. Relocation of Trunk Line into Munhall Glen
 - i. There was some discussion about moving the trunk line into the subdivision and putting it under the north and west roads.
 - ii. There would be about 950-1,000 feet of 27" pipe or larger to install. The depths would be in the 15'-20' range.
 - iii. A quick guesstimate of cost would be in the \$140,000-\$200,000 range.
 - iv. We believe the correct decision is to move forward with the relocation. It will be better for future maintenance and ease of access. Installation of this trunk line has nothing to do with services for this property, and everything to do with what is up stream. This seems to be a City expense, but there is no clarity on this request from staff and that the City would cover these costs.

3) Tree Removal –

- a. We have to clear the site, no other way to develop it under modern storm water regulations. There is not an ability to save the existing trees.
- b. We have added a ring of 102 trees around the outside of the property in addition to 120 street trees. The grade and long-term health of trees in this area will be substantially improved after development.
- c. We believe this is going far beyond the requirements, yet we are continuing to be told to save the trees.
- d. We need direction.
- 4) Railroad Culvert Repair/Replacement/Easement The staff has stated they want us to repair an offsite drain pipe that goes beneath the rail line. There also was a request for us to get a stormwater easement for the existing culvert from the railroad. This pipe actually drains a significant portion of the property west of us in the industrial park.
 - a. This work is not warranted by our development. We will be reducing the storm water off-site flow by 2,500 percent during storm events. Currently the off-site flow rate of



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the farmed property during a 100-year event in an hour is 25.5 CFS. After development it will be 1.2 CFS.

- b. Our development actually improves the viability of the existing culvert.
- c. This culvert does not even touch our property. We would have to go across our neighbor's property to access it.
- d. There had been a request to get an easement from the railroad. The current drainage pattern goes to that location. Why do we need an easement? Stormwater regulations allow us to drain to the same location.
- e. Policy perspective This could be a bad precedent to set as it is contrary to stormwater regulations. We are allowed to release storm water in the same location. If this becomes City policy it could greatly affect future development. What happens if someone, like the railroad, choose not to allow access or grant an easement. They are not allowed to remove an existing culvert or change the flow. So why would we be required to maintain it, when we are actually substantially reducing flow.
- f. Potential expense for the culvert with landscape repair is in the \$15-\$20,000 range for the actual work.
- g. If the City feels the work is warranted, the City should communicate with the railroad or make an emergency decision and just do the repair. It should not be our responsibility.
- 5) Water service in Office Park The staff has made the request for us to loop a water line that services the office park to the northeast of us.
 - a. We already are granting an easement since the water line is on our property and is not in an easement.
 - b. The cost to loop this water line is approximately \$7-\$10,000.
 - c. There is no value to our development of looping the water line, although we understand the benefit to the City and the office park by looping the water line.
 - d. This request seems to be a municipal improvement project that the cost should not be put on the developer.
- 6) School/Park/Affordable housing We had previously requested in a letter dated May 21st, 2020, that the School and Park fee in lieu and the Affordable Housing fees be reduced to the single family attached classification due to the population makeup of the target market for this housing product. We believe the documentation we supplied provided proof that warranted the modification to the fee structure.

Regardless of the fee classification used, we request that the fees be paid at time of building permit approval for each lot. We think this is reasonable for a variety of reasons:

- a. The population impact of the housing, if there is any, will not occur on the City, School District, and Park District for approximately 6 months after building permit issuance.
- b. The fees combine to approximately \$840,000 and the homes will be built over a 4-5-year period. The carry cost alone for these fees could be in the range of \$40,000-\$60,000 per year over the life of the project. This will greatly affect selling prices, affordability, and negatively impact the target market of the properties.
- c. The increase in tax revenue in perpetuity of Munhall Glen to all of the local taxing bodies, while the impact is still questionable, is significant.



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d. Since these homes could be 2, 3, or 4 bedrooms, it is reasonable to wait so that the fees are properly paid at time of building permit.

While we still believe the fees should be reduced, for all of the reasons above we think it is an appropriate decision to allow the fees to be paid at time of building permit issuance, instead of prior to recording of the final plat. We ask that you take the above into consideration and grant our request.

The above are our primary concerns and we are looking for direction. Together the financial impact of the donations and infrastructure requests run well over \$1.36 million dollars or approximately \$27,200 per home. While we understand that as a developer, we have a lot of costs to bear to make a development successful, we believe these costs go beyond and are detrimental to the developments' success.

We very much would like to move forward with this development and think Munhall Glen will be a great addition to the housing needed in St. Charles. In order to make the decision to continue moving forward, we need to understand if these requests will become requirements and/or if the City will take on the cost for the municipal infrastructure improvements requested.

Thanks again for your time and we look forward to hearing from you.

Sincerely,

Court Airhart Airhart Construction

President



City of St. Charles, Illinois Plan Commission Resolution No. <u>17-2020</u>

A Resolution Recommending Approval of a Map Amendment, Special Use for Planned Unit Development and PUD Preliminary Plan for Munhall Glen (Court Airhart, Airhart Construction Corp.)

Passed by Plan Commission on September 9, 2020

WHEREAS, it is the responsibility of the St. Charles Plan Commission to hold public hearings and review requests for Map Amendments and Special Uses; and,

WHEREAS, the Plan Commission held a public hearing and reviewed the petitions for Map Amendment, Special Use for PUD and PUD Preliminary Plan for Munhall Glen (Court Airhart, Airhart Construction Corp.); and,

WHEREAS, in accordance with Section 17.04.320.D, the Plan Commission has considered the following findings for Zoning Map Amendment:

FINDINGS OF FACT FOR MAP AMENDMENT

1. The existing uses and zoning of nearby property.

The property is currently zoned a mix of RS-4 and M-2. The property to he south is zoned RS-4 and so this property melds well to the existing residential. The property to the east is zoned M-2 and used as office space. The buildings have a residential feel and will work well with this property. The properties to the north and west are M-2 and St. Charles owned properties. The zoning change to this property will be a positive for this area and act well as a transition between the residential to the south and more commercial uses to the north.

2. The extent to which property values are diminished by the existing zoning restrictions.

The highest and best use of this property is residential under the RS-4 classification with a PUD. This property will fill a niche of unmet need of first floor master bedroom housing and is a much better use than the current M-2 zoning. The M-2 zoning in this location is unneeded due to the properties to the east of this property toward the DuPage Airport and the Route 64 corridor. The fact that this property has not been developed under M-2 and left as a field while every property around it has been developed is a testament to that fact. This property has been underperforming on the tax rolls as farm land and one residential property. The change in zoning will be a great benefit to the tax rolls and improve its value to the City of St. Charles.

3. The extent to which the reduction of the property's value under the existing zoning restrictions promotes the health, safety, morals or general welfare of the public.

This is not applicable. The current zoning has no benefit to the health, safety, morals or general welfare of the public. In fact, the change in zoning will be a great benefit of the health, safety, morals and general welfare of the public by meeting housing needs and creating a much more orderly procession of development.

4. The suitability of the property for the purposes for which it is presently zoned, i.e. the feasibility of developing the property for one or more of the uses permitted under the existing zoning classification.

The property is not suitable for the purpose for which it is presently zoned. The traffic patterns into the residential area make commercial traffic an issue as well as the location being into a residential neighborhood. The fact that it has sat for so long underutilized while every property around it many, many years ago had been developed is testament to the fact that the value of the property is not M-2. There are many much better options to the east for commercial usages and the best use of this property is rezoning to RS-4 under a PUD.

5. The length of time that the property has been vacant, as presently zoned, considered in the context of the land development in the area where the property is located.

This property is the last property in this area for development. It has been many years since all the properties around it have been developed and this property has sat underutilized. Due to the inadequacies for development as M-2 is the reason it has been left behind as other properties have been developed. The change in zoning to RS-4 PUD residential usage makes much more sense in this area.

6. The evidence, or lack of evidence, of the community's need for the uses permitted under the proposed district.

The rezoning of the property to RS-4 PUD will meet a substantial need of single level and first floor master bedroom homes in St. Charles. The demographics show that this is a substantial void in the market. The change in zoning allowing housing to be built on this property will allow those individuals who need first floor master bedrooms in St. Charles to stay in St. Charles rather than moving away from their community. It will create a better mix of housing in St. Charles meeting the needs of more residents and creating a greater mix of housing in the community.

7. The consistency of the proposed amendment with the City's Comprehensive Plan.

The change in zoning to RS-4 PUD conforms to the purposes and intent of the Comprehensive Plan by promoting development within the current boundaries of the City. It focuses development on an underutilized property thereby enhancing the tax base, utilizing surrounding infrastructure instead of needlessly extending infrastructure past undeveloped properties. It provides housing close to shopping districts and the downtown

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areas as well as promoting development in an area with significant road and transportation corridors promoting orderly and efficient development.

8. Whether the proposed amendment corrects an error or omission in the Zoning Map.

Not applicable.

9. The extent to which the proposed amendment creates nonconformities.

The rezoning of this property will not create any nonconformities.

10. The trend of development, if any, in the general area of the property in question.

The trend in development for M-2 is to be in better transportation corridors specializing in ease of commercial traffic flow for large trucks. This property odes not meet that need. The need is for residential in this area and this change in zoning will allow housing for those looking for single level living or first floor master bedrooms. Changing to RS-4 PUD allows for housing and will be a much better use for the property.

WHEREAS, in accordance with Section 17.04.410.D, the Plan Commission finds said petitions to be in the public interest of the City of St. Charles based upon the following criteria for Planned Unit Developments:

CRITERIA FOR PLANNED UNIT DEVELOPMENTS (PUDs)

- i. The proposed PUD advances one or more of the purposes of the Planned Unit Development procedure stated in Section 17.04.400.A:
 - 1. To promote a creative approach to site improvements and building design that results in a distinctive, attractive development that has a strong sense of place, yet becomes an integral part of the community.
 - 2. To create places oriented to the pedestrian that promote physical activity and social interaction, including but not limited to walkable neighborhoods, usable open space and recreational facilities for the enjoyment of all.
 - 3. To encourage a harmonious mix of land uses and a variety of housing types and prices.
 - 4. To preserve native vegetation, topographic and geological features, and environmentally sensitive areas.
 - 5. To promote the economical development and efficient use of land, utilities, street improvements, drainage facilities, structures and other facilities.
 - 6. To encourage redevelopment of sites containing obsolete or inappropriate buildings or uses.
 - 7. To encourage a collaborative process among developers, neighboring property owners and residents, governmental bodies and the community

The proposed PUD promotes a creative solution to an unmet growing housing need of single story and first floor master bedroom housing. This housing will allow for long-time community residents, business people and leaders in St. Charles to remain in St. Charles as their housing needs change. The PUD zoning promotes creative housing and provides attractive streetscapes that incentivizes porches and pedestrian friendly neighborhoods. It promotes social interaction by providing sidewalks, paths, neighbourhood connection areas as well as connection to a potential linear park to the north of the property. The PUD provides a harmonious usage of the property by changing a potential heavy commercial use of the property to residential use more in scale with the residential use to the south and the less intense commercial use to the east. The development of this property will promote higher levels of landscaping and higher quality tress than currently exist on the property as uncontrolled Buckthorn, Honeysuckle, Mulberry, Box Elder, etc. and other invasive landscaping is allowed to multiply. The installation of detention areas with natural landscaping will enhance water quality and native plants. Munhall Glen will be a benefit to future residents, the surrounding neighboors, local businesses and the City of St. Charles.

- ii. The proposed PUD and PUD Preliminary Plans conform to the requirements of the underlying zoning district or districts in which the PUD is located and to the applicable Design Review Standards contained in Chapter 17.06, except where:
 - A. Conforming to the requirements would inhibit creative design that serves community goals, or
 - B. Conforming to the requirements would be impractical and the proposed PUD will provide benefits that outweigh those that would have been realized by conforming to the applicable requirements.

The proposed RS-4 PUD and PUD Preliminary Plans provide a significantly more harmonious usage of the property than the current more intrusive usage and negative impact on the surrounding properties of the existing M-2 (industrial) zoning classification currently in place. By allowing for the PUD the property will be able to meet a significant housing need in the community of single level and first floor master bedroom housing. This housing would not be allowed in the M-2 zoning classification. By changing the zoning and allowing the PUD, storm water facilities, open space, and sidewalks and paths will allow for residents to enjoy the open space and property as well as provide for a pedestrian connection to the potential linear park to the north. The planned landscaping will be a significant improvement to the invasive species currently on the property and the trees planted as part of the development will be a significant improvement. Due to the size and shape of the property by downzoning the property to an RS-4 PUD it allows for implementation of a variety of lot sizes which provides for varied architecture, improved rear yard setbacks and a unique streetscape that incentivizes porches and neighbour interaction than the current M-2 zoning classification allows. The development will provide stormwater facilities with native species enhancing the water quality where currently no storm water facilities exist. The RS-4 PUD will enhance the opportunity for single level living while not infringing on the size of the homes for the those requiring

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single level living. The PUD promotes quality residential development and provides good transitional zoning to benefit those properties that currently surround it. The development will eb a benefit to the City of St. Charles housing and significant increase in tax base.

iii. The proposed PUD conforms with the standards applicable to Special Uses (section 17.04.330.C.2):

A. Public Convenience: The Special Use will serve the public convenience at the proposed location.

The Special Use will serve the public convenience at Munhall Glen by filling a housing void in the market of single story and first floor master bedroom homes. This void in the market is causing those needing this type of housing to look outside of St. Charles even though they have been long time residents. In addition, by providing housing in this location it helps support area businesses and supports good planning putting residential housing close to both public and private amenities.

B. Sufficient Infrastructure: That adequate utilities, access roads, drainage and/or necessary facilities have been, or are being, provided.

There is sufficient infrastructure and utilities in this area to support the development. There is a major sanitary sewer main on the north end of the property installed for the future development of this property. The utility infrastructure installed on this property will help with the connectivity of utilities, specifically water main, which will help "loop" the water system in the area and provide for better servicing and water circulation. The installation of storm water controls and Best Management Practices on this property will provide stormwater detention where no stormwater controls currently exist. The traffic pattern will provide excellent vehicular movement because Munhall Glen exits onto a major collector, Tyler Road, which links to principal arterials of E. Main St. and Kirk Rd. providing for safe and efficient vehicular movement.

C. Effect on Nearby Property: That the Special Use will not be injurious to the use and enjoyment of other property in the immediate vicinity for the purposes already permitted, nor substantially diminish or impair property values within the neighborhood.

Munhall Glen will not be injurious to the use and enjoyment of surrounding properties and it will act as an excellent transition from the commercial to the east and west and the residential to the south. By approving this Special Use, the downzoning of this property from M-2 Limited Manufacturing to RS-4 PUD will ensure a more harmonious residential usage of the property and ensure Munhall Ave. stays primarily a residential street rather than negatively impacted by commercial heavy trucking transportation uses.

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D. Effect on Development of Surrounding Property: That the establishment of the Special Use will not impede the normal and orderly development and improvement of the surrounding property for uses permitted in the district.

The establishment of the Special Use will not impede the normal and orderly development and improvement of the surrounding properties due to the fact that all surrounding properties are currently developed. As the last piece of property in this area for development the approval of the Special Use will in fact promote a harmonious interconnecting and buffer for the surrounding properties.

E. Effect on General Welfare: That the establishment, maintenance or operation of the Special Use will not be detrimental to or endanger the public health, safety, comfort or general welfare.

The approval of the Special Use will not be detrimental to or endanger the public health, safety, comfort or general welfare. As planned, the property's development will provide buffering between different property usages, will extend and improve municipal infrastructure, and will provide housing needed in the City of St. Charles.

F. Conformance with Codes: That the proposed Special Use conforms to all existing Federal, State and local legislation and regulation and meets or exceeds all applicable provisions of this Title, except as may be varied pursuant to a Special Use for Planned Unit Development.

The proposed Special Use conforms to all applicable provisions of the St. Charles Municipal Code and meets or exceeds all applicable provisions of this Title, except as varied pursuant to the Special Use for Planned Unit Development. The Special Use for PUD zoning allows for a more inventive design, the average lot sizes are significantly larger than the minimum requirements, and the housing will be constructed at or above current codes and energy requirements. The Special Use and minor changes to the zoning requirements allows for an inventive solution for meeting a needed housing niche within the St. Charles housing stock.

iv. The proposed PUD will be beneficial to the physical development, diversity, tax base and economic well-being of the City.

The proposed PUD will be beneficial to the physical development, diversity, tax base and economic well-being of the City. The development of this property will improve connections to the water and sewer systems and provide storm water management facilities where none currently exist. It will provide a diversity of housing by providing single story and first floor master bedroom housing providing solutions for current St. Charles residents whose housing needs have changed over time and want to stay in the City due to civic, cultural, social and religious activities that they have long time connections. The PUD will substantially increase the tax base for the City, School District, Park District, etc. over the current use in perpetuity benefiting many taxing

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bodies. In addition, it provides housing close to many commercial districts benefiting many surrounding businesses and the economic wellbeing of the City.

v. The proposed PUD conforms to the purposes and intent of the Comprehensive Plan.

The proposed PUD conforms to the purposes and intent of the Comprehensive Plan by promoting development within the current boundaries of the City. It focuses development on an underutilized property thereby enhancing the tax base, utilizing surrounding infrastructure instead of needlessly extending infrastructure past undeveloped properties. It provides housing close to shopping districts and the downtown area as well as promoting development in an area with significant road and transportation corridors promoting orderly and efficient development.

NOW, THEREFORE, be it resolved by the St. Charles Plan Commission to recommend to the City Council approval of a Map Amendment, Special Use for Planned Unit Development and PUD Preliminary Plan for Munhall Glen (Court Airhart, Airhart Construction Corp.), subject to the following conditions:

- 1. Connection to South Ave. shall be a full public access.
- 2. Resolution of all staff comments prior to City Council action.

Roll Call Vote:

Ayes: Purdy, Kessler, Becker, Holderfield, Wallace, Funke, Melton, Vargulich, Pretz

Nays: Absent: Recused: 0

Motion carried: 9-0

PASSED, this 9th day of September 2020.

 			Chairmar
St C	harles	Plan (Commission

Community & Economic Development Community Development Division

Phone: (630) 377-4443 Fax: (630) 377-4062



Staff Report

TO: Chairman Rita Payleitner

And Members of the Planning & Development Committee

FROM: Ellen Johnson, Planner

RE: Munhall Glen PUD

DATE: September 10, 2020

I. APPLICATION INFORMATION:

Project Name: Munhall Glen

Applicant: Court Airhart, Airhart Construction Corp.

Purpose: Development approvals for a 50-lot single-family subdivision

	Site Information
Location	West of Munhall Ave. at Tyler Rd. (5 parcels)
Acres	670,397 sf / 15.39 acres
Applications:	Map Amendment
11	Special Use for Planned Unit Development
	PUD Preliminary Plan
Applicable	Ch. 17.12 – Residential Districts
City Code	Ch. 17.26 – Landscaping & Screening
Sections	Title 16 Subdivisions & Land Improvement
	Existing Conditions
Land Use	Single-Family Dwelling (1 parcel); Vacant/Agriculture (4 parcels)
Zoning	RS-4 Suburban Single-Family Residential (1 parcel); M-2 Limited Manufacturing
-	(4 parcels)

Zoning Summary					
North	M-2 Limited Manufacturing; M-1 Special	Medical/office park; multi-			
	Manufacturing; BC Community Business/PUD	tenant comm./industrial bldgs			
East	M-2 Limited Manufacturing; RS-4 Suburban	Auto repair; medical/office park;			
	Single-Family Residential	Ryder Truck Rental			
South	RS-4 Suburban Single-Family Residential	Single-family homes			
West	M-2 Limited Manufacturing; PL Public Lands	Multi-tenant industrial bldg; City			
	_	supply yard			

Comprehensive Plan Designation
Industrial/Business Park (1 parcel); Single-Family Detached Residential (4 parcels)

Aerial





II. OVERVIEW

A. <u>BACKGROUND</u>

The 15-acre subject property encompasses five parcels west of S. Tyler Rd., accessed from Munhall Ave. The parcels are under common ownership. Three of the parcels (13.5 acres) are farmed. A single-family house addressed as 872 Munhall Ave. is constructed on one of the parcels, with the last small parcel extending out from the house lot towards Tyler Rd.

B. CONCEPT PLAN

In June 2020, the Plan Commission reviewed a Concept Plan for a 51-lot single-family subdivision called Munhall Glen, submitted by Airhart Construction Corp. Plan Commission expressed general support for the proposed single-family land use, and offered the following feedback:

- A roadway connection to South Ave. should be incorporated into the plans to promote public safety, accessibility and mobility.
- Mature, high-quality trees on the site should be preserved where possible.
- Additional open space should be incorporated where possible.
- Some concerns were expressed about the overall density and lot sizes / lot coverage.

Members of the public that participated in the meeting expressed a desire for more greenspace and stated concerns about the overall density and traffic impacts on the surrounding neighborhood.

Planning & Development Committee reiterated some of the Plan Commission comments, including general support for the land use and overall project, with some concerns regarding density. Committee members also expressed a preference for a South Ave. roadway connection.

C. PROPOSAL

Court Airhart of Airhart Construction Corp. has filed zoning applications requesting approval of a single-family subdivision on the subject property. The site plan is similar to the Concept Plan previously reviewed.

Details of the proposal are as follows:

- Rezoning to the RS-4 Suburban Single-Family Residential District (6,600 sf min. lot size).
- Demolition of the existing single-family house fronting Munhall Ave.
- Access to the subdivision from Munhall Ave. with gated emergency-only connection to South Ave.
- Internal looped public street configuration with sidewalks on both sides of the street.
- 50 single-family home lots:
 - O Lot sizes range from $6{,}307 \text{ sf} 15{,}263 \text{ sf}$. Average lot area = $8{,}154 \text{ sf}$.
 - \circ Lot widths range from 48 ft. -95 ft. Average lot width = 63.7 ft.
 - o Variety of single- and two-story home models (approx. 1,600-3,000 sf; 2-4 bedrooms).
 - Wider lots will accommodate the "Garden Series" models (1 & 1½-story).
 - Narrower lots will be available for the "Premier Series" models (1 & 2-story).
- Two stormwater detention areas with adjacent "pocket parks".

The following Zoning Applications have been submitted in support of this project:

- 1. Map Amendment To rezone the property from the M-2 Limited Manufacturing District and RS-4 Suburban Single-Family Residential District to entirely RS-4 Suburban Single-Family Residential District.
- **2. Special Use for Planned Unit Development** To establish a PUD with unique development standards for the property.
- **3. PUD Preliminary Plan** To approve preliminary engineering plans, landscape plan, and preliminary plat of subdivision.

III. COMPREHENSIVE PLAN

The Land Use Plan adopted as part of the 2013 Comprehensive Plan identifies the four parcels of the subject property fronting Munhall Ave. as "Single-Family Detached Residential", consistent with the proposed land use. However, the rear parcel is identified as "Industrial/Business Park".

Single-Family Detached Residential is described in the plan as follows (p. 38):

Single family detached residential areas should



consist primarily of single family detached homes on lots subdivided and platted in an organized and planned manner. Downtown, single family residential areas consist primarily of older buildings, many rehabilitated, with small yards and minimal garage space. Single family residential detached homes are the most prevalent building type in the community, and should continue to be so.

The Industrial/Business Park land use category is described in the plan as follows (p.39):

Areas designated for industrial/business park are intended to accommodate a variety of uses ranging from light assembly, storage and distribution, low intensity fabrication operations, research and "tech" industry applications, intense commercial service uses, and more. These areas are also intended to provide for business park/office park uses, which could include "stand alone" office buildings and complexes or several buildings incorporated into a "campus like" setting.

The plan notes these locations are located in areas, "...where they can capitalize on close proximity to regional transportation networks while minimizing negative impacts on residential neighborhoods. (p.47)"

The Plan provides the following Residential land use policies relevant to the proposed development: (p. 44):

Prioritize infill development over annexation and development. While the era of substantial residential growth is over in St. Charles, there remain some isolated opportunities for residential development on the City's west side. While most of these opportunities are within unincorporated Kane County, they fall within the City's 1.5-mile extraterritorial planning jurisdiction defined by State statute. It is recommended that the City carefully consider annexation and growth into these areas while vacant and/or

underutilized residential properties exist within the City's boundaries. When residential development does occur within the City's growth areas, it should occur in areas immediately adjacent to existing developed areas so as to prevent "leap frog" development and the resulting costs and burdens of unnecessarily extending infrastructure systems in an unwise manner.

Ensure residential areas are adequately screened/ buffered from adjacent non-residential uses and activity. The composition of the City's commercial districts along corridors that transect the City means that there are many areas where commercial uses abut residential properties and neighborhoods. The use of horizontal and vertical buffering and screening, including berms, fencing, and landscaping, should be promoted to protect neighborhoods from abutting commercial or industrial land uses. The City should identify areas where land use conflicts are problematic and explore solutions to mitigate the conflicts, including buffering and screening. Additionally, the City's landscape ordinance could be revised to require enhanced screening and an amortization schedule to ensure compliance for non-conforming properties within a set time frame.

IV. ANALYSIS

A. ZONING

The subject property is zoned RS-4 Suburban Single-Family Residential and M-2 Limited Manufacturing. Proposed is rezoning the entire property to the RS-4 District, the purpose of which is stated in the Zoning Ordinance as follows:

"To accommodate medium to high-density single-family residential development in the City. The minimum lot size in this district is six thousand six hundred (6,600) square feet. The RS-4 District also provides for limited institutional uses compatible with surrounding residential neighborhoods."

RS-4 zoning is consistent with the adjacent residential neighborhood to the south. This subdivision was platted in the late1970s and is known as Cambridge.

PUD approval is also requested to accommodate certain deviations from bulk standards of the RS-4 District.

The table below compares the RS-4 District requirements with the proposed plan. Requested zoning deviations are denoted in **bold italics**. The plan proposes a variety of lot sizes and lot widths. Zoning deviations are needed to allow some lots under the 6,600 sf minimum lot area, and some under the 60 ft. minimum lot width. Deviations from building coverage and interior side yard setbacks are also requested.

	RS-4	Proposed Plan
Min. Lot Area	6,600 sf	Range from 6,307 sf – 15,263 sf (Average: 8,154 sf 13 lots are under 6,600 sf)
Min. Lot Width	60 ft.	Range from 48 ft. – 95 ft. (Average: 63.7 ft 23 lots are under 60 ft.)
Max. Building Coverage	30%	37.5%

Max. Building Height	34 ft. / 2 stories, whichever is less	34 ft. / 2 stories
Min. Front Yard	20 ft.	20 ft.
Min. Exterior Side Yard	15 ft.	15 ft.
Min. Interior Side Yard	Combined width of 14 ft., neighbor less than 5 ft.	Combined width of 12 ft., neither side less than 6 ft.
Min. Rear Yard	30 ft.	30 ft.

B. <u>LANDSCAPING</u>

A landscape plan has been submitted as part of the PUD Preliminary Plan. The plan depicts street trees within the parkway and trees along the rear lot lines of each lot. Planting plans for the two "pocket parks" located on the outlots are also included. A homeowners' association will be responsible for maintaining the outlots.

Initially, trees were depicted within the 10 ft. wide public utility and drainage easements along the rear lot lines. Trees are prohibited within the easement. A revised landscape plan has been submitted. For Lots 1-38, the applicant is proposing to shift the easement 5 ft. into the rear yards to allow for a 5 ft. wide planting area within which trees could be planted. For Lots 39-50 (internal lots), trees would be shifted out of the 10 ft. easement and planted closer to the houses.

Staff Comments

- The revised placement of the rear yard trees will need to be reviewed by engineering.
- Privacy fencing should be considered for lots backing up to non-residential land uses.
- Planting plans are needed for the stormwater detention areas.

Tree Preservation Plan

The applicant has submitted a Tree Preservation Plan. Per Section 8.30.070 of the City Code, Tree Preservation Plans shall include a survey of existing trees 6" or more in diameter, with tree preservation zones identified.

The submitted Tree Preservation Plan splits the subject property into five zones. Trees greater than 6" in diameter were surveyed in the Residential Zone. General composition of vegetation was documented in the remaining zones. A total of 10 trees were surveyed in the Residential Zone. 9/10 are recommended for protection, if possible. However, the Plan notes that proposed construction will impact 100% of the site due to grading, etc., which will affect all existing trees. It notes that new parkway trees and rear yard trees will be, "more beneficial to the long-term health of the trees on the site as well as neighboring properties".

City Code states that high-quality trees should be preserved and protected. Removal of such trees is authorized when one or more of the following conditions exist:

- 1. The tree is diseased, dead or dying.
- 2. The tree is damaged or injured to the extent that it is likely to die or become diseased, or such that it becomes a hazard.
- 3. Removal of the tree is consistent with good forestry practices, that is, consideration is given to the species of the tree, location, conditions, age, safety, and the historic and aesthetic value of the tree to be removed.

- 4. Removal of the tree will enhance the health of remaining trees within the immediate vicinity.
- 5. Removal of the tree is required to repair a sewer line or water main, or excavation for such repair will damage the tree to the extent that it is likely to die or become diseased.
- 6. All reasonable efforts have been undertaken in the land planning, architectural and engineering design of the proposed building, building addition, development or site improvement to minimize tree damage and/or removal, and the tree or trees must be removed to provide adequate space for the permitted construction.

E. SITE ACCESS / STREET IMPROVEMENTS / TRAFFIC STUDY

Full access to the development is proposed from Munhall Ave., with secondary access via a connection to South Ave. to the west. The internal public street forms a looped configuration. The looped street is proposed as a public street. Proposed ROW width is 60 ft. with 27 ft. wide pavement measured from curb face. This pavement width permits parking on one side of the street.

South Ave. Connection

The plans depict the connection to South Ave. with a gate at the property line with the intent that the connection be utilized as an emergency fire access road. Various City departments have expressed preference for a full, open connection to South Ave. The Fire Dept. cited snow removal concerns if the connection is gated. Public Works is not supportive of a gate blocking public right-of-way and prefers to require South Ave. to be constructed to meet current City Code, including curb, gutter and sidewalk up to the end of the existing pavement, approx. 350 ft. west of the subject property. The Police Dept. has requested that a full South Ave. connection be provided, citing that through streets assist public safety in response times and points of access. From a Planning perspective, a full connection would promote interneighborhood connectivity and would provide residents with a more convenient route to downtown.

The applicant noted during the Plan Commission public hearing that they are agreeable to removing the gate, but request the connection be restricted to emergency vehicles only. Signage would be utilized to help enforce this restriction.

Traffic Impact Study

The applicant has submitted a Traffic Impact Study prepared by Gewalt Hamilton Associates, Inc., updated 9/2/2020. The study analyses the impact of the proposed development on the adjacent roadway network and concludes that impact will be minimal.

Regarding trip generation, the development is expected to generate 40 trips during the morning peak hour and 52 trips during the evening peak hour. 55% of site traffic is anticipated to travel to/from the site via Rt. 64, with the reminder on Tyler Rd. and 7th Ave.

Capacity analyses were performed at four intersections under gated South Ave. and open South Ave. access scenarios. The four intersections analyzed were Tyler Rd. at Munhall Ave., Indiana Ave. at 13th Ave. (north leg), Indiana Ave. at 13th Ave. (south leg), and Munhall Ave. at the site access. All intersections are expected to operate at a Level of Service A or B (considered "desirable" based on the LOS ranking system). The study notes the gated vs. open connection to South Ave. has minimal impact on intersection operations.

The study was reviewed on behalf of the City by HLR Engineering. HLR provided a letter dated 9/4/2020 stating it concurs with the findings of the study. One minor revision to Exhibit 6B is needed.

F. ENGINEERING

Engineering review comments have been provided to the developer. Certain comments have been discussed with the developer and will require further coordination between the City and developer. These include City replacement of a sanitary sewer main at the north end of the site, improvements to a culvert on the railroad property north of the site, and how stormwater BMPs will be incorporated. All comments will need to be addressed prior to City Council approval of the PUD Preliminary Plan.

D. PLAT OF SUBDIVISION

A Preliminary Plat of Subdivision has been submitted as part of the PUD Preliminary Plan. The plat proposes the following:

- 50 buildable single-family home lots (Lots 1-50).
- Two outlots covering the stormwater detention areas (Outlots A & B). An HOA will be required to own and maintain the outlots.
- Dedication of internal streets to the City (60 ft. right-of-way).
- Dedication of an extension of South Ave. to the City (60 ft. right-of-way).
- 10 ft. public utility and drainage easements along front and rear property lines.

Approval of a Final Plat of Subdivision will be required after PUD Preliminary Plan approval.

Staff Comments:

- Add "St. Charles" to the location description beneath the subdivision name.
- 10 ft. public utility & drainage easements are needed around the full perimeter of each lot (including outlots, except where easements have previously been granted). Where the side lot line abuts the side lot line of another lot, easement width may be reduced to 5 ft. on each side.
- Stormwater detention easement provisions are provided twice (page 4). Remove the duplicate provisions under the PU&DE provisions.
- Remove public access easement provisions as no public access easements are proposed.
- Remove the County Engineer's certificate.

E. BUILDING ARCHITECTURE

The applicant has submitted a number of home model designs intended to be offered for the proposed development. Buildings in the RS-4 District are not subject to Design Review, nor are architectural plans required to be approved as part of a single-family residential PUD.

The Zoning Ordinance does not contain any monotony restrictions prohibiting, for example, the same models from being constructed on adjacent lots. For one recent residential PUD approved by the City, language was included in the PUD ordinance requiring a monotony code be incorporated into the Homeowners Association Declaration of Covenants, with a summary of the monotony code restrictions included in the PUD Ordinance.

The applicant stated during the public hearing that they are agreeable to imposing monotony restrictions on the development, such as prohibiting the same elevations from locating adjacent and across the street.

F. INCLUSIONARY HOUSING

The Inclusionary Housing Ordinance, Title 19 of the City Code, requires construction of, or fee in-lieu for, affordable units as a percentage of any new residential development. The Inclusionary Housing worksheet submitted by the applicant proposes payment of a fee in-lieu of providing affordable units. The fee in-lieu amounts to \$194,362.18.

G. SCHOOL AND PARK DISTRICT

Land-Cash worksheets submitted by the applicant have been forwarded to St. Charles School District #303 and the St. Charles Park District for review and comment. Full cash contributions are proposed in accordance with Title 16 of the City Code.

IV. PLAN COMMISSION RECOMMENDATION

Plan Commission held a public hearing on 9/9/20. No members of the public spoke at the hearing. One email was received from a neighboring unit owner of Tyler Ridge Business Park citing drainage concerns. Another neighbor stated support for the project. These emails are attached to the packet.

The Commission voted 9-0 to recommend approval, subject to the following conditions:

- 1. The South Ave. connection shall be a full public access.
- 2. Resolution of staff comments prior to City Council action.

VI. ATTACHMENTS

- Fee Request Letter
- Emails from Residents
- Applications: Map Amendment; Special Use for PUD; PUD Preliminary Plan; received 7/30/2020
- Tree Preservation Plan
- Preliminary Plat of Subdivision
- Preliminary Engineering Plans
- Landscape Plan (Revised)
- House Renderings
- Traffic Study / HLR Review Letter

"Building a better tomorrow"

September 11th, 2020

Ellen Johnson
City Planner
Community & Economic Development
City of St. Charles
2 E. Main Street
St. Charles, IL 60174

Re: School, Park and Affordable Housing Fees - Munhall Glen

Dear Ellen,

We had previously requested in a letter dated May 21st, 2020, that the School and Park fee in lieu and the Affordable Housing fees be reduced due to the population makeup of the target market of this housing product. We believe the documentation we supplied provided proof that warranted the modification to the fee structure.

If that is not a decision that will be granted, we request that the fees be paid at time of building permit approval for each lot. We think this is reasonable for a variety of reasons:

- 1) The population impact of the housing, if there is any, will not occur on the City, School District, and Park District for approximately 6 months after building permit issuance.
- 2) The fees combine to approximately \$840,000 and the homes will be built over a 4-5 year period. The carry cost alone for these fees could be in the range of \$40,000-\$60,000 per year over the life of the project. This will greatly affect selling prices, affordability and negatively impact the target market of the properties.
- 3) The increase in tax revenue in perpetuity of Munhall Glen to all of the local taxing bodies, while the impact is still questionable, is significant.
- 4) Since these homes could be 2, 3, or 4 bedrooms, it is reasonable to wait so that the fees are properly paid at time of building permit.

While we still believe the fees should be reduced, for all of the reasons above we think it is a logical decision to allow the fees to be paid at time of building permit issuance, instead of prior to recording of the final plat. We ask that you take the above into consideration and grant our request.

Sincerely,

Court Airhart
Airhart Construction

President



From: Christine Czajka <ckc7478@att.net> Sent: Tuesday, September 1, 2020 2:28 PM

To: CD <cd@stcharlesil.gov>

Cc: Christine Czajka <ckc7478@att.net>; ICE - Adrian Czajka <czajkaedi@aol.com>

Subject: Munhall Glen

Our property backs up to the proposed construction development of Munhall Glen from the Airhart construction Company.

We are definitely for the Munhall Glen project with Airhart construction.

It would increase our property values, as the existing zoning would diminish our property values.

Therefore, yes, we look forward to the hopeful future construction of Munhall Glen with Airhart construction Company.

Sincerely, Adrian and Christine Czajka Adams Ave., Saint Charles Sent from my iPhone From: Richard MacDonald <rmaccpa@gmail.com>

Sent: Tuesday, September 8, 2020 11:40 AM

To: CD <cd@stcharlesil.gov>

Subject: Munhall Glen Project @S. Tyler Road

Dear ladies and Gentleman:

I own Unit A /B-1 of the 525Tyler road complex (adjacent to the north east corner of the proposed development). My complex leadership has not responded to my inquiry of their action, so I am writing this.

Please note there are major elevation differences between the developing property and the 525 Tyler road business complex, especially along the north/east border. This causes major drainage and erosion, mud and water drainage problems along the shared borders of these properties.

Can you please address the necessary retaining walls, and drainage issues needed to safeguard our properties? Especially along the current house and fence location (proposed building #4) area.

Will the trees overhanging my unit be removed? Without the trees and/or a retention wall, this area will cause problems for Building #1 of our complex.

Thank you for your time and consideration.

Richard MacDonald Goob Property, member 525 Tyler Road, Suite A St. Charles, IL 60174 Phone 630-215-8706

E-mail:Rmaccpa@gmail.com

Prepared For: Airhart Construction 500 E Roosevelt Rd. West Chicago, IL 60185

Munhall Glen Subdivision – Tree Inventory and Preservation Plan

The existing project site is approximately 15.39 acres and contains a mixture of residential, agricultural, wetland and woodland areas. This report will break down the dense woodland areas by zones and will provide general information of each zone. Per discussion with staff and due to dense invasive undergrowth, specific information was provided for trees greater than 6" Diameter at Breast Height (DBH) (species, size and condition) in the residential zone. The rest of the zones have a dense brush layer, typical to invasive tree species making access challenging. If a more detailed analysis will be required, a significant amount of brush will need to be removed to inventory these areas.

The tree preservation zones are as follows:

- Residential Zone Individual Trees identified
- South Zone Abutting Residential General Community Composition
- West Zone Abutting Yard General Community Composition
- East Zone Abutting Commercial General Community Composition
- Wetland / Drainage Swale General Community Composition

Below is a table with a list of the tree species found on-site. Included in this table is each species each species' Coefficient of Conservatism (C Value) and native/invasive status. C Values range from 0 - 10 and represent an estimated probability that a plant is likely to occur in a landscape relatively unaltered from what is believed to be pre-European settlement condition. For example, a C of 0, is given to plants such as *Acer negundo*, Box Elder, that have demonstrated little fidelity to any remnant natural community (i.e. may be found almost anywhere). Similarly, a C of 10 is applied to plants that are almost always restricted to a presettlement remnant (i.e. a high-quality natural area). Invasive (non-native) species are assigned a C-value of 0 and are considered invasive.

SPECIES NAME (NWPL/ MOHLENBROCK)	COMMON NAME	C VALUE	NATIVITY
Acer negundo	Box Elder	0	Native
Acer saccharinum	Silver Maple	1	Native
Juglans nigra	Black Walnut	3	Native
Morus alba	White Mulberry	0	Invasive
Populus deltoides	Eastern Cottonwood	0	Native
Prunus serotina	Black Cherry	0 Native	
Quercus alba	Northern White Oak	5 Native	

Rhamnus cathartica European Buckthorn		0	Invasive
Robinia pseudoacacia	Black Locust	0	Invasive
Ulmus pumila Siberian Elm		0	Invasive

This floristic information was obtained from Herman, B., Sliwinski, R. and S. Whitaker. 2017. Chicago Region FQA (Floristic Quality Assessment) Calculator. U.S. Army Corps of Engineers, Chicago, IL. This version of the calculator has floristic information pertaining to the Chicago Region as originally delineated by the authors of the Floristic Quality Assessment - Swink, F. and Wilhelm, G. 1994. Plants of the Chicago Region, 4th Ed. The Indiana Academy of Science, Morton Arboretum, Lisle, IL. Updated Coefficients of Conservatism (C-value) are based on Flora of the Chicago Region: A Floristic and Ecological Synthesis. Gerould Wilhelm & Laura Rericha. 2017. Indiana Academy of Science. Indianapolis, IN.

Residential Zone

The residential zone is the only zone that an individual tree identification was able to occur. The following table represents the Trees Inventoried and their associated Rating. The rating is based on tree health and structural integrity.

			1		
Tree #	DBH	Common Name	Species	Rating	Recommendation
1	15"	Maple	Acer	Good	Protective Fence if possible
2	42"	Oak	Quercus	Good	Protective Fence if possible
3	10"	Maple	Acer	Good	Protective Fence if possible
4	14"	Maple	Acer	Poor	Remove
5	38"	Oak	Quercus	Good	Protective Fence if possible
6	12"	Oak	Quercus	Good	Protective Fence if possible
7	14",14"	Elm	Ulmus	Good	Protective Fence if possible
8	36"	Oak	Quercus	Fair	Protective Fence if possible
9	44"	Oak	Quercus	Good	Protective Fence if possible
10	38"	Oak	Quercus	Good	Protective Fence if possible

South Zone Abutting Residential

The area inventoried along the south edge property line abuts the Cambridge Phase No.1 residential subdivision to the south and is approximately 1.70 acres of woodland. The trees in this area consisted of Buckthorn, Black Locust, mulberry, Cottonwood, Silver Maples, and Boxelder. The majority of the trees in this area are invasive and non-desirable but are in fair to good condition and provide screening from the residential subdivision to the agricultural area. There are several good size Silver Maples in this area that could be considered desirable, though the effects from grade change, due to storm water management requirements and construction would likely cause them to not survive.

Rough Percentages of trees found in this area:

Acer negundo (Boxelder): <5%

Acer saccharinum (Silver Maple): 40%

Morus alba (Mulberry): <5%

Populus deltoides (Cottonwood): <5% Rhamnus cathardica (Buckthorn): 40% Robinia pseudoacacia (Black Locust): <5%

West Zone Abutting Yard

The area inventoried along the west edge property line abuts an industrial material yard and is approximately 0.6 acres of woodland. The trees in this area consisted of Buckthorn, Siberian Elm, Cottonwood, Silver Maples, Boxelder, Cherry, and Walnut. The majority of the trees in this area are invasive and non-desirable but are in fair to good condition and provide screening from the industrial yard to the west. There are several Walnut, Silver Maples, Cottonwoods & Siberian Elms in fair condition in this area that could be considered desirable, though the effects from grade change, due storm water management requirements and construction would likely cause them to not survive.

Rough Percentages of trees found in this area:

Acer negundo (Boxelder): <5%

Acer saccharinum (Silver Maple):10%

Juglans nigra (Walnut): 15%

Populus deltoides (Cottonwood): 10%

Prunus serotina (Cherry): <5%

Rhamnus cathardica (Buckthorn): 45% Ulmus pumila (Siberian Elm): 10%

East Zone Abutting Commercial

The area inventoried along the east edge property line abuts the Tyler Ridge Condo Subdivision and is approximately 1.1 acres of woodland. The trees in this area consisted of Buckthorn, Siberian Elm, Silver Maples, Boxelder, Cherry, and Mulberry. The majority of the trees in this area are invasive and non-desirable but are in fair to good condition and provide screening from the commercial condos to the east to the agricultural area. There are several

Cottonwoods & Siberian Elms in fair condition in this area that could be considered desirable, though the effects from grade change, storm water management requirements, and construction would likely cause them to not survive.

Rough Percentages of trees found in this area: Acer negundo (Boxelder):15%
Acer saccharinum (Silver Maple): 5%
Morus alba (Mulberry): 5%
Prunus serotina (Cherry):5%
Rhamnus cathardica (Buckthorn): 65%
Ulmus pumila (Siberian Elm):5%

Wetland / Drainage Swale

The area inventoried along north property line and the area that runs along the drainage swale through the center of the property is approximately 4.3 acres of woodland. The trees in this area consisted of Buckthorn, Siberian Elm, Cottonwood, Silver Maples, Boxelder and Mulberry. The majority of the trees in this area are invasive and non-desirable but are in fair to good condition and provide screening to the neighboring industrial to the west and the old rail lines to the north. There are a few large oaks in the NW corner and several Cottonwoods & Siberian Elms in fair condition in this area that could be considered desirable, though the effects from grade change, due storm water management requirements and construction would like cause them to not survive.

Rough Percentages of trees found in this area: Acer negundo (Boxelder): 20%
Acer saccharinum (Silver Maple): 5%
Morus alba (Mulberry): 5%
Populus deltoides (Cottonwood): 5%
Rhamnus cathardica (Buckthorn): 60%
Ulmus pumila (Siberian Elm):5%

Recommendations

The majority of the wooded areas on-site consist of invasive trees with intermittent desirable species. The proposed construction will be impacting 100% of the site and due to significant elevation change and storm water management requirements, all trees on site will be affected. In lieu of preservation of existing low quality and invasive trees, a significant landscape plan incorporating 92 high quality front yard parkway trees and 98 high quality backyard trees to be used as screening, and will be more beneficial to the long term health of trees on the site as well as neighboring properties.

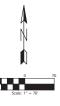
If it is possible to keep trees, the following protocols should be used:

- Install tree protection fence per erosion control plan prior to any construction;
- Fence the public portion (parkways) of the entire tree protection zones with a 6' chain-link fence to prevent wounds to the parkway trees as well as soil compaction prior to any construction activity. Post the fence with a sign stating "Tree Protection Zone Keep Out";
- At no time shall any equipment, materials, supplies or soil fill be allowed in the tree protection zone;
- The entire tree protection zone should be mulched to improve the growing conditions for tree roots, and minimize the maintenance of the parkway lawn; and
- Tree protection zone is the designated area the encompasses the entire tree canopy.

This tree preservation plan incorporates all reasonable steps necessary to minimize damage to trees on property and adjacent to the property.

The Tree Preservation Plan was p	performed by Certified Arborist Jay Peters
	7.11.2020

Jay C. Peters; Certified Arborist # IL-1201





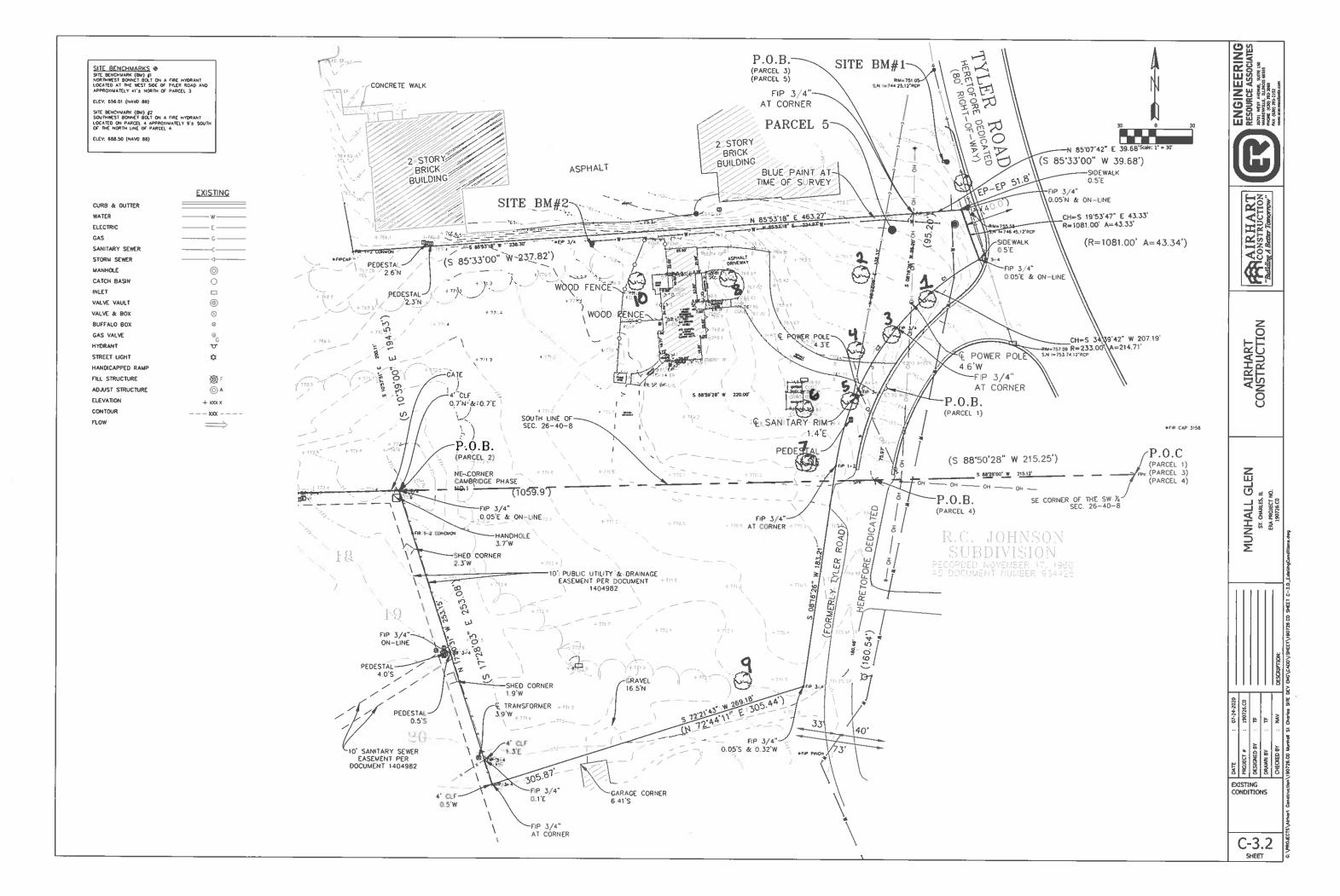
AIRHART CONSTRUCTION

MUNHALL GLEN ST. CHARLES, IL ERAPROJECT NO.

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RESOURCE ASSOCIATES
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TREE PRESERVATION ZONES

EX SHEET



MUNHALL GLEN

ST. CHARLES, IL 60174 JOB NO. 190726.C0 JULY 24TH, 2020 PRELIMINARY ENGINEERING

SECTION 26 T40N R8E



AREA SUMMARY 670.397 SF = 15.390 AC

ZONING CLASSIFIATION EXISTING = M-2 PROPOSED = RS-4

LOCATION MAP



2416 GALEN DRIVE CHAMPAIGN, ILLINOIS 61821 PHONE (217) 351-6268 FAX (217) 355-1902

WARRENVILLE, ILLINOIS 60555 PHONE (630) 393-3060 FAX (630) 393-2152

10 S. RIVERSIDE PLAZA , SUITE 875 CHICAGO, ILLINOIS 60606 PHONE (312) 474-7841 FAX (312) 474-6099

3S701 WEST AVENUE, SUITE 150



Nicholas A. Varchetto II P.F. NO. 062-068622



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UTILITY PLANS

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BMP LOCATIONS

TYPICAL SECTIONS

SANITARY DETAILS CONSTRUCTION DETAILS

RAIN GARDEN PLAN

WETLAND IMPACT PLAN

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C-13.0

C-14.0 C-15.0

C-11.0 : C-11.1

ST CHARLES PUBLIC WORKS: (630) 377-4486 ENGINEERING RESOURCE ASSOCIATES: (630) 393-3060

UTILITY INFORMATION IS BASED UPON FIELD MEASUREMENTS AND BEST AVAILABLE RECORDS. FIELD DATA IS LIMITED TO THAT WHICH IS VISIBLE AND CAN BE MEASURED. THIS DOES NOT PRECLIDE THE EXISTENCE OF OTHER UNDERGROUND UTILITIES.

THE CONTRACTOR SHALL NOTIFY J.U.L.I.E. (1-800-892-0123) 48 HOURS
PRIOR TO ANY EXCAVATION WORK TO DETERMINE THE EXACT LOCATION OF
EXISTING UTILITIES.

EXCEPT WHERE MODIFIED BY THE CONTRACT DOCUMENTS, ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE FOLLOWING.

DOCUMENTS:
"STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION IN THE
STATE OF ILLINOIS", ILLINOIS DEPARTMENT OF TRANSPORTATION, LATEST

"STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS", LATEST EDITION. "ILLINOIS LIBRAN MANUAL"

"DUPAGE COUNTY STORMWATER ORDINANCE" LATEST EDITION

5 THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION AND MAINTENANCE OF ADEQUATE SINCE AND WARRING DEVICES TO INFORM AND PROTECT FOR PRESECT. THE MANUAL ON UNKNOWN TRAFFEE CONTROL DEVICES FOR STREETS AND HIGHWAYS.", AS ADOPTED BY THE ILLINOSI DEVARIMENT OF TRANSPICKETATION, LATEST EDITION, SHALL BE CONSISTED. APPROPRIATE CONTROL BETHOUGH SPRICED BY APPLIED TO THE SPECIFIC STRINGTIONS AND TYPES OF CONTROL TORS OF APPLIED TO THE SPECIFIC STRINGTIONS BERN FREE OWNER.

THE CONTRACTOR SHALL ESTABLISH THE NECESSARY PERFORMANCE BONDS REQUIRED. PERMITS SHALL BE OBTAINED FROM ALL OUTSIDE GOVERNMENTAL AGENCIES HAVING JURISDICTION PRIOR TO INITIATION OF CONSTRUCTION ACTIVITIES.

THE CONTRACTOR IS RESPONSIBLE FOR HAVING THE MOST RECENT SET OF THE "APPROVED" FINAL ENGINEERING PLANS WITH THE LATEST REVISION DATE ON THE JOB SITE PRIOR TO THE START OF CONSTRUCTION.

THE CONTRACTOR IS TO VERIFY THE LOCATION OF ALL EXISTING UTILITIES
 PRIOR TO THE START OF CONSTRUCTION AND WILL BE RESPONSIBLE FOR
 ANY DAMAGE TO THE SAME.

10. CONTRACTOR SHALL RESTORE OFF-SITE SURFACES TO ORIGINAL CONDITION IF DAMAGED BY CONSTRUCTION.

THE CONTRACTOR IS TO PROVIDE THE CITY ENGINEER WITH RECORD DRAWINGS OF ALL UTILITIES SHOWING LOCATIONS OF ALL SEWER PIPE, MAINS, SERVICE STUBS, AND STRUCTURES.

12. THE ENGINEER WILL NOT BE RESPONSIBLE FOR THE CONTRACTORS MEANS, METHODS, TECHNIQUES, SECURISES, OR PROCEDURES OF CONSTRUCTION, OR THE SAFETY PRECAUTIONS AND PROGRAMS INCIDENT THERETO, AND THE ENGINEER WILL NOT BE RESPONSIBLE FOR THE CONTRACTOR'S FAULE TO PREFORM OR FURNISH THE WORK IN ACCORDANCE WITH THE CONTRACT DOCUMENTS.

INCLUDENTS.

THE FIGHER WARRANTS THE DESIGN, RECOMMENDATIONS, AND SECREPATIONS OF OWNER EER ROMALGATED OF CONTINUOUS AND SECREPATIONS OF OWNER EER ROMALGATED OF CONTINUOUS AND SECREPATIONS OF COMMENDATIONS AND SECREPATIONS OF CONTINUOUS ON DESIGNATION OF CONTINUOUS ON SECREPATION OF CONTINUOUS ON SECREPATION OF CONTINUOUS ON SECREPATION OF CONTINUOUS ON SECREPATION OF THE SECREPATION OF THE

5. AT LEAST 2 WORKING DAYS SEFORE COMMENCEMENT OF ANY WORK ACTIVITIES. THE CONTRIBUTION WILL BE SEQUISED TO ATTEMD AN OW.STIT MECONSTRUCTION CONTRIBUTED. AT THIS CONTRIBUTION OF CONTRIBUTION AND THE REACHED 24 HOUSES A DAY.

16. IT SHALL BE THE RESPONSIBILITY OF THE OWNER TO APPLY FOR ALL REQUIRED IEPA PERMITS AND COMPLY WITH ALL EPA RULES AND REGULATIONS.

(1) ALL RIM AND INVERTS
(2) GRADE INFLECTION POINTS WITH PERIODIC GRADES SHOT IN LEVEL AREAS
(3) DETENTION POIN GRADES WITH VOLUME CALCULATION.
(4) NOTE COMPARING ACTUAL TO REQUIRED POIND VOLUME

DUST CONTROL WILL BE IN ACCORDANCE WITH IDOT "STANDARD SPECIFICATIONS FOR ROAD AND BRINDGE CONSTRUCTION IN THE STATE OF ILLINOIS", ILLINOIS DEPARTMENT OF TRANSPORTATION, LATEST EDITION.

STORM SEWER:

I REMPORCID CONCRETE PIPE STORM SEWER SHALL MEET OR EXCEED THE RECORDINATION OF ASTINCTS CASES IN PIPE PIPE STORM SEWER SHALL MEET OR RECORDINATION OF ASTINCTS CASES IN PIPE PIPE STORM SEWER SHALL MEET FOR THE STORM SEWER SHALL MEET FALL MEET SHALL BE FERNOMENE WHITH ASTIN ASTINCTION OF ASTINCTI

SEWER PIPE JOINTS SHALL BE "O-RING" TYPE - ASTM C-44
RCP AND SHALL BE PUSH-ON TYPE- ASTM D-3212 FOR PVC
 WERTICAL SEPARATION - WATERMAINS AND SEWERS:

1) A WATERMAN SHALL BE OPPMATED FROM A SEWER SO THAT THE DRAW OF SEWER WHENDERS WITH SHAPE SHAPE SO THAT THE DRAW OF SEWER WHENDERS WITERMANS CROSS STOM ASWERS. SMATTARY SEWERS OF SEWER SEWER CONCENTIONS. THE VIETNAL SEPARATION SHALL BE MAINTAINED FOR THAT FORTION WEIGHT OF SEWER SEWER CONCENTIONS. THE VIETNAL SEPARATION SHALL BE MAINTAINED FOR THAT FORTION PROBLEMS OF SEWER SEWERS CONSECTION. SHAPE SHAPE

2.) BOTH THE WATERMAIN AND SEWER SHALL BE CONSTRUCTED OF SUP-ON OR MECHANICAL JOINT CAST OR DUCTILE ISON PIPE. ASSESTOS-CENENT PRESSURE PIPE, PRE-STRESSED CONCRETE PIPE, OR PIPC PIPE EQUIVALENT TO WATERMAIN STANDARDS OF

A.) IT IS IMPOSSIBLE TO OBTAIN THE PROPER VERTICAL SEPARATION AS DESCRIBED IN (1) ABOVE, OR:

B.) THE WATERMAIN PASSES UNDER A SEWER OR DRAIN.

4.) CONSTRUCTION SHALL EXTEND ON EACH SIDE OF THE CROSSING UNTIL THE PERPENDICULAR DISTANCE FROM THE WATERMAIN TO THE SEWER OR DRAIN LINE IS AT LEAST TEN FEET.

MANHOLES AND CATCH BASINS SHALL BE PRECAST REINFORCED CONCRETE - ASTM C-478 AND ASTM C-443 CONFORMING TO THE MINIMUM SIZE CRITERIA SPECIFIED IN THE PLANS.

INLETS SHALL BE TWENTY-FOUR (24) INCH DIAMETER PRECAST REINFORCED CONCRETE CONFORMING TO ASTM C-478.

10. RIM GRADES IN CURB AND GUTTER ARE EDGE OF PAVEMENT ELEVATIONS.

LILENTINO, FIELD THE AND/OR DRAW PIPES INCOMPRIED DURING CONSTRUCTION OF PRATORS SHALL BE CONNECTED TO DURING CONSTRUCTION OF PRATORS SHALL BE CONNECTED TO DURING CONSTRUCTION OF PRATORS SHALL BE CONNECTED TO CONSTRUCT AND SHALL BE REPORTED FOR THE CONSTRUCT OF SHALL BE SHAPED WITH A NEW POPE OF SHALL BE SHAPED CONSTRUCTION A RECORD AND PRITORS OF PRATORS SHAPED CONSTRUCTION A RECORD OF THE CONSTRUCTION OF THE DISCONLINE OF THE CONSTRUCTION AND THREST OVER TO THE SHAPED CONSIDERATION OF THE SHAPED CON

12. ALL FOOTING DRAINS AND DOWNSPOUTS SHALL DISCHARGE TO THE STORM SYSTEM.

13. ANY PIPES OR MANHOLES CONTAINING SEDIMENT SHALL BE CLEANED OUT PRIOR TO FINAL ACCEPTANCE.

14. ALL WINDOW WELLS, WHERE APPLICABLE, SHALL DISCHARGE TO THE STORM SYSTEM.

15. ALL MANHOLES IN PAVEMENT SHALL HAVE EXTERNAL CHIMNEY SEALS.

SANITARY SEWER:

ALL PLEASES (GANTY SANTARY SEWER PIPE SHALL BE INSTALLED IN ACCISIONACE
WITH HATTIR D-221-89. ALL FLEXIBLE GRAVITY SANTARY SEWER PIPE SHALL BE
PIPC SDS 2-PIPE MERTING. THE RECURRENISTICS ATTAIN DAVIN WITH HATTIR
DE ELASTOMERIC GASETTS COMPLYING. WITH HATTIR 4-77 AND PRESSURE BATTON IN
ACCISIONACE WITH ASTIN D-212. WATERWARD GUALITY PO-SHALL BE
PITHOSORICH BE ACCIONACE WITH ASTIN D-221 AND D-157 CASETTS TO
COMPLY WITH 477 AND PRESSURE BATTON IN ACCIONACE WITH ASTIN D-121.

2. EMBEDMENT MATERIALS FOR BEDDING, HAUNCHING AND INITIAL BACKFILL TO AT LEAST TWELVE INCHES OVER THE TOP OF THE PIPE WITH 0.4.7, PROCESSED MATERIAL PRODUCES FOR HIGHMAY CONSTRUCTION USED IN THE PROJECT CLASSIFIED ACCORDING TO PARTICLE SIZE. SHAPE AND GRADATION IN ACCORDANCE WITH ASTIN D-221-89, SECTION 9, TABLE 1.

ALL RIGID GRAWITY SEWER PIPE TO BE INSTALLED IN ACCORDANCE WITH ASTM C-12 AND BEDDING MATERIAL CA-7.

THE MINIMUM BUILDING SANITARY SEWER SERVICE SIZE SHALL BE BE SIX (6) INCHES IN DIAMETER. THE SERVICE LATERAL SHALL SLOPE TOWARD THE MAIN AT THE MINIMUM RATE OF ONE (1) PERCENT.

NO MORE THAN TWO PRECAST ADJUSTING RINGS WITH A MAXIMUM HEIGHT ADJUSTMENT OF SIX INCHES SHALL BE ALLOWED.

9. MANHOLES SHALL INCLUDE EXTERNAL CHIMNEY SEALS.

ALL SANITARY SEWER CONSTRUCTION REQUIRES SIX (6) INCHES OF CA-7 CRUSHED GRAVEL OR CRUSHED STONE BEDDING UNDER THE PIPE. BEDDING STONE SHALL EXTEND TO A POINT YNELYE INCHES ABOVE THE TOP OF PIPE.

12. BACKFILLING OF THE TRENCH SHALL BE ACCORPLISHED BY CAREFUL BEPLACEMENT OF THE EXCANATED MATTERAL AFTER THE PIPE, BEDDINGS AND THE CONFR MATERIAL HAVE BEEN INSTALLED, ANY PIPE INSTALLED UNDER OR WITHON FIVE OF THE TO'A PARKEMENT EDGE, SIDEWIAL, OR CURB AND GOTTER SHALL BE MCKFILLED TO THE TOP OF THE TRENCH WITH CAT'S MATERIAL.

"BAND-SEAL" OR SIMILAR FLEXIBLE-TYPE COUPLINGS SHALL BE USED IN THE CONNECTION OF SEWER PIPE DISSIMILAR MATERIALS. ALL CHANGES OF MATERIAL SHALL OCCUR INSIDE A MANHOLE.

14. HERES COMPLETED TO AN EXESTING SERVER BANK ITS MEANS OTHER THAN HERE CONTROLLED TO AN EXESTING SERVER BANK ITS MEANS OTHER THAN POLICIOUS SERVER BE USED A J CHICALAR BANK OF STRONG TO SERVER BANK ITS WAS OFFICED AS SERVER AND INSOFTE SERVER BANK ITS WAS OFFICED SERVER BANK OFFI SERVER BANK

15. MANHOLE FRAMES SHALL BE NEEMAN NO. R-1710 / WATERTIGHT LID OR EAST JORDAN IRON WORKS 1020AGS. ALL CLOSED LIDS SHALL HAVE A CONCEALED PICK HOLE. WATER AND SANITARY LIDS SHALL BE WERT TIGHT AND SELF-SEA ING. LIDS SHALL BE EMBOSSED WITH "SANITARY SEWER: AND CUTTY OF LIDGORY. LINESS OTHERWISE NOTICE."

16 ALL FLOOR DRAINS SHALL DISCHARGE TO THE SANITARY SEWER

17. ALL SANITARY SEWER PIPES SHALL BE TESTED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS OF WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS, AS A MINIMARY AND WITH CITY OF LOCKPORT SANITARY CODE REQUIREMENTS, INCLUDING VISUAL, TELEVISED, INFLITRATION, ENFAITEMENTO, ANT ETSTS, LEARAGE TESTS AND DEFLECTION TESTS.

18. THE SINKE SHALL MET THE GEORGIE LESS AND LETTERS THE TEST AS THE UNDER PRESSURE AND TELEVISION INSPECTION. PLOS SWIFE PIPE MUST MEET'S NO FRECTION TEST BROWNING THE STATE OF THE CITY AND THE CONDUCTED IN THE PRESENCE OF AN EMPLOYEE OF THE CITY AND THE EMPORETE REPRESENTATION.

PAVEMENT, SIDEWALK:

PAVEMENT THICKNESS SHALL COMPLY WITH DUPAGE COUNTY AND BURR RIDGE REQUIREMENTS.

HANDICAPPED RAMPS AND DEPRESSED CURBS SHALL BE PROVIDED AT LOCATIONS SHOWN ON PLANS.

3. EXPANSION JOINTS SHALL BE PLACED, AS A MINIMUM AT ALL CONSTRUCTION JOINTS IN THE CUBB. TWO NO.4 REINFORCING BARS SHALL BE PLACED CONTINUOUSLY BETWEEN EXPANSION JOINTS. EXPANSION JOINTS. SHALL BE DOWELED AND SPACED NO MORE THAN SIXTY (60) FEO OI CENTER.

APPIGE TO JALIES ANY NAVIGEST MATERIAL THE CONTRACTOR
TO SERVICE OF SOMESTIP REPORTED AND COMPANY
THE SUBGRADE. THE PARKHATH BASE COURSE SHALL BE
PROGFIGURED WITH A FLEXI LODGE DUARP TRUCK. THE
SOMESTIP SHALL BE NOTIFIED AT LEXET 24 HOUSE SETTOR.
OF THE STATE ANY UNIFICATE AND SHAPE SHAPE DEPORTED
TO THEST THAT WITH STATE AND SHAPE AND THE SERVICE OF SOME
SUBGRADE.

SOIL EROSION CONTROL PLAN:

THE PROJECT AREA SHALL BE GRADED SO A MINIMAL AMOUNT OF STORM WATER RUNOFF AND LIKEWISE SOIL SEDIMENT WILL DISCHARGE UNITESTRICTED FROM THE SITE.

INLET PROTECTORS SHALL BE USED IN ALL STORM GRATES DURING CONSTRUCTION AND SHALL REMAIN IN PLACE USING THE RESTORATION IS SUPPLICIATELY STRAIGHED. THE WEEF A MUNIFICATION CLOSE OF THE PROPERTY OF THE PROTECTOR SHALL REFER A MUNIFICATION CLOSE.
 THE CITY ENGINEER CAN DETERMINE IF ADDITIONAL PRACTICES ARE NEEDED FOR BETTER SOIL RESISTOR AND SECTIONAL CONTRACTION.

SILT FENCING SHALL REMAIN IN PLACE THROUGH THE CONSTRUCTION OF HOUSE/BUILDINGS TO SERVE AS EROSION CONTROL FOR AT THAT CONSTRUCTION

AT THAT CONSTRUCTION.

TO PREVENT SOIL FROM LEAVING THE SITE ON CONSTRUCTION VEHICLE WHEELS, WORK ENTRANCES SHALL BE CONSTRUCTED OF GRAVEL AND SHALL EXTEND AT LEAST 100 FEET INTO THE DOB SITE THE EXISTING PAVEMENT SURFACES SHALL BE INSPICETED DAILY FOR SOIL DEBRIS AND SHALL BY SOFTED DAILY.

DISPOSAL OF DEBRIS EXCAVATION AND PAVEMENT REMOVAL SHALL BE THE CONTRACTOR'S RESPONSIBILITY AND CONSIDERED AS AN INCIDENTAL EXPENSE.

8. ANY TOPSOIL THAT WILL BE STOCKPILED ON SITE SHALL BE MANAGED IN ACCORDANCE WITH THE CURRENT MOPES REGULATIONS. IF THE STOCKPILE WILL REMAIN ON SITE FOR MY EXTENDED PERIOD, IT SHALL BE TABILIZED WITH GRASS ARDIOG OTHER VEGETATION AND SILT FEMCING SHALL BE PLACED MORNION THE STOCKPILE.

COMPACTION OF THE EXCAVATED MATERIAL PLACED IN AREAS NOT REQUIRING STRUCTURAL FILL SHALL BE MODERATE.

5. EXCAVATION OF EARTH AND OTHER MATERIALS WHICH ARE SUITABLE FOR USE AS STRUCTURAL FILL: THE EXCAVATION SHALL BE TO WITHIN A TOLERANCE OF 0.3 -7. OF THE FLAM SIBERBADE ELEXATIONS. THE TOLERANCE WITHIN PAVENEST ARBAS SHALL BE SUCH THAT THE BATTH MATERIAL SHALL BALANCE AS PART OF THE FINE GROWING OFERSTORY.

MARCA.

B PAMMENT FOR THE REMOVAL OF UNCULTRABE MATERIAL
(DICLIONAN TOPSOL), EXCHANTION SHALL BE BASED ON THE
(DICLIONAN TOPSOL), DEVIATION SHALL BE BASED ON THE
CONTRACTION SHALL REPORTED AS PART OF HIS BIRD A BUT PRICE.
PER CASIC-VARD FOR THE REMOVAL OF INSISTRABE BANTERIALS.

MATERIAL, REPACLAMENT WITH SUITINESS MATERIALS, DEVIATION OF THE CONTRACTION THOM A ROBROW SOLICE. AND COMPACTION
TO THE REGISTRANCE ASSESSMENT OF THE CONTRACT OF THE ADMINISTRA

ALL DISTURBED AREAS SHALL BE RESTORED W/6" TOPSOIL AND SEED AND BLANKET UNLESS OTHERWISE INDICATED.

10. IT IS THE RESPONSIBILITY OF THE DEVELOPER TO MAINTAIN ALL THE SEDIMENTATION CONTROL MEASURES. INSPECTIONS SHALL BE CONDUCTED AFTER A BAN EVENT, AND IF HAINTENANCE OF THE STRUCTURES IS NECESSARY, INCLUDING REPAIR OF DAMAGI AND REMOVAL OF DEPOSITS OR SEDIMENT FROM VEGETATIVE FILTERS, IT SHALL BE DOING BY THE DEVLICION.

ALL ACCESS TO AND FROM THE CONSTRUCTION SITE IS TO BE RESTRICTED TO THE CONSTRUCTION ENTRANCE.

ALL TEMPORARY AND PERMANENT EROSION AND SEDIMENT CONTROL PRACTICES MUST BE MAINTAINED AND REPAIRED AS NEEDED TO ASSURE EFFECTIVE PERFORMANCE OF THEIR INTENDED FUNCTION.

11. THE ENGINEER SHALL BE NOTIFIED OF MAJOR AMENDMENTS OF THE SITE DEVELOPMENT OR EROSION AND SEDIMENTATION CONTROL PLANS, WHICH WILL BE APPROVED IN THE SAME MANNER AS THE ORIGINAL PLANS.

EXCAVATION OF TOPSOIL AND OTHER STRUCTURALLY UNSUITABLE
MATERIALS MAY REQUIRE EARTH EXCAVATION AND COMPACTED
EARTH FILL MATERIAL IN ORDER TO ACHIEVE THE PLAN SUBGRADE

PLACEMENT OF THE EXCAVATED MATERIAL SHALL BE IN AREAS DESIGNATED BY THE OWNER FOR FUTURE USE, WITHIN AREAS TO BE LANDSCAPED, AND THOSE ARES NOT REQUIRING STRUCTURAL FILL MATERIAL.

EXCESS MATERIALS, IF NOT UTILIZED AS FILL OR STOCKPILED FOR FUTURE LANDSCAPING, SHALL BE COMPLETELY REMOVED FROM THE CONSTRUCTION SITE AND DISPOSED OF BY THE CONTRACTOR.

THE CONTRACTOR SHALL MAINTAIN PROPER SITE DRAININGS AT ALL TIMES DURRING THE COURSE OF CONSTRUCTION AND PREVENT STORM WATER FROM RUNNING INTO OR STANDING IN EXCAVATED AREAS.

INSTALL TEMPRARY EROSION CONTROL MEASURES.
 MASS GRADE SITE AND EXCAVATE DETENTION FACILITIES.
 CONSTRUCT UTILITIES AND PERFORM TREE REMOVALS.
 CONSTRUCT WILLITIES AND BUILDING FOUNDATIONS.
 PERFORM RESTORATION, STABILIZATION, AND REMOVAL OF TEMPORARY FROSION CONTROL MEASURES.



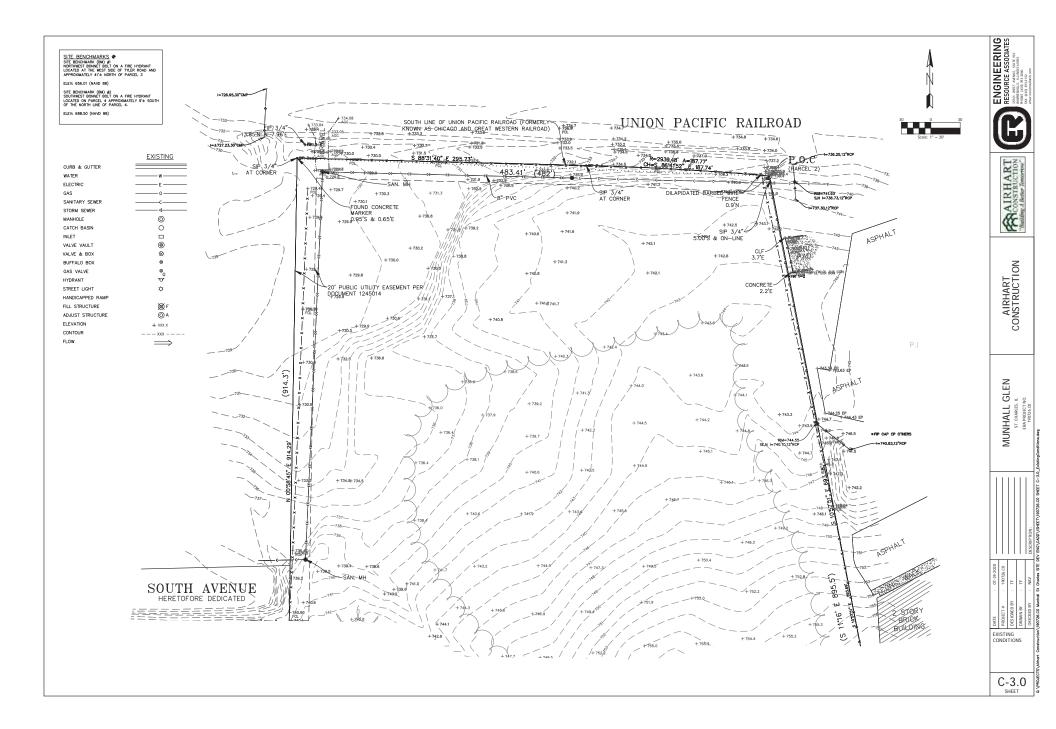


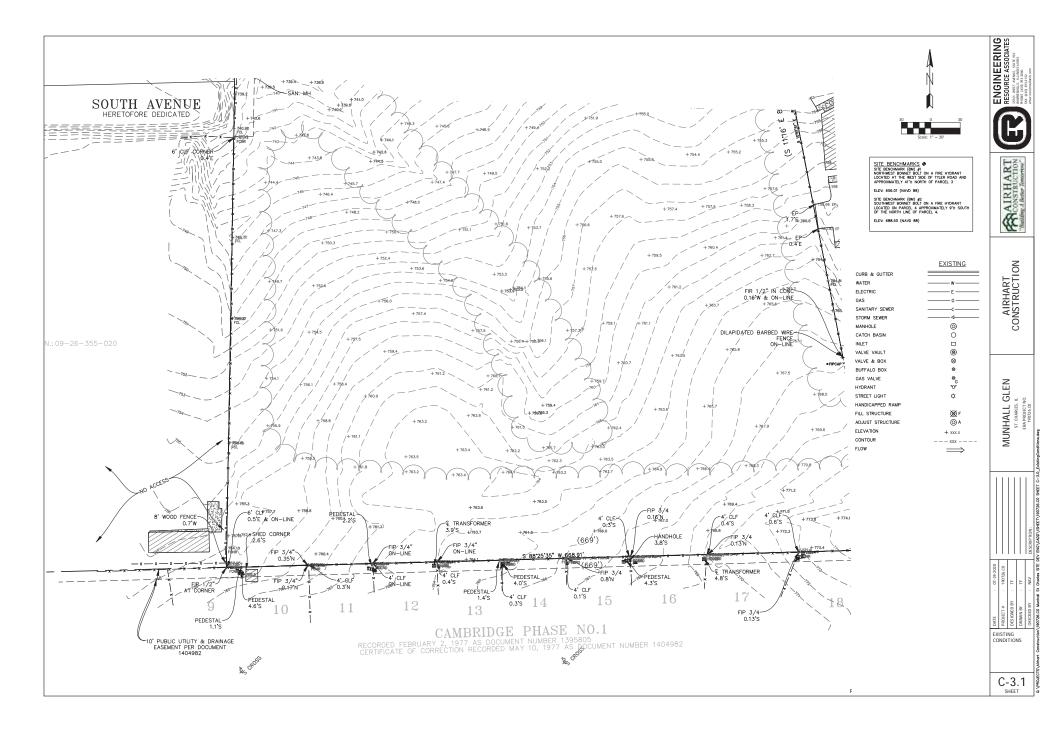
AIRHART CONSTRUCTION

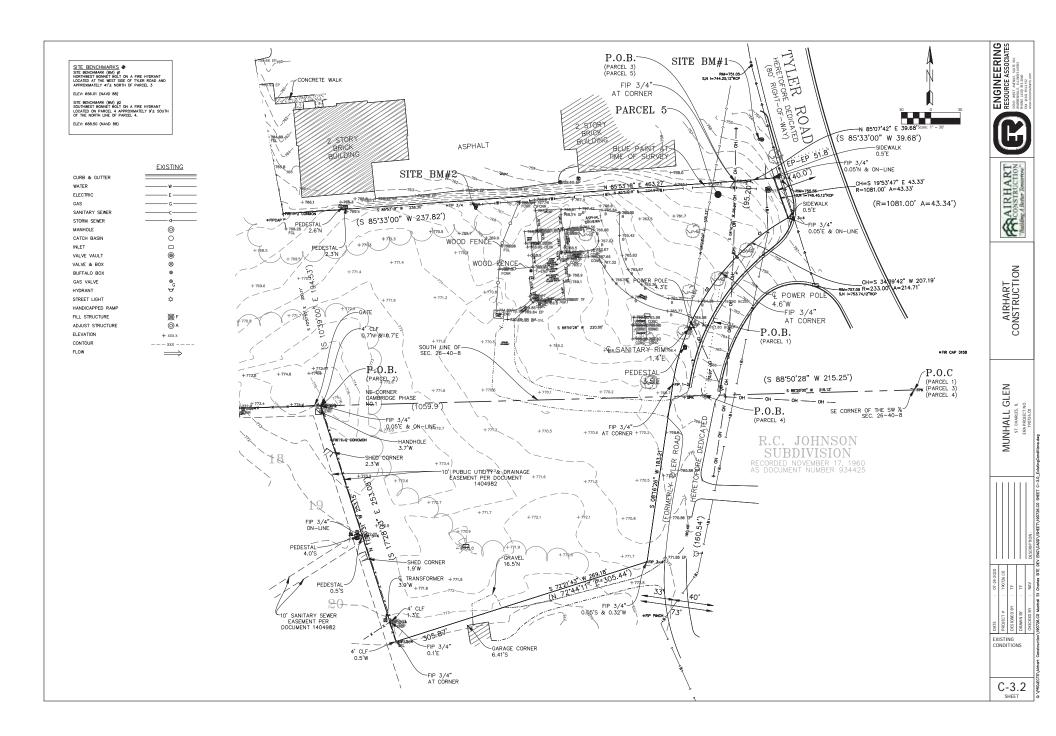
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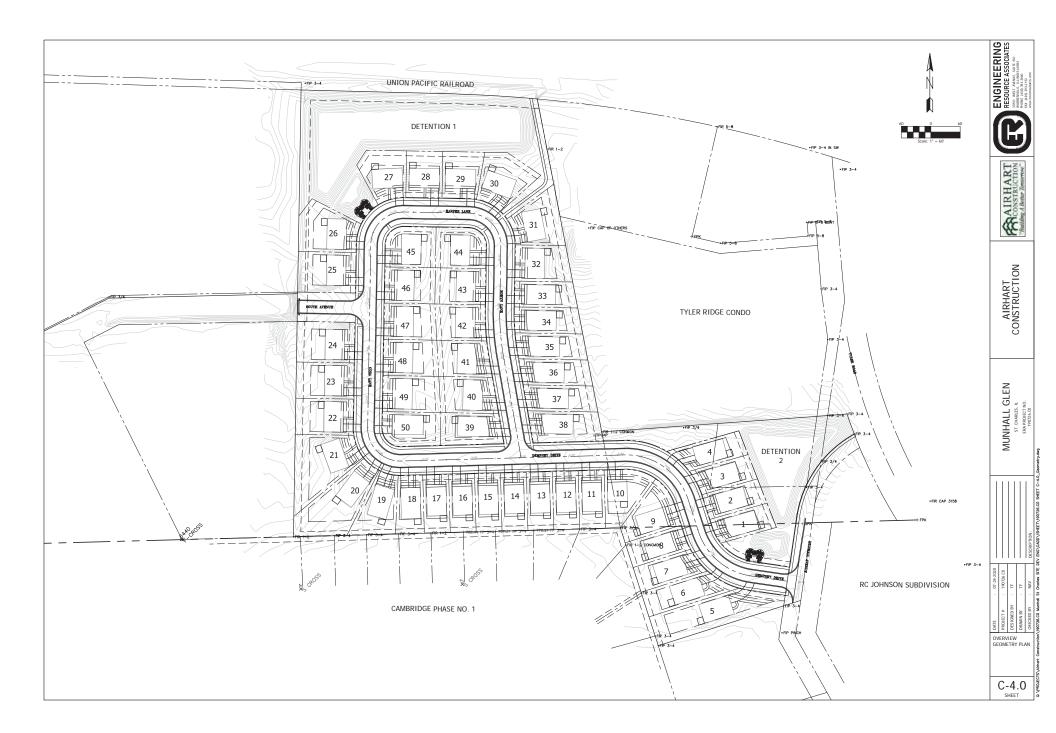
GENERAL NOTES

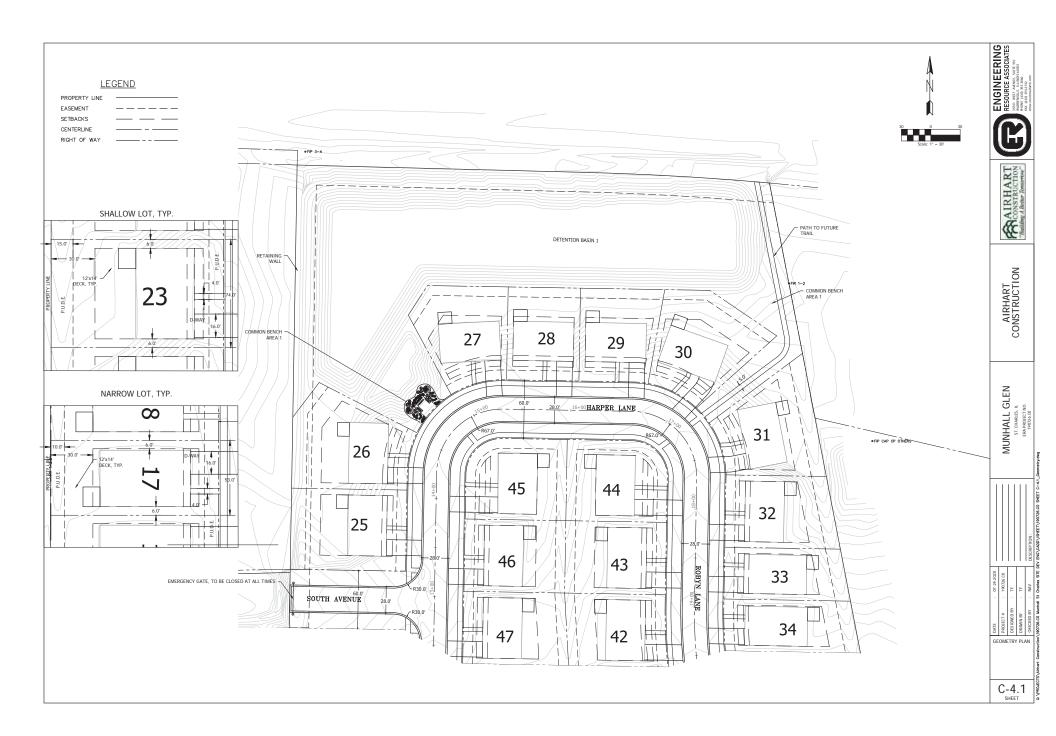
C-2.0 SHEET



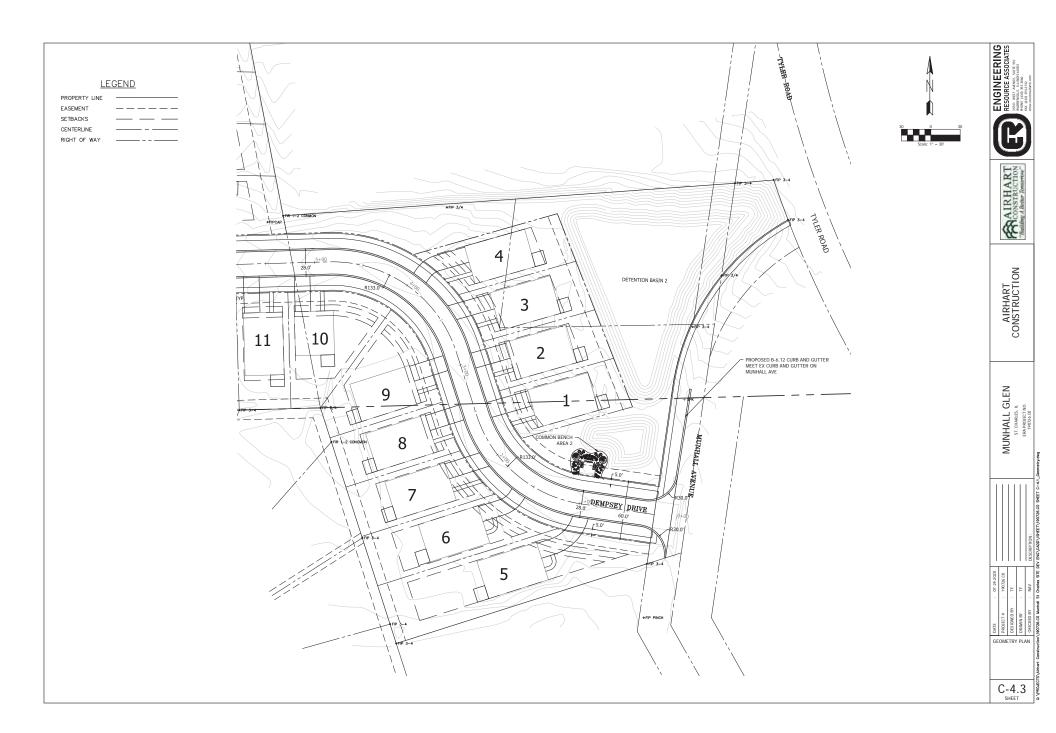


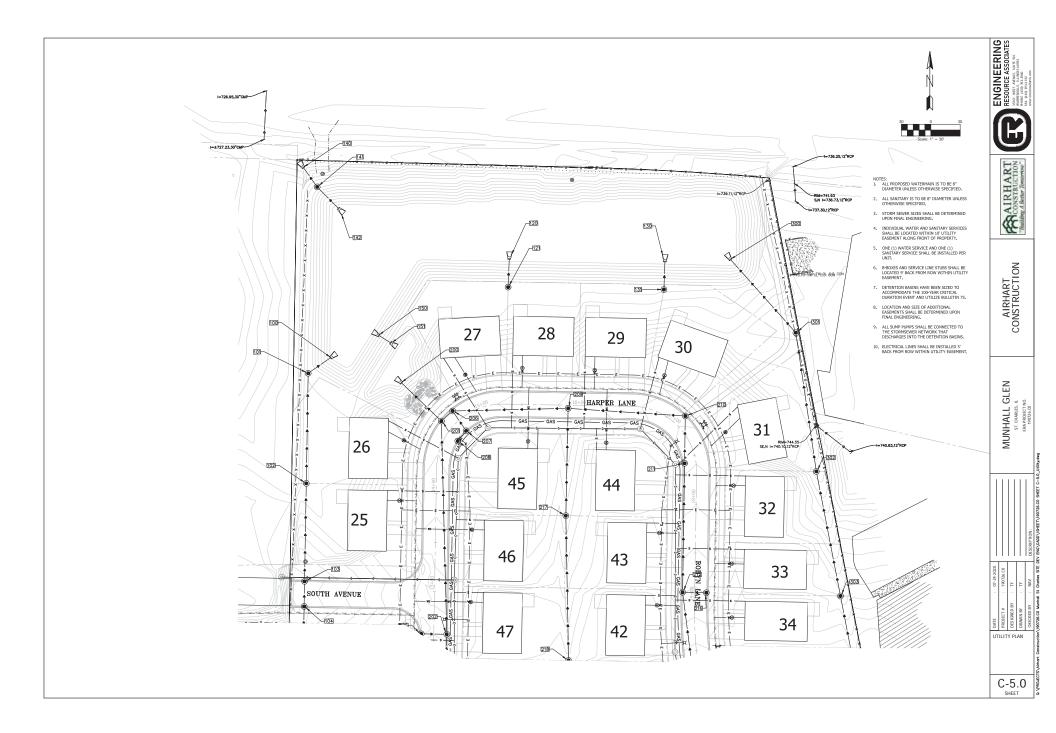


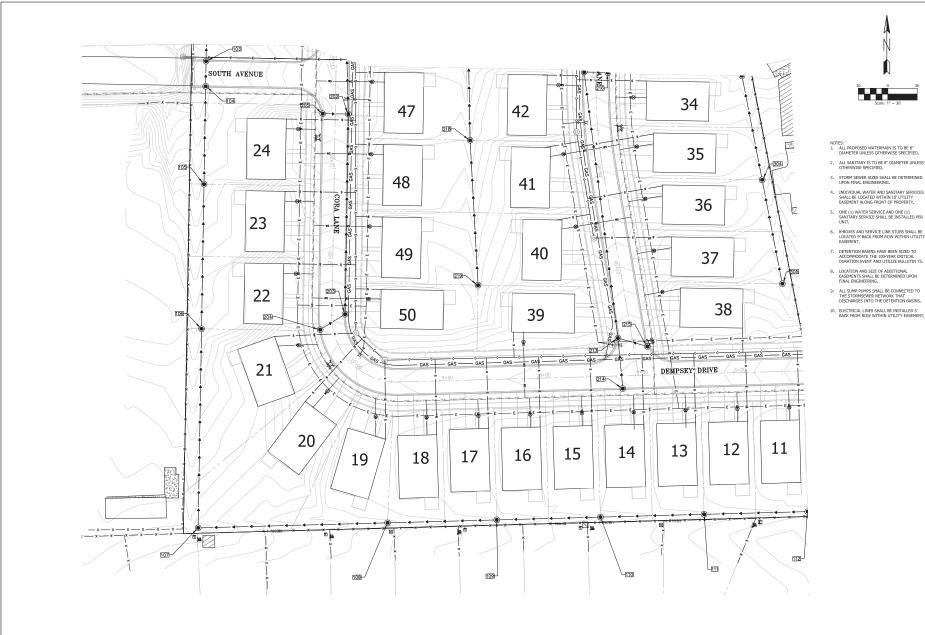












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- ALL SANITARY IS TO BE 8" DIAMETER UNLESS OTHERWISE SPECIFIED.

MUNHALL GLEN
ST. CHARLES, IL
ERA PROJECT NO.
190726.00

AIRHART CONSTRUCTION

UTILITY PLAN

C-5.2 SHEET



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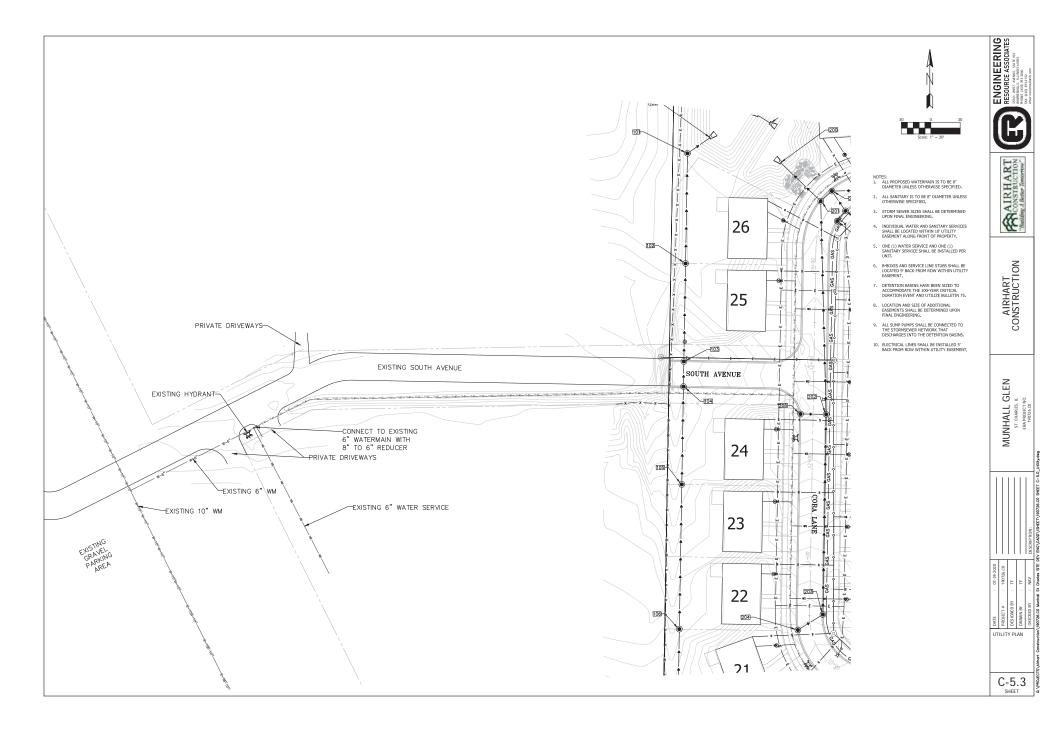


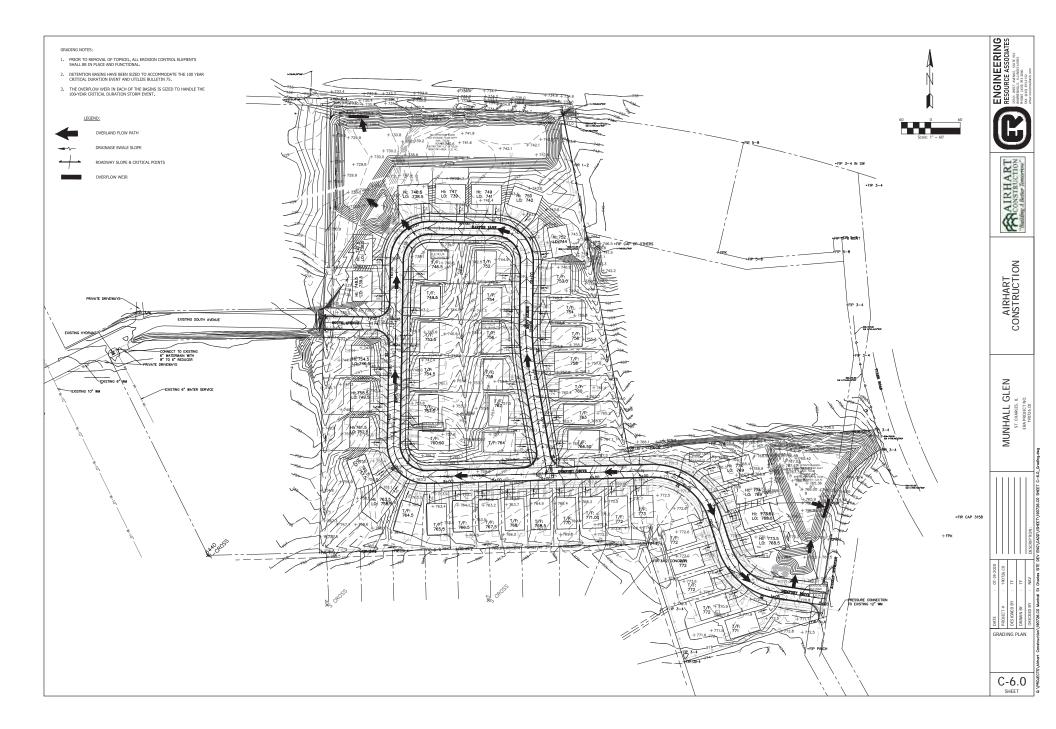
AIRHART CONSTRUCTION

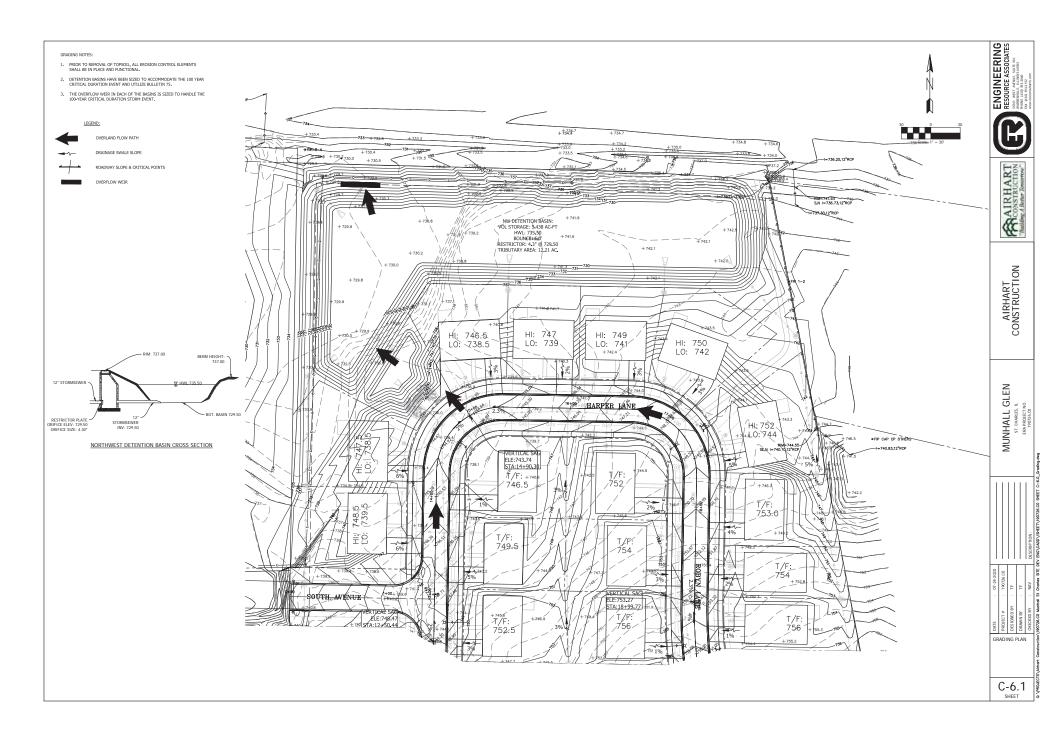
MUNHALL GLEN
ST. CHARLES, IL
ERA PROJECTINO.
190726.00

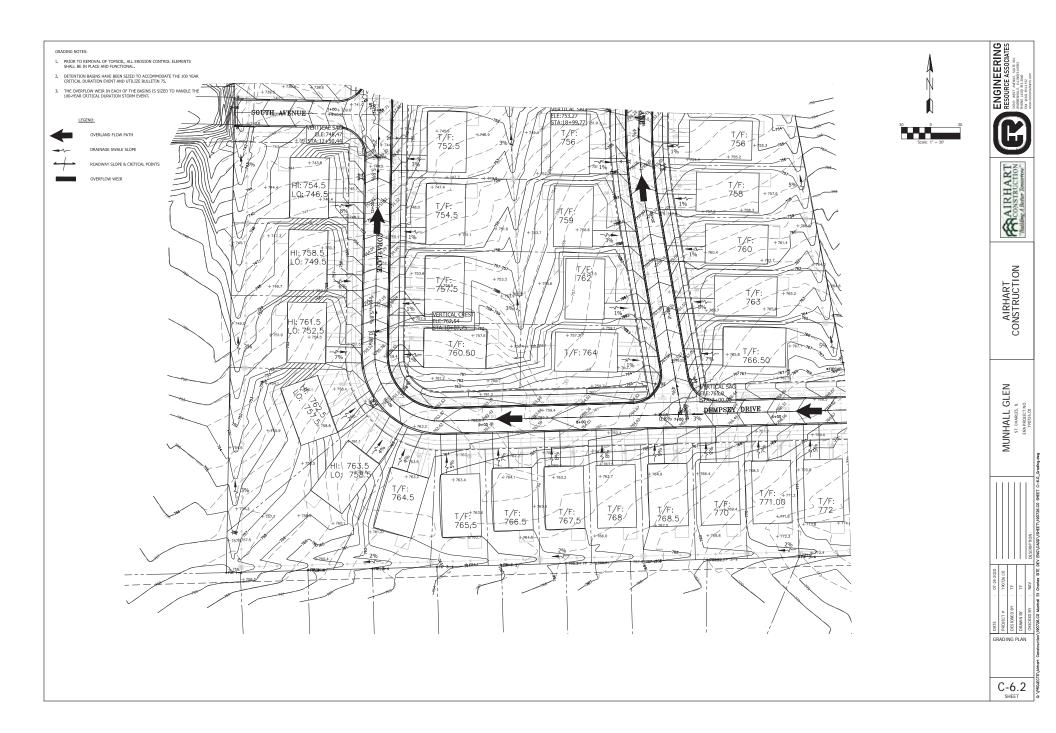
UTILITY PLAN

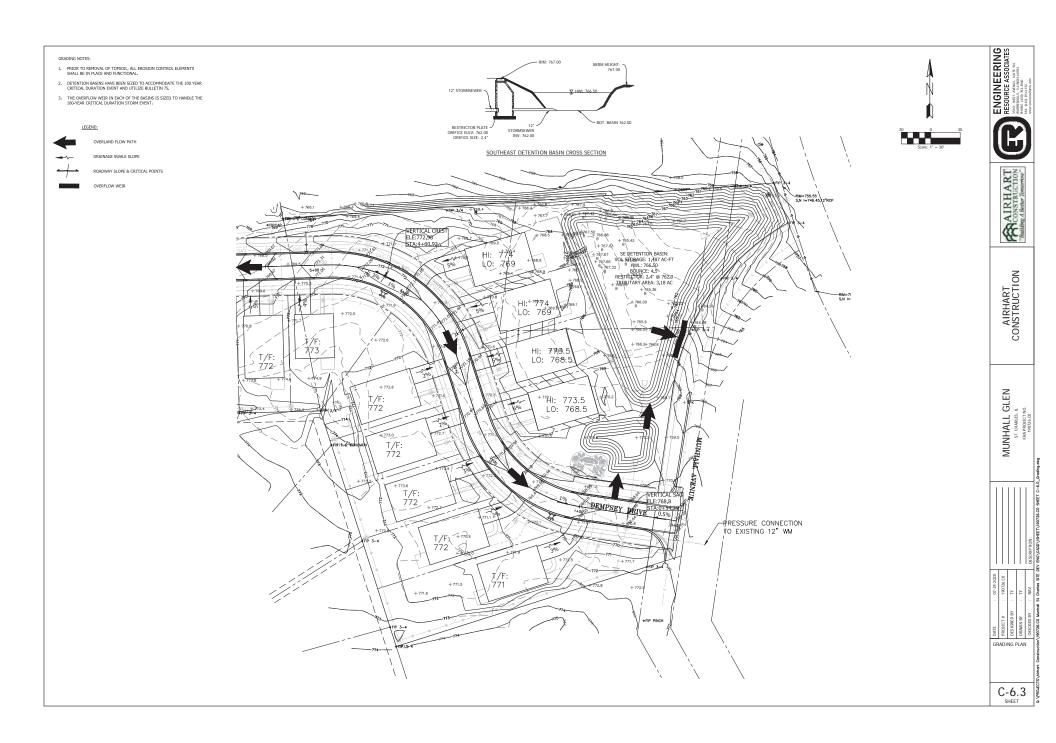
C-5.2













1 HMA PAVEMENT MIX C, N50

2 SUB-GRADE (SEE MIX TABLE)

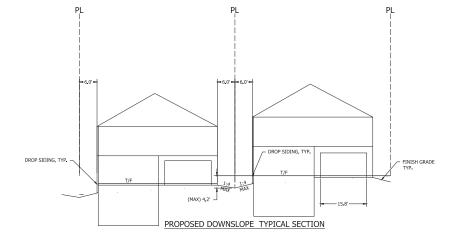
3 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12

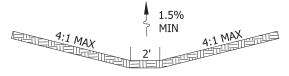
4 SEEDING, CLASS 2A, TOPSOIL 6"

5 PORTLAND CEMENT CONCRETE SIDEWALK (5') SUBBASE GRANULAR MATERIAL, TYPE B 4"

MIX TABLE:

1.5" HMA SURFACE COURSE - MIX C, N50 4.5" HMA BINDER COURSE - IL 19, N50 8" AGGREGATE BASE COURSE (CA-6)





BACKYARD SWALES

ENGINEERING
RESOURCE ASSOCIATES
STOR WASSTARLE, SUITE SO
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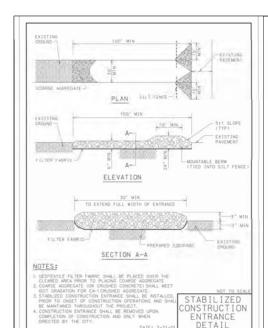


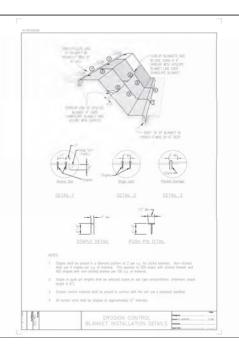
AIRHART CONSTRUCTION

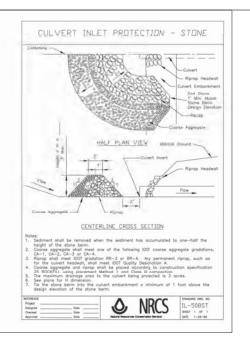
MUNHALL GLEN ST. CHARLES, IL ERAPROJECTINO.

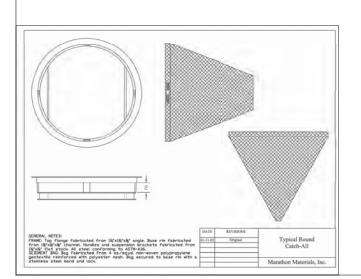
TYPICAL SECTIONS

C-7.0

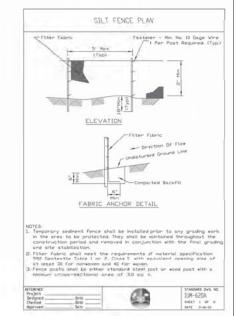


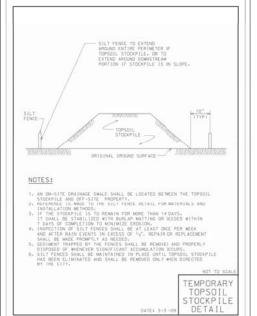






ENTRANCE DETAIL





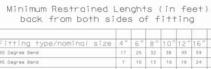
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FROSION CONTROL DETAILS

> C-8.0 SHEET

WATER DETAILS

C-9.0 SHEET

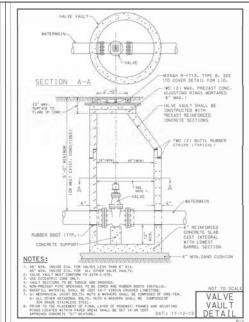


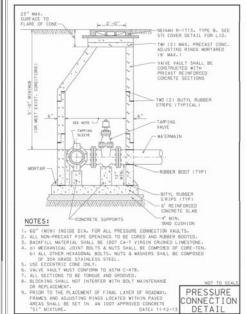
itting type/nominal size 4" 6" 8" 10" 12" 16" 16" 10 begree Bend 17 25 32 38 45 59 15 Degree Bend 7 10 13 16 19 24 2.5 Degree Bend 2 2 3 4 4 6 39 55 73 87 103 134 .25 Degree Bend Top Side Vertical Offset * (45 Degree) 16 23 30 36 43 55 oftom Side Vertical Offset * (45 Degree) 4 6 8 10 11 15 Tee Run X Bronch ** 6" by Tee Run X Bronch ** 8" by 1 1 24 49 1 1 15 41 65 ee Run X Bronch ** Tee Run X Branch ** 1 1 1 26 52 95 Tee Run X Bronch ** 16" by educer *** 6" by leducer *** B" by 52 30 71 54 29 90 75 55 51 123 113 97 94 54 educer *** 10" by educer *** 16" by educer www

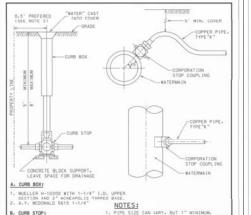
* Vertical affact with minimum 10°cf solid ploe between upper and lower bend-** Tee with MiniMum 10° solid ploe on both sides of run. harber indicates length of branch to be restrained.

*** Length back from Large Eng of Reducer.

WATER MAIN RESTRAINT







MOTES:

MELLER B-25155-N (1'-1'-2'-2')

FORD B-4-4-464-6-10-1'

B-4-4-77-70-10-2'

A.T. MCDONALD 76104-0 1'-1'-2'-2'

A.T. MCDONALD 76104-0 1'-1'-2'-2'

MELLER B-25008-N (1'-1-1'-2'-2')

MELLER B-25008-N (1'-1-1'-2'-2')

STRIES BBASES JOHN:

MELLER B-25008-N (1'-1-1'-2'-2')

STRIES BBASES JOHN:

MELLER B-25008-N (1'-1-1'-2'-2')

STRIES BBASES JOHN:

MELLER B-25008-N (1'-1-1'-2'-2')

STAINLESS STEEL TAP THROUGH SLEEVE.

NOT 10 SCALL

MURLLER B-25008-N (1" 1-1/2", 2")
FORD FB1000 -4-0-N, 1"
FB 1000-6-0-N, 1"
FB 1000-6-0-N, 2"
A.Y. McDONALD T4701-80 1", 1-1/2", 2"
O SERIES BRASS, WITH 90 SWIVEL ELBOW

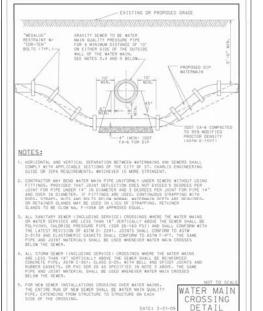
COPPER WATER SERVICE CONNECTION DETAILS DATE: 11-12-1

NOTE: . ALL BENDS DI 10'AND LAMDER SHALL BE BLOCKED WIN AT LEAST NO' THICK PRESUMED CONCOLTE BLOCKIST ADJINST UNDISTURBED VERTICAL EARTH FACE. 2. ALL CONCRETE TO BE MIN. 3.000 PSI. IN ADDITION TO THE ABOVE THRUST BLOCKING: ALL MECHANICAL JOINTS, BENDS OVER 10 AND FIRE HYDRANTS SHALL HAVE A "MECALUG" RESTRAINT, OR AS APPROVED BY THE ENGINEERING DIVISION-BOLTS SHALL BE "CONTERN". DATE: 1-2-2 THRUST BLOCK INSTALLATION

DATE: 3-2-

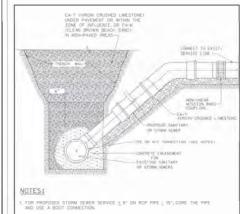
DETAIL

DETAILS



CONNECTION

DETAIL



2. FOR PROPOSED STORM SEWER SERVICE ≥ 8°, A MANHOLE SHALL BE INSTALLED.

FOR CONNECTING SANITARY OR STORM SEWER SERVICE TO AN EXISTING SANITARY OR STORM SEWER

12", REMOVE A SECTION OF PIPE AND INSTALL A TEE OR WYE CONNECTION.

4. FOR CONNECTING SANTARY SEWER SERVICE TO AN EXISTING SANTARY SEWER 2 15", CROULAR SAW-OUT AND USE A HUB WYE OR TEE SADDLE CONNECTION.

). FOR PROPOSED STORM OR SANITARY SEWER SERVICES, ENCASE ALL CONNECTIONS IN LOW STRENGTH CONCRETE TO PREVENT THE FITTINGS FROM ROTATING.

6, FOR TRENCHES WITHIN AN EXISTING PAVED SURFACE AREA, CA-7 (VARGN CRUSHED LIMESTONE) BACKFEL SHALL BE USED

7. FOR PROPOSED 6" STORM SEWER SUMP LINE, INSTALL PLUG AND 2" X 4" POST AT END OF LINE.

SANITARY

AND STORM

SERVICE

DETAIL

NETRIAN R-1713 IOR R-1914C
IN FLOOD PLANS ON OVERLAND
FLOOD MOUTHS & DIRECTIO
DOWNERS AND STREET
COVER DETAIL FOR LID.
VOI (23 MAX. PRECAST CONC.
ADJUSTING RINGS MONTARED
BY MAK.)
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FROUDIE EXTERNAL CHIMEY SEA
ANNOLES AND INTERNAL
ANNOLES, WERE MCCSSAMY.
EXTERNAL CHIMEY SEASON
TEXTRIBAL CHIMEY SEASON
ANNOLES, WERE MCCSSAMY.
EXTERNAL CHIMEY SEASON
TEXTRIBAL - 20 -PROVIDE 3" VERTICAL LIP ON COME SECTION # 60" DIAMETER 3.0" MIN. FLEXIBLE MANHOLE PIPE BOOT WITH 2 STAINLESS BANDS IEACH PIPE PENETRATION I NOTES: NOTES: MANDELS MAST CONFORM TO ASTIM C-478.

MANDELS MAST CONFORM TO ASTIM C-478.

MANDELS SECTIONS TO BE TOMORE AND ORDOWED.

MANDELS SECTIONS TO BE TOMORE AND ORDOWED.

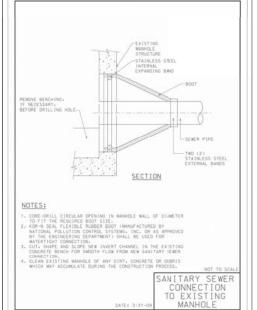
MANDELS SECTIONS TO BE TOMORE AND ORDOWED.

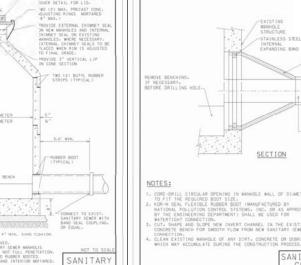
MANDELS SECTIONS TO BE RUBBER BOOSTED AND INTERIOR MOTABLE.

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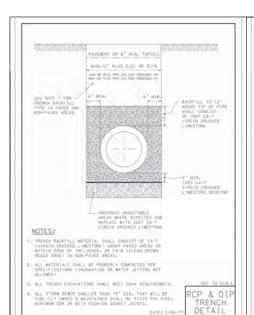


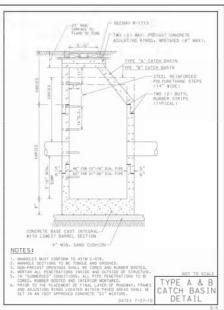
AIRHART CONSTRUCTION

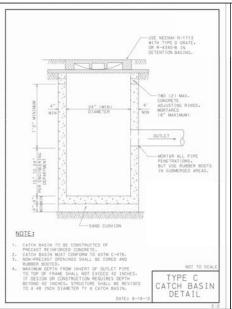
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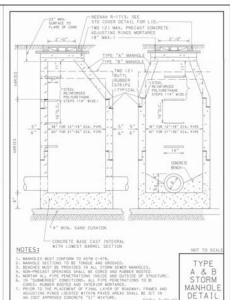
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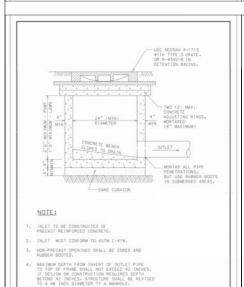
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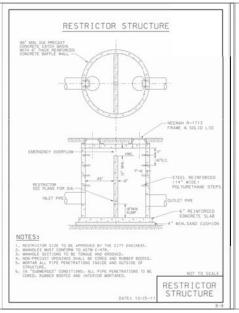




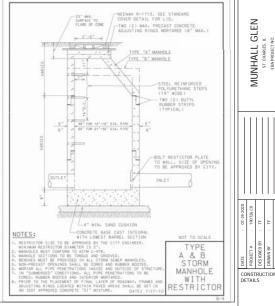




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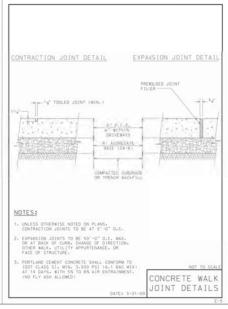
CONSTRUCTION DETAILS

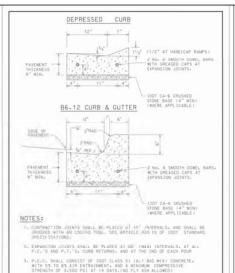
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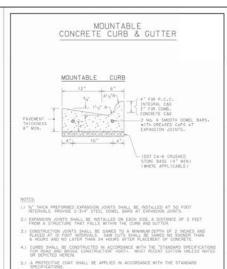
AIRHART CONSTRUCTION "Building A Better Tomorrow"

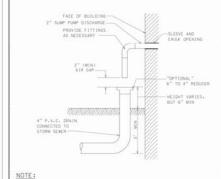
AIRHART CONSTRUCTION

C-11.0 SHEET









IN NO EVENT SHALL THE SUMP PUMP DISCHARGE INTO THE SANTARY SEWER SYSTEM.

A PLUMBING PERMIT IS REQUIRED PRIOR TO ANY CONNECTION.NOLIDE A PLAT OF SURVEY WITH THE PERMIT APPLICATION, SHOWING THE POINT OF DISCHARGE AND CONNECTION TO THE STORM SEWER. THE SITE PLAM SHALL ASSO DILLIDE PRE-SPECEFACTIONS AND INVEST ELEVATIONS, CALL THE CITY ENGINEERING DIVISION 24-HOURS IN ADJANCE TO SCHEDULZ AN INSPECTION.

SUMP PUMP DISCHARGE CONNECTION DETAIL

FOR RESIDENTIAL STREETS ONLY DATE: 10-22-B6.12 COMB. CONCRETE CURB AND GUTTER (SPECIAL) B6.12 P.C.C. INTEGRAL CURB AND GUTTER (SPECIAL)

A. FLEXIBLE PAVEMENT B'/4" IMINI HOT MIX ASPHALT OR MATCH EXISTING THICKNESS. WHICHEVER IS GREATER 4" IMIN'S SUBBASE GRANGLAR MATERIAL. - ETHERACTED SUBGRADE B. RIGID PAVEMENT - HE (MIN) PIRTLAND CEMENT CONCRETE



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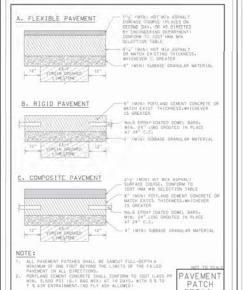
- NOT MIX ASPHALT SUFFACE & RINGER COUNTY SHALL DOFORM TO JOST HMA MIX . HOT WIN ASPHALT RUPINGER ANNIHOUSE COLUMN TO BOOT CASS PV. MIN. 3500 PSI
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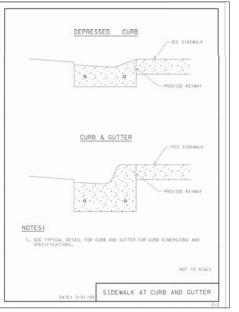
MINIMUM PAVEMENT

4° (W)N) SUBBASE GRANULAR MATERIAL. MOT DOMPACTED JUBGRADE

DETAILS



PATCH



MOUNTABLE

CURB & GUTTER



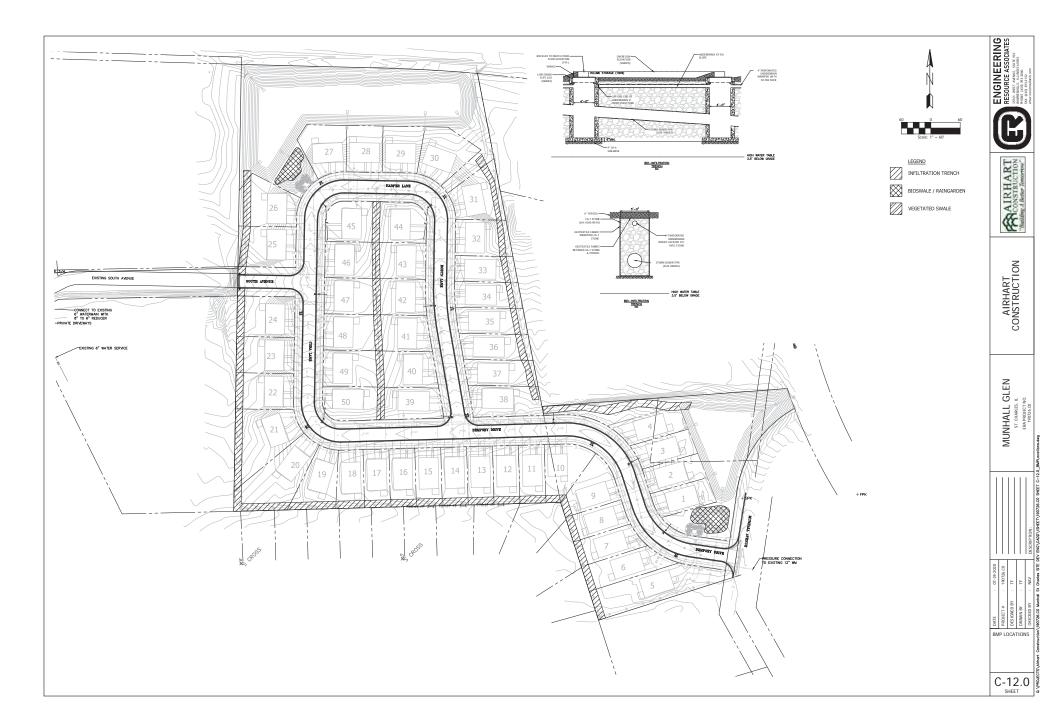
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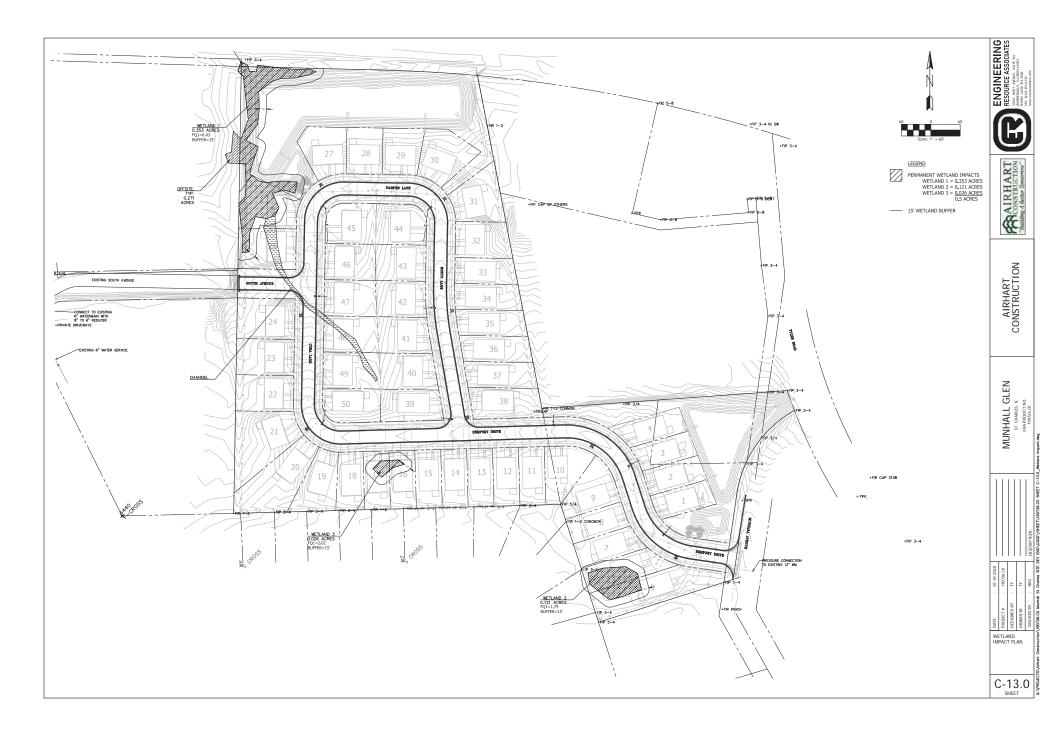
AIRHART CONSTRUCTION "Building A Better Tomorrow"

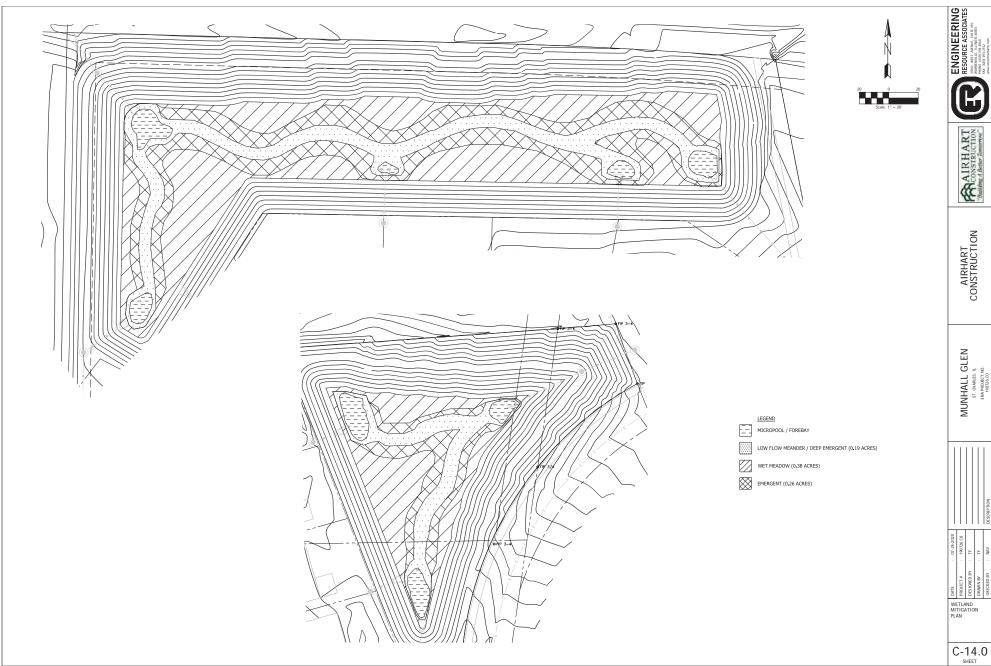
AIRHART CONSTRUCTION

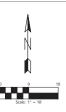
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CONSTRUCTION







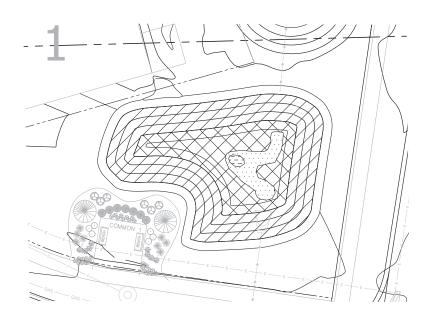


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FOR (60) 293-2152
FOR (60) 293-2152

AIRHART CONSTRUCTION

MUNHALL GLEN
ST. GHRES, IL
ERA PROJECT NO.
190726.00





LEGEND

OPEN WATER

EMERGENT PLANT COMMUNITY (0.012 ACRES)

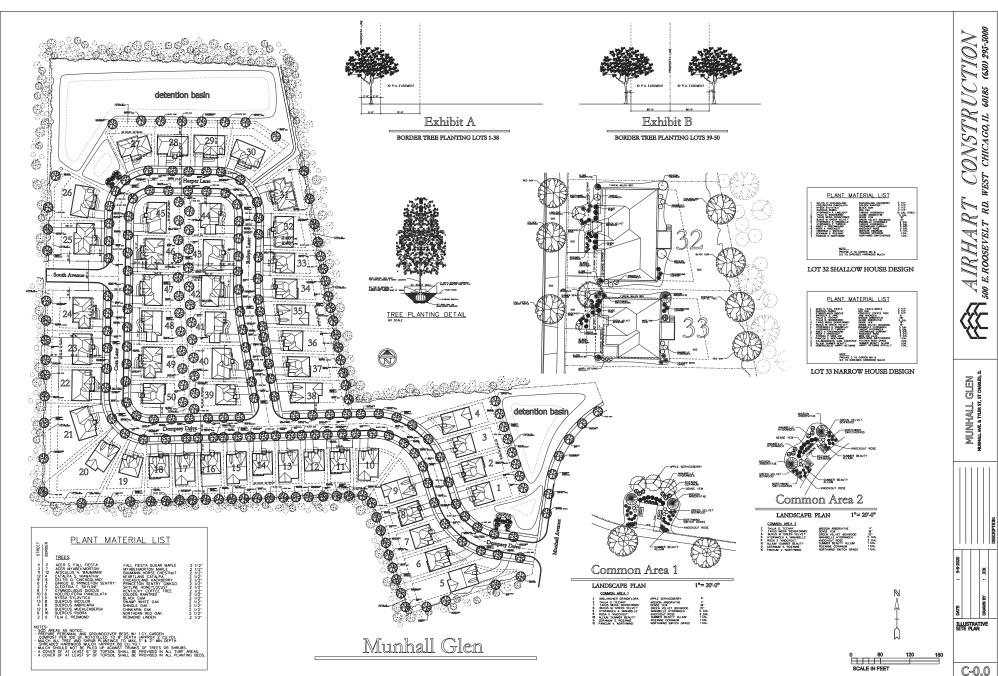
MESIC PRAIRIE BUFFER (0.052 ACRES)

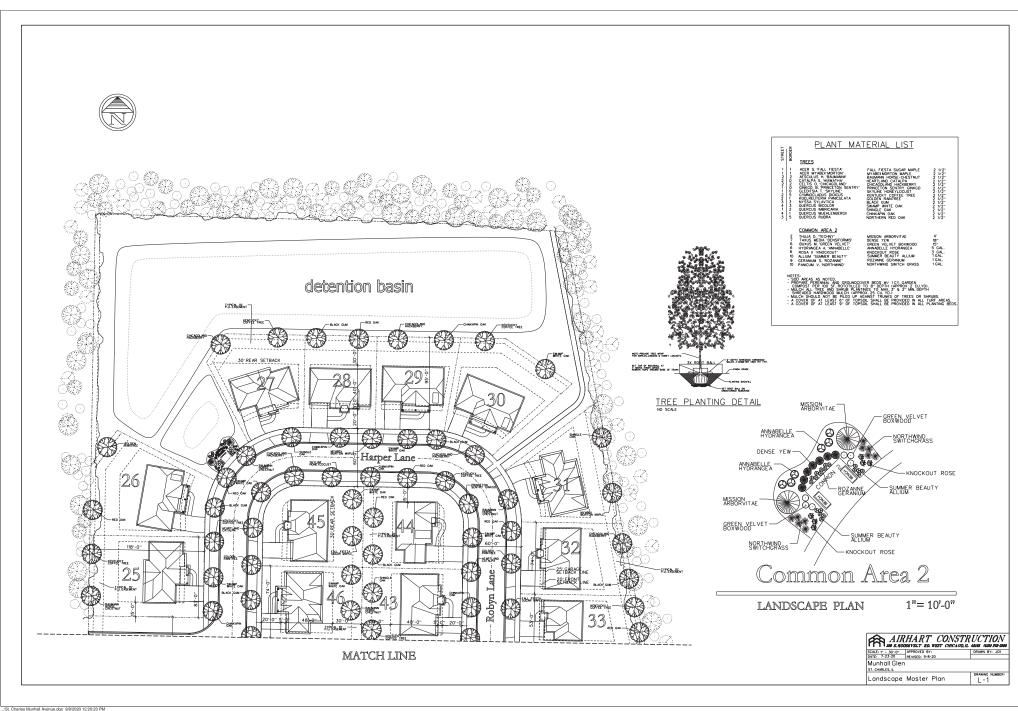
WET PRAIRIE / SEDGE MEADOW (0.045 ACRES)

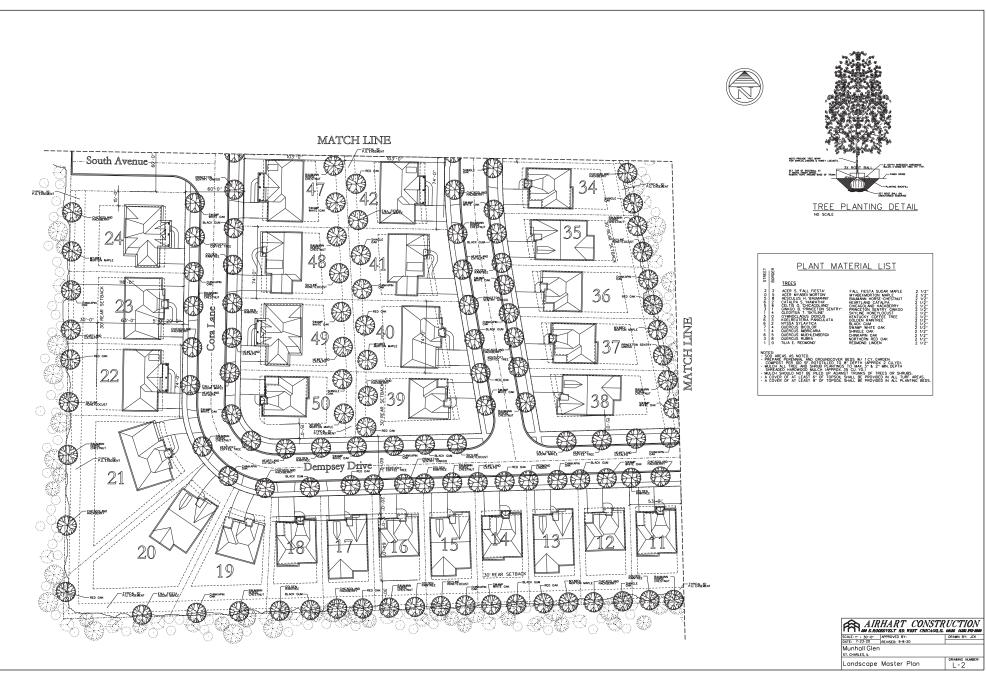
DATE : 07-24-2020
PROJECT # : 190726-C0
DESIGNED BY : TF
DRAWN BY : TF
OHECKED BY : NAV

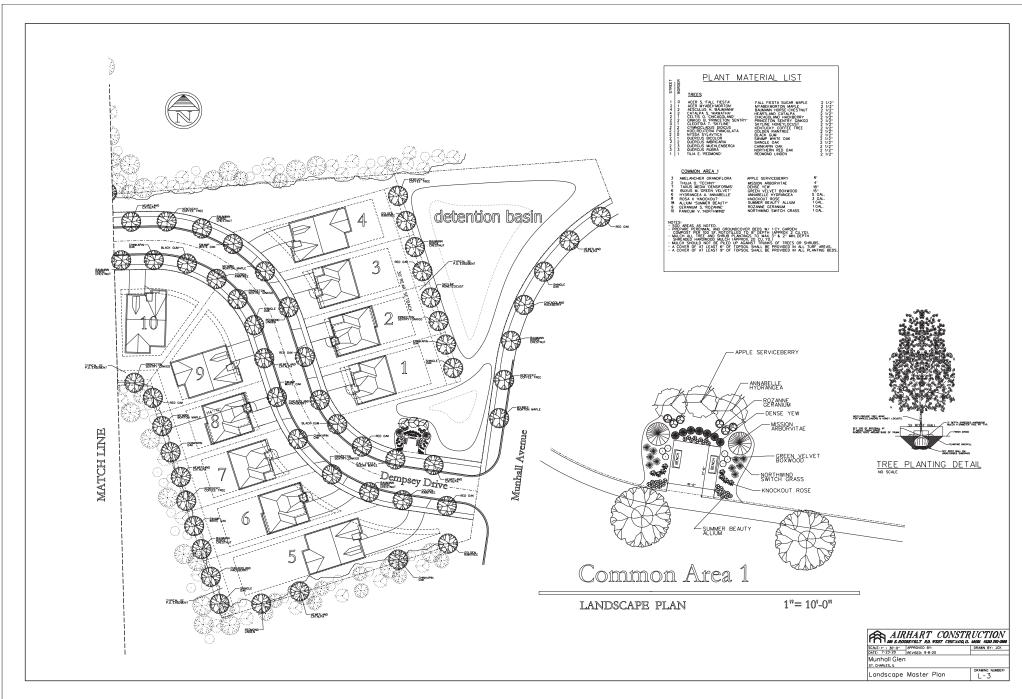
C-15.0

RAIN GARDEN PLAN

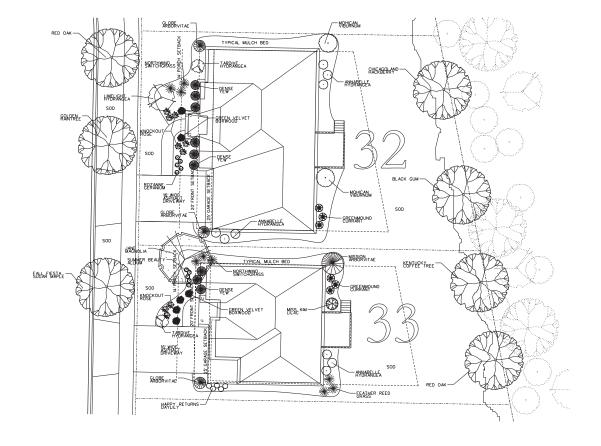








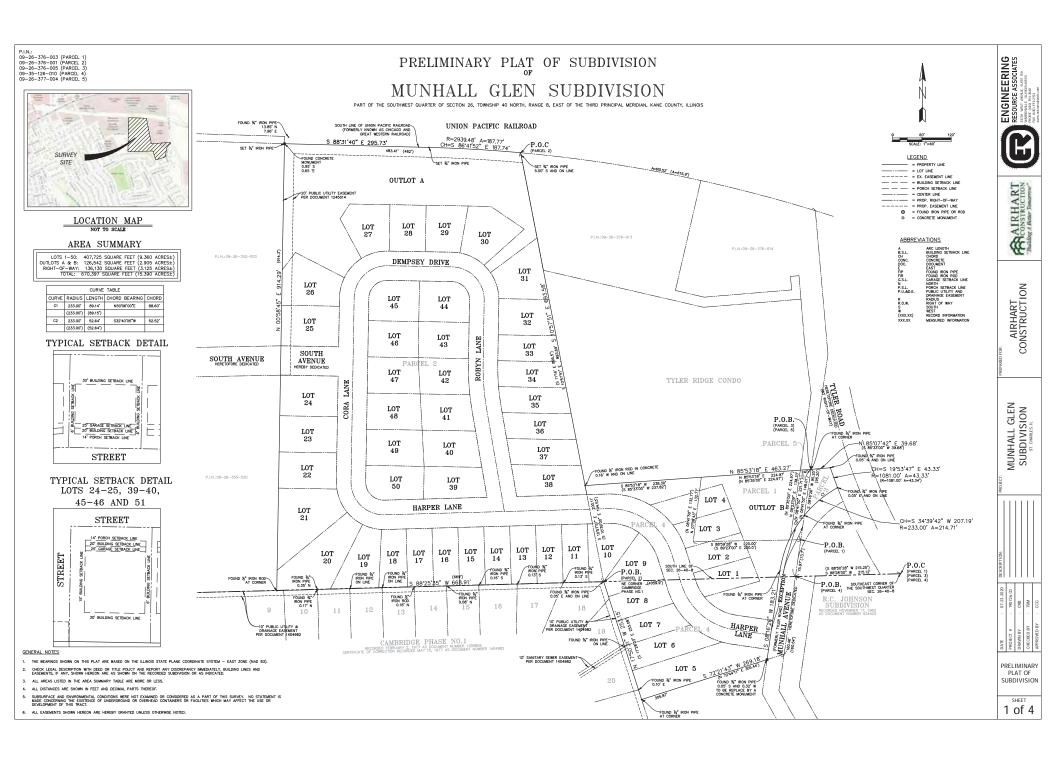


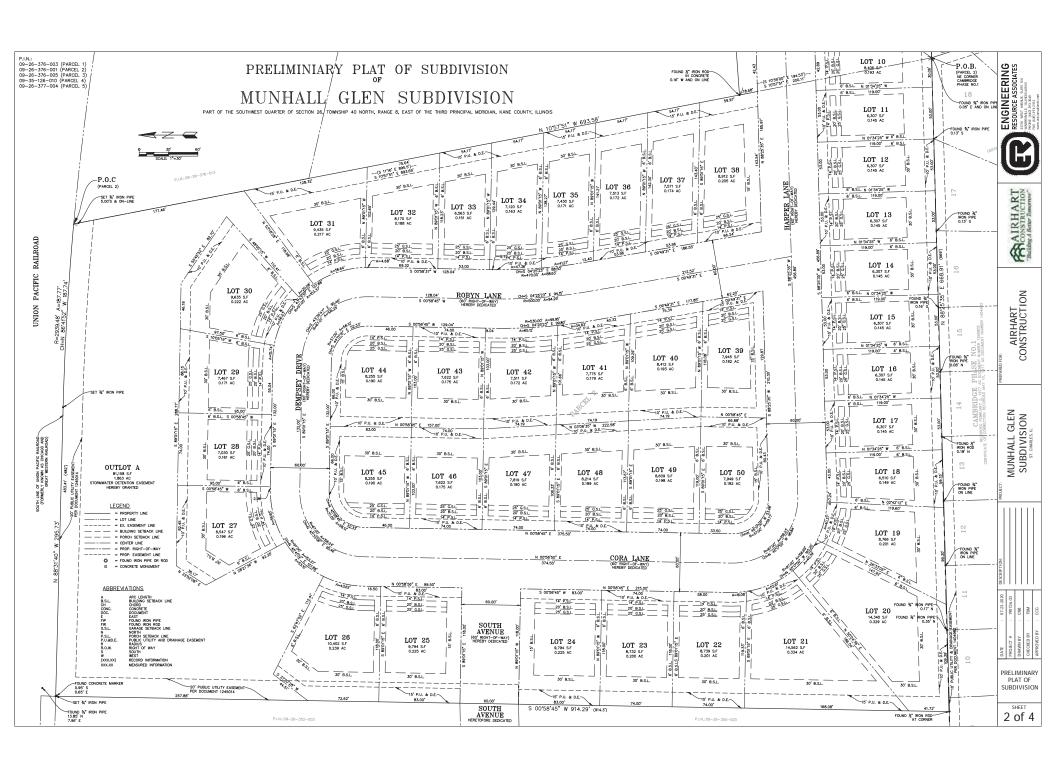


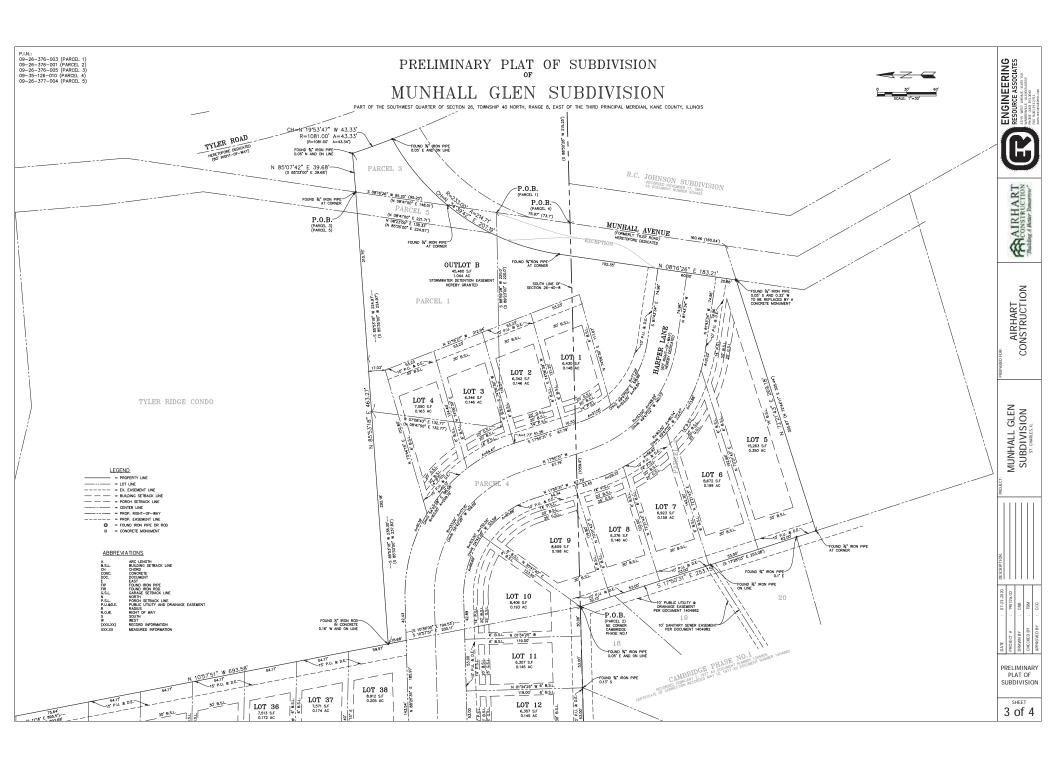
LOT 32 SHALLOW HOUSE DESIGN

11111134313313355	ACER S. FALL FESTA'S OUBCRUS RUSS DIQUES OF MANAGEMENT OF	SUMMER BEAUTY ALLIUM	2 1/2" 2 1/2" 6 48" 5 GAL. 18" 5 GAL. 5 GAL. 5 GAL. 1 GAL. 1 GAL. 1 GAL. 1 GAL.
	NOTE:		

LOT 33 NARROW HOUSE DESIGN







PLEASE MAIL A COPY OF THE RECORDED PLAT TO: RECORDED PLAT TO: ENGINEERING RESOURCE ASSOCIATES 35701 WEST AVENUE, SUITE 150 WARRENVILLE, ILLINOIS 60555

OWNER'S CERTIFICATE STATE OF ILLINOIS SS COUNTY OF KANE THIS IS O CERTIFY THAT THE UNDERSIONED IS THE OWNER OF THE LAND DESCRIBED IN THE ANNEXED PLAT, AND THAN HE AS CAUSED THE SAME TO BE SURVEYED AND SUBDIVIDED AS INDICATED THEREON, FOR THE USES AND PURPOSES THE SAME UNDER THE STATE AND THE SAME WHITE THEREON HOSE AND SUPPORT THE SAME UNDER THE STATE AND THE THEREON ALSO, THIS IS TO CERTIFY THAT PROPERTY BEING, SUBDIVIDED AFORESAID, AND TO THE BEST OF THE OWNER'S KNOWLEDGE AND BELIEF, SAID SUBDIVISION LIES ENTIRELY WITHIN THE LIMITS OF: ST. CHARLES COMMUNITY UNIT SCHOOL, DISTRICT 303 DATED THIS DAY OF AD 20 NOTARY'S CERTIFICATE STATE OF ILLINOIS) COUNTY OF KANE _____, A NOTARY PUBLIC, IN AND FOR SAID COUNTY, GIVEN UNDER MY HAND AND NOTARIAL SEAL THIS _____ DAY OF _____, A.D. 20___, AT _____ ILUNOIS. COUNTY CLERK'S CERTIFICATE STATE OF ILLINOIS SS I, HERE ARE NO DELINQUENT GENERAL TAXES, NO UNITY LERK OF KANE COUNTY, LILNOIS, DO HEREBY CERTIFY THAT ANY OF THE LAND INCLUDED IN THE ANNEXED PLAT. I PURITHER CERTIFY THAT I HAVE RECEIVED ALL STATUTORY FEES IN CONNECTION WITH THE ANNEXED PLAT. GIVEN UNDER MY HAND AND SEAL AT ______, ILLINOIS, THIS _____, DAY OF _____, A.D. 20___. DIRECTOR OF COMMUNITY DEVELOPMENT CERTIFICATE STATE OF ILLINOIS) I, DO HEREBY CERTIFY THAT THE REQUIRED IMPROVEMENTS HAVE BEEN INSTALLED, OR THE REQUIRED GUARANTEE BOND HAS BEEN POSTED FOR THE COMPLETION OF ALL REQUIRED LAND DIRECTOR OF COMMUNITY DEVELOPMENT DATED AT ______ ILLINOIS, THIS _____ DAY OF _____ A.D. 20____

PLAN COMMISSION	CERTIFICATE
STATE OF ILLINOIS	ss
COUNTY OF KANE	5
APPROVED THIS CITY OF ST. CHARLES	
CHAIRMAN	
CERTIFICATE AS	TO SPECIAL ASSESSMENTS
STATE OF ILLINOIS	>
COUNTY OF KANE	} ss
I DO HEREBY CERTIFY SPECIAL ASSESSMENT APPORTIONED AGAINS	THAT THERE ARE NO DELINQUENT OR UNPAID CURRENT OR FORFEITER S OR ANY DEFERRED INSTALLMENTS THEREOF THAT HAVE NOT BEEN T THE TRACT OF LAND INCLUDED IN THE PLAT.
COLLECTOR OF SPECI-	AL ASSESSMENTS
DATED AT	ILINOIS, THIS DAY OF A.D. 20
COUNTY ENGINEER	R'S CERTIFICATE
STATE OF ILLINOIS	Σ
COUNTY OF KANE	} ss
THIS PLAT HAS BEEN	APPROVED BY THE KANE COUNTY ENGINEER WITH RESPECT TO
ROADWAY ACCESS TO	PURSUANT TO ILCS CHAPTER 765 PARAGRAPH 205/
DATED THIS	DAY OF, A.D. 20
COUNTY ENGINEER	
DATED AT	, ILUNOIS, THIS DAY OF, A.D. 20
MORTGAGEE'S CEI	RTIFICATE
STATE OF ILLINOIS)
COUNTY OF KANE	ss
ACCEPTED AND APPR	OVED BY AS MORTGAGEE.
DATED AT	, ILLINOIS, THIS DAY OF , A.D., 20
BY:	
ATTEST:	
CITY COUNCIL CE	RTIFICATE
STATE OF ILLINOIS	>
COUNTY OF KANE	} ss
APPROVED AND ACCE CITY COUNCIL OF CIT	PTED THIS DAY OF, A.D. 20 Y OF ST. CHARLES, ILLINOIS
	MAYOR
ATTEST:	

PUBLIC LITLITY AND DRAINAGE EASEMENT PROVISORS

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DECLARATION OF COVENANTS CONDITIONS RESTRICTIONS AND EASEMENTS ALL OF THE PROPERTY INCLUDED IN THIS SUBDIVISION SHALL BE HELD, TRANSFERRED AND CONVEYED SUBJECT TO THAT CERTAIN DECLARATION OF COVENANTS, CONDITIONS, EASEMENTS AND

SPECIAL FLOOD HAZARD AREA CERTIFICATE

STATE OF ILLINOIS
COUNTY OF DUPAGE

FOR REVIEW ONLY

ILLINOIS PROFESSIONAL LAND SURVEYOR NUMBER 035-004022 LICENSE EXPIRES: NOVEMBER 30, 2020



SURVEYOR'S CERTIFICATE

STATE OF ILLINOIS SS COUNTY OF DUPAGE

THIS IS TO CERTIFY THAT I, COLUN C. GRAVES, ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 035-004022, HAVE SURVEYED AND SUBDIVIDED THE FOLLOWING DESCRIBED PROPERTY:

PARCE, 5

THAT PART OF THE SOUTHWEST QUARTER OF SECTION 28, TOWNSHIP 40 NORTH, RANGE 8, EAST OF THE BRIEF PRINCIPAL WERRICHARD, SCHOOL ST. TOWNSHIP 40 NORTH, RANGE 8, EAST OF THE BRIEF PRINCIPAL WERRICHARD AND A STATE AND

I HEREBY GRANT PERMISSION TO THE CITY OF ST. CHARLES CLERK OR AN EMPLOYEE OF THE CITY CLERK'S OFFICE TO RECORD THIS PLAT. THE REPRESENTATIVE SHALL PROVIDE THE SURVEYOR WITH A RECORDED COPY OF THIS PLAT.

GIVEN UNDER MY HAND AND SEAL AT ______ ILLINOIS, THIS ______ DAY OF ______ A.D. 20____

FOR REVIEW ONLY

DESIGN FIRM PROFESSIONAL LICENSE NO. 184.001186 LICENSE EXPIRES: APRIL 30, 2021



AIRHART

AIRHART CONSTRUCTION

MUNHALL GLEN SUBDIVISION

PRELIMINARY SUBDIVISION

SHEET 4 of 4

Floor Plans GARDEN SERIES





The Hemsley Ranch



The Hemsley Ranch Garden series - 1664

The Hemsley's charming exterior welcomes you to this home through a covered entry. Entering through the foyer into the expansive great room, the open floor plan allows for easy entertaining. The kitchen and breakfast area open to the rear yard and are located close to the rear entry. This entry includes space for a drop zone as well as a "Costco" sized storage room.

FEATURES:

- Foyer: Includes a coat closet, oak flooring, and opens to the great room
- Great Room: This room is bathed in natural light through three large windows, includes oak flooring and is open to the dining room
- Dining Room: Includes oak flooring and is open to the great room and kitchen
- Kitchen: Includes an expansive island, oak flooring, custom
 cabinetry, stainless steel appliances, pantry, granite counter
 tops with a stainless steel sink overlooking the rear yard, and
 space for a breakfast table.
- Rear Entry: Includes an expansive walk in closet for coats and "Costco" purchases, a perfect spot for a drop zone and is ideally located near kitchen
- Bedroom 1: The master bedroom suite showcases four bright windows and an expansive walk in closet
- Bathroom 1: Private bath suite boasts a water closet, two sinks in a large vanity and a custom shower with tile floors and walls with a high set window

- Bathroom 2: Full bathroom located discretely near bedroom 2
- Bedroom 2/Flex: This room includes a large closet, double windows overlooking the front yard and could be used as a home offic
- Laundry Room: Large centrally located laundry room provides easy access with linen closet located close by in hallway
- Nine foot ceilings throughout the main level
- Optional Deck: 12'0" x 10'0" deck provides enjoyable outdoor space in private back yard
- 2 car attached garage
- · Full basement
- Optional 2nd floor available which includes loft, bathroom, and 2 additional bedrooms
- Artist renderings and floor plans are for illustration purposes, changes or modifications may occur without notice.



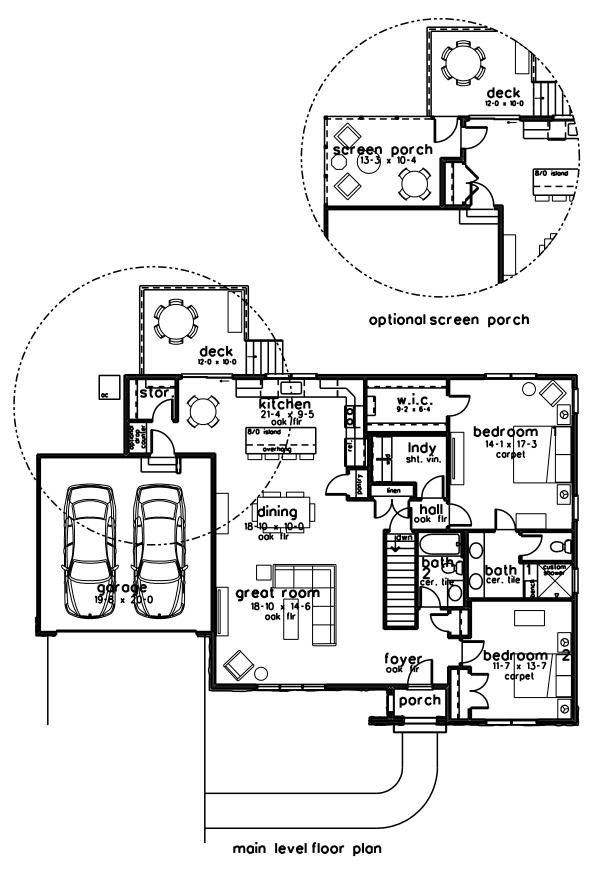




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The Hemsley Ranch



The Hemsley - 2 Story



The Hemsley - 2 Story Garden series - 2556

The Hemsley's charming exterior welcomes you to this home through a covered entry. Entering through the foyer into the expansive great room, the open floor plan allows for easy entertaining. The kitchen and breakfast area open to the rear yard and are located close to the rear entry. This entry includes space for a drop zone as well as a "Costco" sized storage room.

The upper level includes a spacious loft as well as 2 additional bedrooms and full bathroom.

FEATURES:

- Foyer: Includes a coat closet, oak flooring, and opens to the great room
- Great Room: This room is bathed in natural light through three large windows, includes oak flooring and is open to the dining room
- Dining Room: Includes oak flooring and is open to the great room and kitchen
- Kitchen: Includes an expansive island, oak flooring, custom cabinetry, stainless steel appliances, pantry, granite counter tops with a stainless steel sink overlooking the rear yard, and • space for a breakfast table.
- Rear Entry: Includes an expansive walk in closet for coats and "Costco" purchases, a perfect spot for a drop zone and is ideally located near kitchen
- Bedroom 1: The master bedroom suite showcases four bright windows and an expansive walk in closet
- Bathroom 1: Private bath suite boasts a water closet, two sinks in a large vanity and a custom shower with tile floors and walls with a high set window
- Powder Room: Located discretely or rear hall with pedestal vanity

- Office/Flex This multi-use room can be used as home office overflow bedroom, TV room, etc.
- Laundry Room: Large centrally located laundry room provides easy access with linen closet located close by in hallway
- Nine foot ceilings throughout the main level
- Loft: This open space offers a lot of options, such as a great art studio, computer room, craft space, game room, etc.
 Includes large linen closet for storage. This space also could be turned into a 4th bedroom!
- Bedroom 2: Includes walk-in closet and large sitting area overlooking front yard through 2 large windows
- Bedroom 3: Includes large closet and window overlooking front yard
- Bath 2: Full bath with easy access from bedrooms 2, 3 and loft
- Optional Deck: 12'0" x 10'0" deck provides enjoyable outdoor space in private back yard
- · 2 car attached garage
- Full basement
- Artist renderings and floor plans are for illustration purposes, changes or modifications may occur without notice.



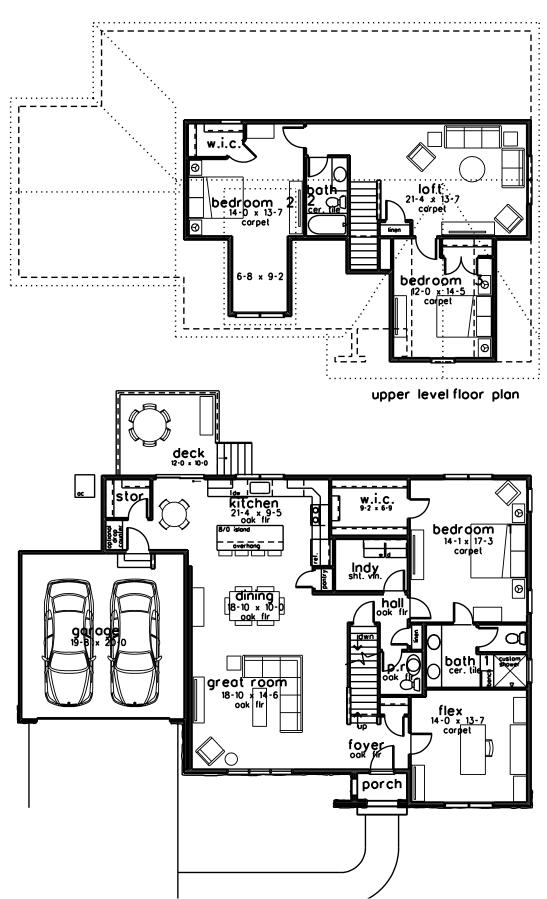




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The Hemsley - 2 Story First Floor Master



The St. James Ranch



The St. James

Garden series - 1915

The St. James delivers ample amenities within a very comfortable floor plan. Entering from the porch, the foyer welcomes you, and provides views into the den which is large enough to hold a piano, or can function as an offic away from the household business. The rear of the home provides plenty of light filled living space. The kitchen, dining, and great room are open to each other, all with views of the rear yard. The master bedroom suite is expansive and has all the features you would want. Two additional bedrooms and a full bathroom complete this home.

FEATURES:

- Porch: Covered porch welcomes your guests to the home.
- Foyer: Includes coat closet, oak flooring, plenty of light, and opens to the den.
- Den: Provides a welcoming space for a seating area or home
 office and is large enough for a pia
- Great Room: Includes oak floo, three large windows overlooking the rear yard, and is open to the kitchen and dining room
- Dining Room: Includes oak flooring and overlooks the rear yard though large sliding glass doors which provide easy access to the rear deck
- Kitchen: Includes a spacious island, oak flooring, custom cabinetry, stainless steel appliances, pantry, and granite counter tops with stainless steel sink overlooking the backyard through 2 windows.
- Laundry Room: Laundry room is centrally located, includes coat closet and provides access to garage.

- Bedroom 1: The large master bedroom suite showcases 2 bright windows overlooking the rear yard and an expansive walk-in closet
- Bathroom 1: Private bath suite boasts a water closet, two sinks in a large vanity and a custom shower with tiled floors and walls and a high set window
- Bathroom 2: Full bathroom located discretely of the hall near bedroom 2 and 3
- Bedroom 2: This bedroom includes a large closet and double windows overlooking the front yard
- Bedroom 3: This bedroom includes a large closet and double windows overlooking the front yard
- · Nine foot ceilings throughout
- Optional Deck: 12'0" x 10'0" deck provides enjoyable outdoor space in private back yard
- · 2 car attached garage
- Full basement
- Artist renderings and floor plans are for illustration purposes only, changes or modifications may occur without notice

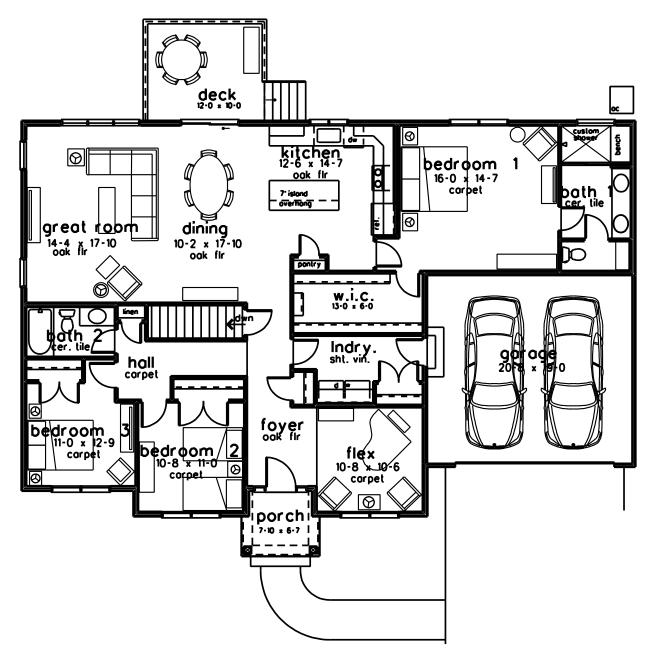






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The St. James Ranch



main level floor plan

The Macrae Ranch



The Macrae Ranch

Garden series - 1635

The Macrae's large front porch adds charm to the striking design of this ranch home. The open floor plan is ideally suited for easy living. With ample storage, great living spaces, and laundry room just in the right location, this home lives large.

FEATURES:

- Porch: The generous covered front porch welcomes you to the home and provides plenty of room for seating
- Foyer: Includes a coat closet, oak flooring, and opens to the great room
- Great Room: Includes three large windows overlooking the front porch, oak flooring, and is open to the dining roo
- Dining Room: Includes oak flooring and is open to the great room and kitchen
- Kitchen: Includes expansive island with seating on 2 sides, oak flooring, custom cabinetr, stainless steel appliances, pantry, and granite counter tops with stainless steel sink overlooking rear yard
- Sitting Room: This sunny multi use room, which can be used
 as a breakfast area, is open to the kitchen and leads to deck
 overlooking rear yard, includes oak floorin
- Mud Room: Light filled entry from garage includes an expansive walk in closet for coats and "Costco" purchases and is ideally located near kitchen
- Bedroom 1: The master bedroom suite showcases four bright windows and an expansive walk in closet

- Bathroom 1: Private bath suite boasts a water closet, two sinks in a large vanity and a custom shower with tile floors and walls, and a high set window
- Bathroom 2: Full bathroom located discretely off the hal
- Bedroom 2: This room includes a large closet, double windows overlooking the front yard, and could be used as a home offic
- Laundry Room: Large centrally located laundry room provides easy access with linen closet located close by in hallway
- Nine foot ceilings throughout the main level
- Optional Deck: 12'0" x 10'0" deck provides enjoyable outdoor space in private back yard
- 2 car attached garage
- Full basement
- Optional 2nd floor available which includes loft, bathroom and additional bedroom
- Artist renderings and floor plans are for illustration purposes only, changes or modifications may occur without notice.



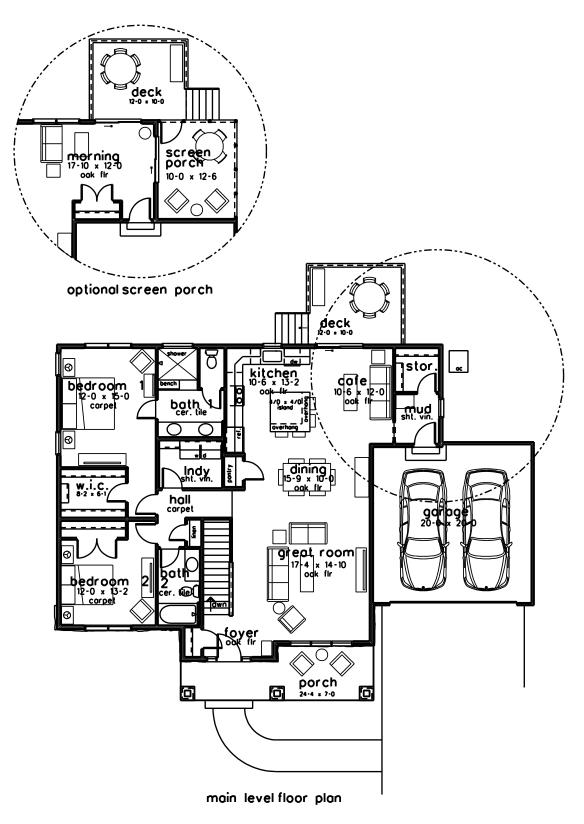




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The Macrae Ranch



The Macrae - 2 Story



The Macrae - 2 Story Garden series - 2295

The Macrae's large front porch adds charm to the striking design of this ranch home. The open floor plan is ideally suited for easy living. With ample storage, great living spaces, and laundry room just in the right location, this home lives large. The upper level includes an additional bedroom and loft space for home office, art studio, etc

FEATURES:

- · Porch: The generous covered front porch welcomes you to the home and provides plenty of room for seating
- Foyer: Includes a coat closet, oak flooring, and opens to the great room
- Great Room: Includes three large windows overlooking the front porch, oak flooring, and is open to the dining roo
- Dining Room: Includes oak flooring and is open to the great room and kitchen
- Kitchen: Includes expansive island with seating on 2 sides, oak flooring, custom cabinetr, stainless steel appliances, pantry, and granite counter tops with stainless steel sink overlooking rear yard
- Sitting Room: This sunny multi use room, which can be used as a breakfast area, is open to the kitchen and leads to deck overlooking rear yard, includes oak floorin
- Mud Room: Light filled entry from garage includes an expansive walk in closet for coats and "Costco" purchases and is ideally located near kitchen
- Laundry Room: Large centrally located laundry room provides easy access with linen closet located close by in hallway
- Bedroom 1: The master bedroom suite showcases four bright windows and an expansive walk in closet

- Bathroom 1: Private bath suite boasts a water closet, two sinks in a large vanity and a custom shower with tile floors and walls, and a high set window
- Bathroom 2: Full bathroom located discretely off the hal
- Bedroom 2: This room includes a large closet, double windows overlooking the front yard, and could be used as a home offic
- Loft: This open space offers a lot of options, such as a great art studio, computer room, craft space, game room, etc. Includes large linen closet for storage. This space also could be turned into a 4th bedroom!
- Bedroom 3: Includes walk-in closet, this bedroom overlooks the rear yard through 2 large windows
- Bath 3: Full bath with easy access from bedroom 3 and loft
- Nine foot ceilings throughout the main level
- Optional Deck: 12'0" x 10'0" deck provides enjoyable outdoor space in private back yard
- 2 car attached garage
- Full basement
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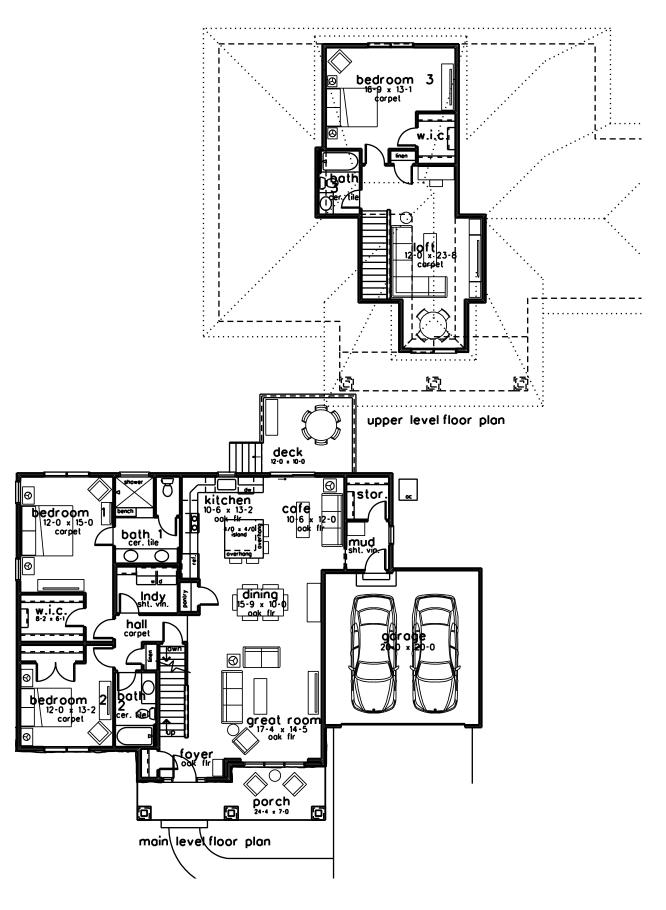




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The Macrae-2 Story First Floor Master



The Cloverdale



The Cloverdale Ranch

Garden series - 1915

The Cloverdale delivers ample amenities with extra glass and a modern elevation. Entering from the porch, the foyer welcomes you, and provides views into the den which is large enough to hold a piano, or can function as an offic away from the household business. The rear of the home provides plenty of light filled living space. The kitchen, dining, and great room are open to each other, all with views of the rear yard. The master bedroom suite is expansive and has all the features you would want. Two additional bedrooms and a full bathroom complete this home.

FEATURES:

- Porch: Covered porch welcomes your guests to the home.
- Foyer: Includes coat closet, oak flooring, plenty of light, and opens to the den.
- Den: Provides a welcoming space for a seating area or home
 office and is large enough for a pia
- Great Room: Includes oak floo, three large windows overlooking the rear yard, and is open to the kitchen and dining room
- Dining Room: Includes oak flooring and overlooks the rear yard though large sliding glass doors which provide easy access to the rear deck
- Kitchen: Includes a spacious island, oak flooring, custom cabinetry, stainless steel appliances, pantry, and granite counter tops with stainless steel sink overlooking the backyard through 2 windows.
- Laundry Room: Laundry room is centrally located, includes coat closet and provides access to garage.

- Bedroom 1: The large master bedroom suite showcases 2 bright windows overlooking the rear yard and an expansive walk-in closet
- Bathroom 1: Private bath suite boasts a water closet, two sinks in a large vanity and a custom shower with tiled floors and walls and a high set window
- Bathroom 2: Full bathroom located discretely of the hall near bedroom 2 and 3
- Bedroom 2: This bedroom includes a large closet and double windows overlooking the front yard
- Bedroom 3: This bedroom includes a large closet and double windows overlooking the front yard
- · Nine foot ceilings throughout
- Optional Deck: 12'0" x 10'0" deck provides enjoyable outdoor space in private back yard
- · 2 car attached garage
- Full basement
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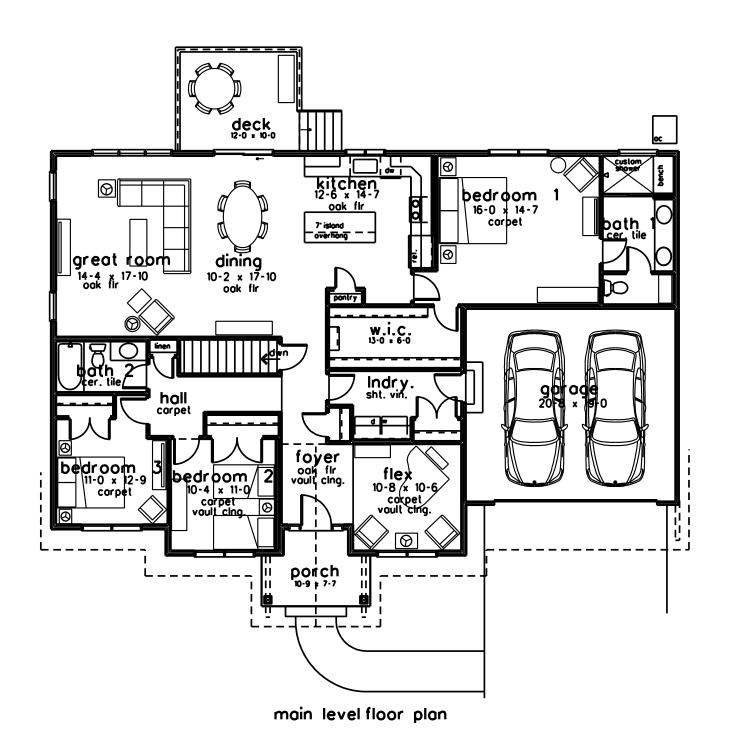




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The Cloverdale Ranch



The Leyland Ranch



The Leyland Ranch Garden series - 1785

The Leyland's single story provides an ease of living with its open floor plan and well designed spaces. The large kitchen is open to the great room and dining room which look out through expansive windows to the back yard. Just off the dining area is ample storage and easy access to the laundry room as well as an innovative pocket office This inviting floor plan, including master bedroom and 2 additional bedrooms, has everything you need

FEATURES:

- Porch: The expansive covered porch welcomes you to the home and provides plenty of room for seating
- Foyer: Includes coat closet, oak flooring, plenty of light and open stairs to the lower level
- Great Room: Includes oak floo, two large windows overlooking the rear yard, and open to the kitchen and dining
 room; perfect for entertaining
- Dining Room: This large space is open to the great room and includes oak flooring with views and access to the rear yar
- Kitchen: Boasts expansive island with sink over-looking the great room, oak flooring, custom cabinetr, stainless steel appliances, large pantry, and granite counter tops with stainless steel sink
- Pocket Office This clever use of space located off the rear hall provides the perfect area to handle the busyness of life
- Laundry Room: Light filled laundry room off the rear hall i ideally located near kitchen
- Mud Room: Includes an expansive walk in closet for coats and "Costco" purchases and is ideally located near kitchen

- Bedroom 1: The master bedroom suite showcases 2 bright windows overlooking the rear yard
- Bathroom 1: Private bath suite boasts a water closet, two sinks in a large vanity, custom corner shower with tile floors and walls, a high set window, and a large walk in closet
- Bathroom 2: Full bathroom located discretely off the hall near bedroom 2 and the office/flex r
- Bedroom 2: This bedroom includes a large closet and double windows overlooking the front yard
- Office/Flex/Bedroom 3 This space provides plenty of possibilities and can be used as home office, bedroom TV room, etc.
- Nine foot ceilings throughout
- Optional Deck: 12'0" x 10'0" deck provides enjoyable outdoor space in private back yard
- 2 car attached garage
- · Full basement
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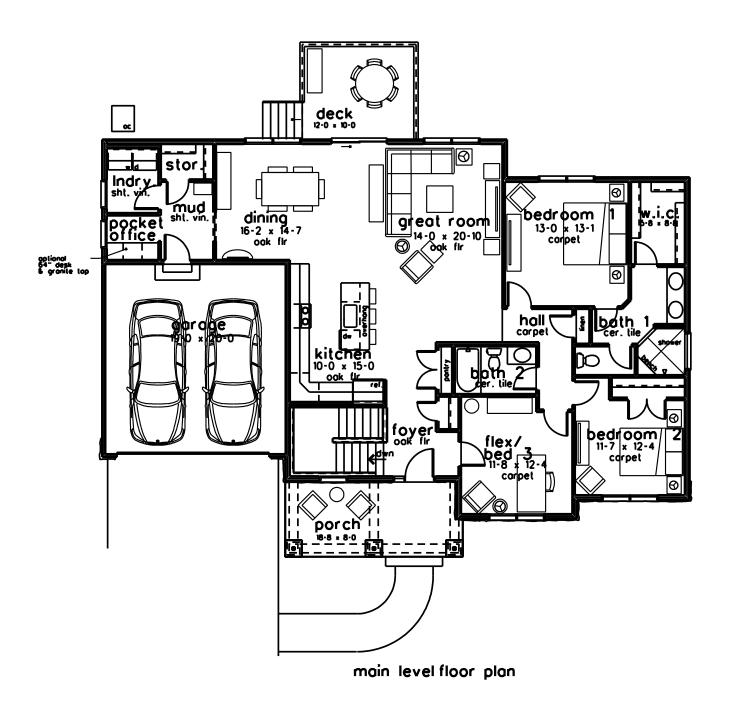




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The Leyland Ranch





The Avery Ranch Garden series - 1915

The Avery delivers ample amenities with a traditional brick elevation. Entering from the porch, the foyer welcomes you, and provides views into the den which is large enough to hold a piano, or can function as an offic away from the household business. The rear of the home provides plenty of light filled living space. The kitchen, dining, and great room are open to each other, all with views of the rear yard. The master bedroom suite is expansive and has all the features you would want. Two additional bedrooms and a full bathroom complete this home.

FEATURES:

- Porch: Covered porch welcomes your guests to the home.
- Foyer: Includes coat closet, oak flooring, plenty of light, and opens to the den.
- Den: Provides a welcoming space for a seating area or home office and is large enough for a pia
- Great Room: Includes oak floo , three large windows overlooking the rear yard, and is open to the kitchen and dining room
- Dining Room: Includes oak flooring and overlooks the rear yard though large sliding glass doors which provide easy access to the rear deck
- Kitchen: Includes a spacious island, oak flooring, custom cabinetry, stainless steel appliances, pantry, and granite counter tops with stainless steel sink overlooking the backyard through 2 windows.
- Laundry Room: Laundry room is centrally located, includes coat closet and provides access to garage.

- Bedroom 1: The large master bedroom suite showcases 2 bright windows overlooking the rear yard and an expansive walk-in closet
- Bathroom 1: Private bath suite boasts a water closet, two sinks in a large vanity and a custom shower with tiled floors and walls and a high set window
- Bathroom 2: Full bathroom located discretely of the hall near bedroom 2 and 3
- Bedroom 2: This bedroom includes a large closet and double windows overlooking the front yard
- Bedroom 3: This bedroom includes a large closet and double windows overlooking the front yard
- Nine foot ceilings throughout
- Optional Deck: 12'0" x 10'0" deck provides enjoyable outdoor space in private back yard
- · 2 car attached garage
- Full basement
- · Artist renderings and floor plans are for illustration purposes only, changes or modifications may occur without notice

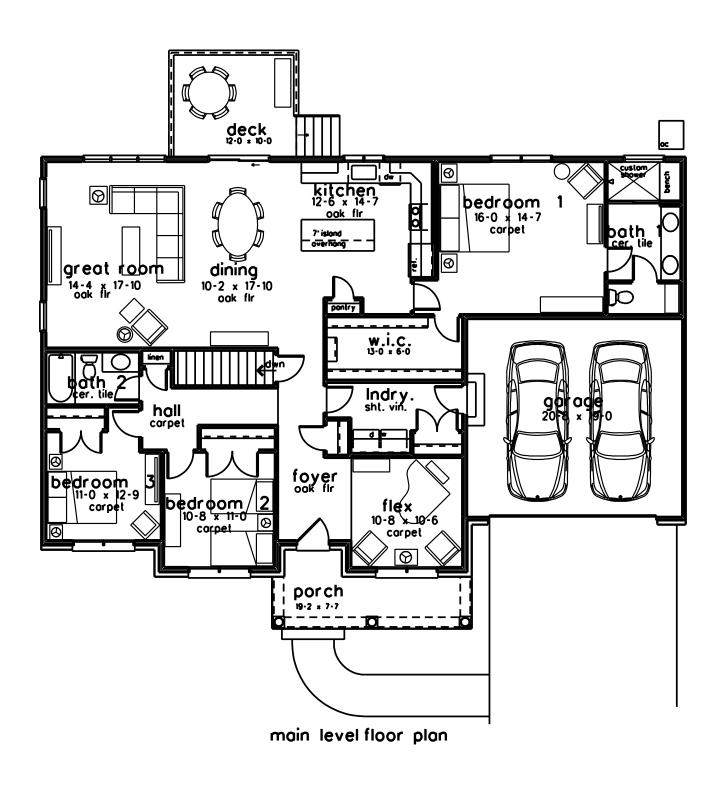






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The Avery Ranch



The Chestnut Hill



The Chestnut Hill

Garden series - 2751

The Chestnut Hill's large front porch and craftsman exterior invite you into this home. The main level provides plenty of opportunity for entertaining with its wide open floor plan as well as providing cozy, personal spaces. The offic provides a private place to get away. The master bedroom and luxurious master bath complete the main floo . The upper level includes a spacious loft as well as 2 additional bedrooms and full bathroom.

FEATURES:

- Covered Front Porch: Welcomes you to the home and provides plenty of room for seating
- Foyer: Expansive foyer includes 2 coat closets and oak floorin
- Great Room: Includes two large windows overlooking the front porch and oak floorin
- Dining Room: Includes oak flooring is open to the great room
 and kitchen
- Kitchen: Boasts expansive island, oak flooring, custom cabinetry, stainless steel appliances, and granite counter tops with stainless steel sink which overlooks the rear yard through-2 windows.
- Gathering Room: This sunny multi-use room is open to the kitchen, looks out onto the rear yard, and includes oak floorin
- Laundry Room: Light filled laundry room includes rear hall closet and is ideally located near kitchen
- Bedroom 1: The master bedroom suite showcases four bright
 windows and an expansive walk in closet
- Bathroom 1: Private bath suite boasts a water closet, two sinks on a large vanity, custom shower with tiled floor and

- walls and a high set window, and a linen closet for extra storage
- Office/Flex This dual-use space can be used as home office overflow bedroom, TV room, etc.
- Bathroom 2: Full bathroom located discretely off the rear hall near office/flex room with a large linen closet near
- Nine foot ceilings throughout the main level
- Loft: This open space provides lots of options from a great art studio, computer room, craft space, game room, etc. includes large linen closet for storage
- Bedroom 2: This bright room includes a walk in closet, 2 sets of double windows, and a fun nook overlooking the front yard
- Bedroom 3: Includes a large double window and a walk-in closet
- Bath 3: Full bath with easy access from bedrooms 2 & 3
- Optional Deck: 12'0" x 10'0" provides enjoyable outdoor space in private back yard
- · 2 car attached garage
- Full basement
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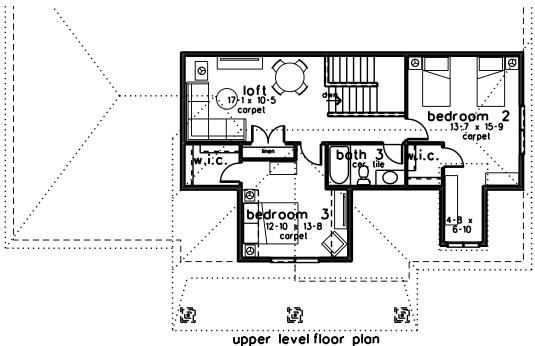


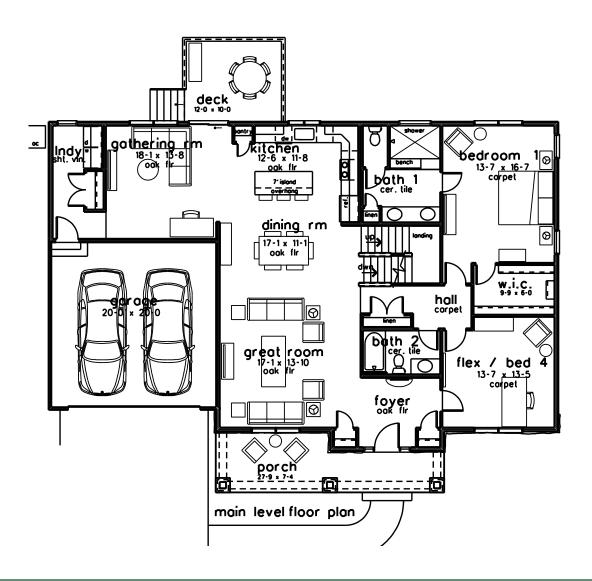


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The Chestnut Hill





Premier Series Floor Plans









The Carlton

Premier Series—1924

This traditional style home offers exceptional living and entertaining opportunities. The open kitchen serves as an excellent gathering area with easy access to the great room and dining. A covered entry with porch, shown with the stone option, provides a warm and inviting exterior.

FEATURES:

- Foyer: spacious entry with closet and open views to the den
- Den: 11'9' x 10'4" a spacious first floor room with large windows overlooking the front porch
- Great Room: 18' x 18'2" this generous room boasts triple windows, creating a light-filled room
- Kitchen: 9'6" x 15'4" this wonderful family kitchen has an island with eating bar, windows over the sink, spacious walk-in pantry, custom crafted cabinetry and stainless steel appliances
- Breakfast Room: 10'9" x 10'4" convenient access to the rear yard through the patio door
- Powder Room: features a pedestal sink
- Master Bedroom: 12' x 12' houses two large windows and a full bathroom
- Master Bathroom: full bathroom and large 10'4" x 6' walk-in closet

- Bedroom 2: 13'3" x 11' includes a closet and large windows
- Bedroom 3: 10' x 12'1" includes a closet and large windows
- Bedroom 4: 10' x 13'1" includes a closet and a window overlooking the backyard
- Bath 2: full bathroom with a nearby linen closet
- Second floor laundry room
- 9' ceilings on the first floor
- Full basement
- 2 car attached garage (Shown with optional windows in the garage door)
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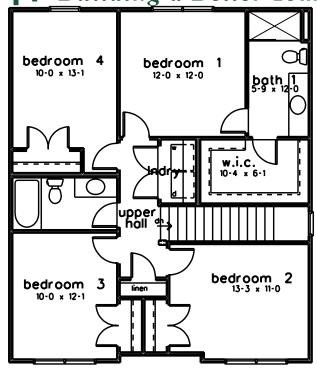


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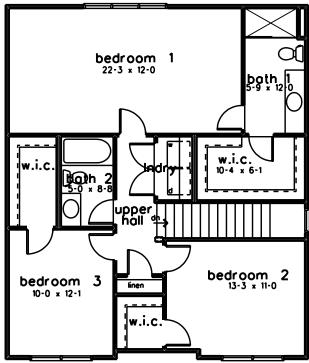
The Carlton

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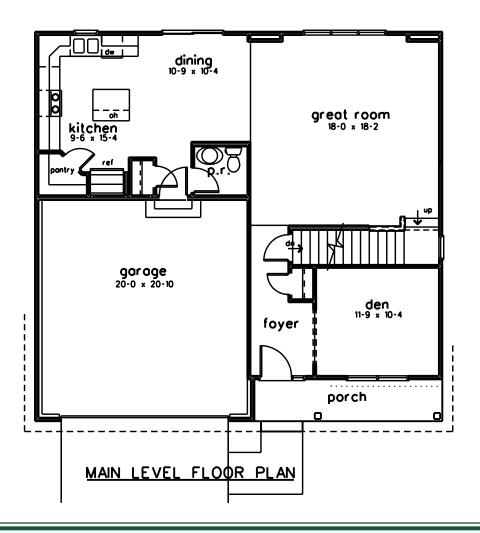
"Building a Better Tomorrow"



UPPER LEVEL FLOOR PLAN



3 BEDROOM OPTION



The Glen Arbor



The Glen Arbor

Premier Series—1700

This innovative ranch design provides an abundance of bright sunny living areas and flexible multipurpose rooms set in a dramatic open floor plan.

FEATURES:

- Foyer: spacious and open entry with coat closet and 9' ceiling
- Bedroom 3: 13'5"x 10'6" features a large sunny windows at the font of the home perfect for a home office
- Bedroom 2: 10'x11'9" quiet and private bedroom retreat with nearby full bathroom
- Bathroom 2: full bathroom near bedroom 2 and bedroom 3
- Great Room: 15'8"x 14'4" bright and airy room with 9'ceilings, and triple windows. This open layout provides plenty of entertainment opportunities.
- Dining room: 9'4" x 13'4" open to the kitchen and great room, may be formal or informal
- Kitchen: 19'3" x 9'5" features oak floors, stainless steel appliances, custom cabinets and a large pantry. The kitchen is

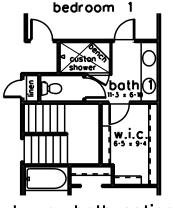
open to the great room/dining room and includes a large bar top that is perfect for conversing while cooking

- Bedroom 1: 15' x 14'2" with 9' ceiling and views to rear yard
- Master Bathroom: with long vanity, spacious shower, and large walk-in closet
- Main floor laundry room
- Mud room with space for an optional bench and coat closet
- Two car attached garage
- · Full basement
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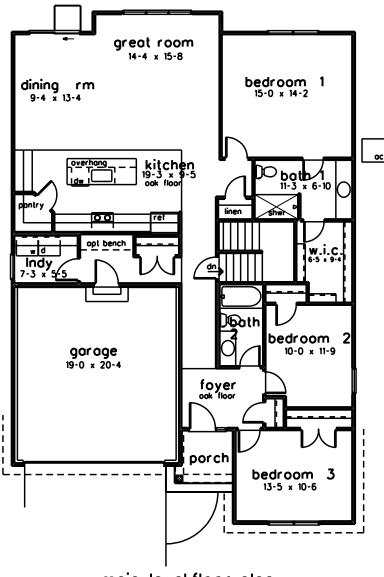




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main level floor plan



The Sinclair

Premier Series-2120

This traditional home offers exceptional living and entertaining opportunities. The spacious kitchen serves as an excellent gathering area with easy access to the great room and breakfast room.

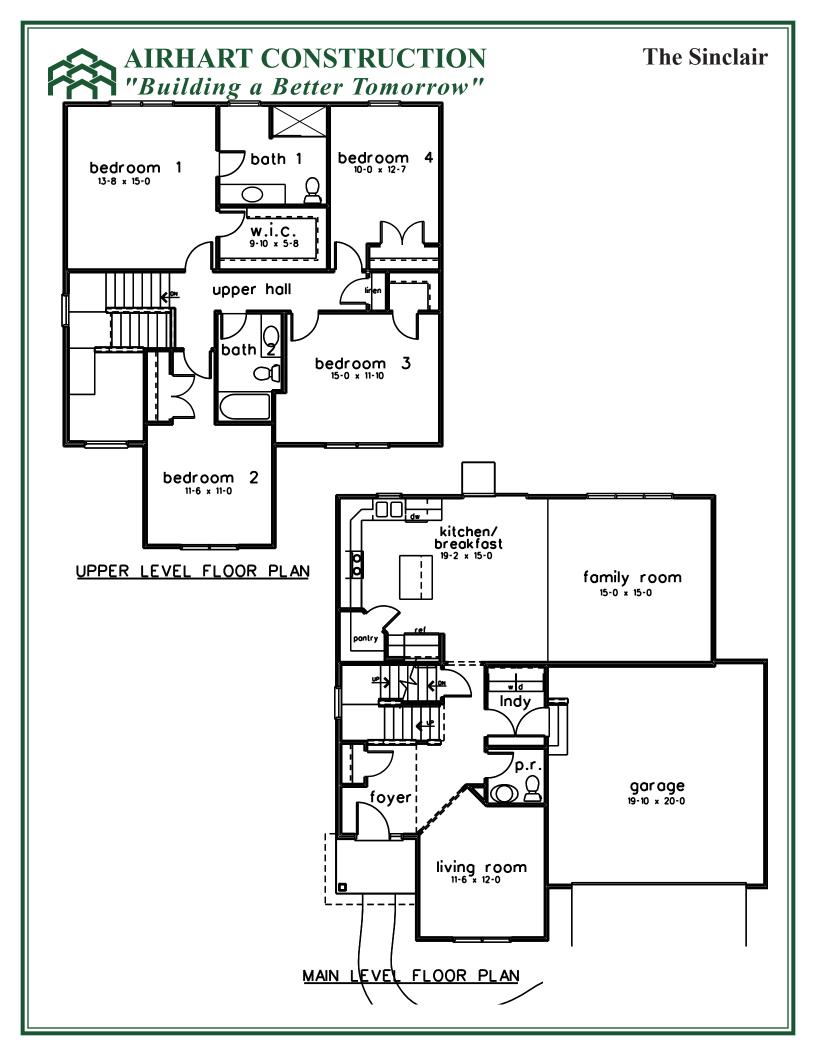
FEATURES:

- Foyer: spacious entry is flooded with light from the soaring two-story ceiling, which holds a bright, large window
- Living Room/Den: 11'6" x 12' angled entry and large windows overlooking the front yard
- Family Room: 15' x 15' generous room boasting triple windows which create a sunny room
- Kitchen/Breakfast Room: 19'2" x 15' this wonderful kitchen has custom cabinetry, a large center island with eating bar, a walkin pantry and stainless steel appliances. The spacious breakfast area is perfect for family gatherings, and has large sliding glass door to the backyard.
- Powder Room: features a pedestal sink
- Staircase: features a window and a two-story foyer, which creates a light-filled upper hallway.
- Master Bedroom: 13'8" x 15' has two large windows, an oversized walk-in closet, and a bathroom



- Master Bathroom: full bathroom with a sunny window
- Bedroom 2: 11'6" x 11' includes closet and large windows
- Bedroom 3: 15' x 11'10" includes walk-in closet and large windows
- Bedroom 4: 10' x 12'7" closet and window overlooking the backyard
- Bath 2: full bathroom with nearby linen closet
- · First floor laundry
- 9' first floor ceilings
- · Full basement
- 2 car attached garage (Shown with optional windows in the garage door)
- Artist renderings and floor plans are for illustration purposes, changes or modifications may occur without notice. Please review the Construction Standards and Energy Features brochure and contract blueprint for detailed information.

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The Oakfield

Elevation C









Elevation B

The Oakfield

Premier Series—1871

This well-designed plan provides many amenities that you would expect to find in a ranch home today. The master suite features a wonderful bathroom with a large walk-in closet, and the great room is open to the kitchen, breakfast and dining room, providing a great place to entertain.

FEATURES:

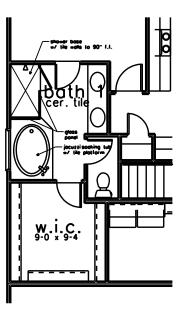
- Foyer: 10'4"x8'10" spacious and open entry with window, coat closet and 9' ceiling
- Den/Bedroom 3: 12' x 11'9" large sunny window and large closet
- Bedroom 2: 12'x10'7"
- Bathroom 2: full bathroom
- Dining Room: 12'3"x 12'11" open to great room and kitchen, 9'ceilings, and two large windows. This open layout provides plenty of entertainment opportunities.
- Great Room: 14'3"x15' triple windows, open to kitchen and dining room
- Kitchen: 11'6" x 14'8" features stainless steel appliances, custom cabinets and a pantry. The kitchen is open to the great room/dining room and includes a large bar top that is perfect for conversing while cooking.

- Breakfast Room: 9'8" x 11'6" open to the great room and features a sliding glass door
- Bedroom 1: 12'10" x 15' with 9' ceiling and views to the backyard
- Master Bathroom: has a spacious full bathroom and a large walk-in closet
- Laundry Room: 10'9" x 7'5" located on the main floor
- Two car attached garage (Shown with optional windows in the garage door)
- · Full basement
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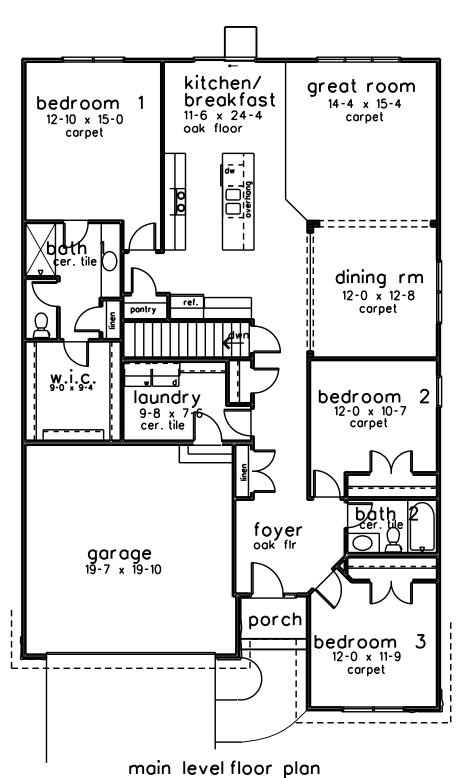




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luxury bath option





The Lynford Premier Series—1707

This innovative ranch design provides an abundance of bright sunny living areas and flexible multipurpose rooms set in a dramatic open floor plan. The Lynford design features a three car tandem garage.

FEATURES:

- Foyer: spacious and open entry with coat closet and 9' ceiling
- Bedroom 3: 13'5"x 10'6" features a large sunny windows at the font of the home perfect for a home office
- Bedroom 2: 10'x11'9" quiet and private bedroom retreat with nearby full bathroom
- Bathroom 2: full bathroom near bedroom 2 and bedroom 3
- Great Room: 13'1"x 14'1" bright and airy room with 9'ceilings, and triple windows. This open layout provides plenty of entertainment opportunities.
- Dining room: 9'6" x 13'1" open to the kitchen and great room, may be formal or informal
- Kitchen: 19'3" x 9'5" features oak floors, stainless steel appliances, custom cabinets and a large pantry. The kitchen is

open to the great room/dining room and includes a large bar top that is perfect for conversing while cooking

- Bedroom 1: 15' x 14'2" with 9' ceiling and views to rear yard
- Master Bathroom: with long vanity, spacious shower, and large walk-in closet
- Main floor laundry room with an extra closet
- Mud room with space for an optional bench and coat closet
- Three car attached garage
- · Full basement
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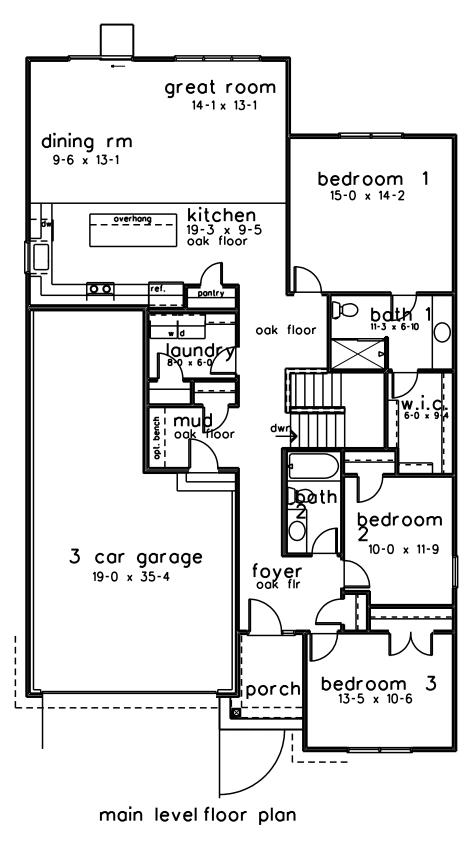




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The Legacy Premier Series 2523

The Legacy is a spacious family home which includes 9' first floor ceilings, 4 bedrooms, 2 ½ bathrooms, an expansive kitchen, and a great room. New home design features include a second floor laundry room, first floor mud room, and a kitchen fit for families of any size.

FEATURES:

- Foyer: the beautiful covered entry leads to the open foyer, which features two sidelight windows, a coat closet, and 9' ceiling
- Living Room/Den: 11'x 11' can easily be transformed into a first floor bedroom.
- Great Room: 18'4"x15' three windows, 9' ceiling, open to kitchen and dining area
- Kitchen: 15'3" x 8'4" features island with eating bar, stainless steel appliances, custom cabinets and a walk-in pantry. The kitchen is open to the great room/dining area. Sink is set on an angle under two corner windows
- Dining Area: 12' x 15'4" open to the great room, features a pepper box bay with a sliding glass door to rear yard. This ample eating area can fit a large family table and hutch.
- Mudroom: located off of the garage entry, this room is perfect for coats and boots. Add optional coat hooks and sitting bench for a creative way to store the kids' backpacks and sports equipment.

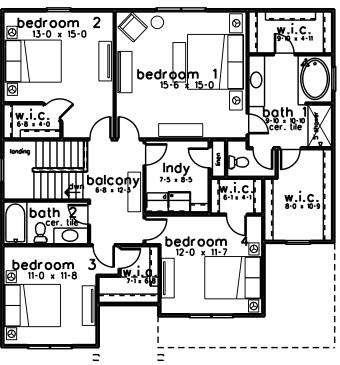
- Bedroom 1: 16'6" x 15' spacious bedroom showcasing two windows with views of the backyard
- Master Bathroom: has a long vanity, shower, separate soaking tub, linen closet an oversized 8' x 11'10" walk-in closet.
- Bedroom 2: 13' x 12'7'
- · Bedroom 3: 11'x 11'8" features a walk-in closet
- Bedroom 4: 12'x11'7" features a walk-in closet
- Bathroom 2: full bathroom
- Laundry Room: 7'11" x 8'5" second floor laundry
- Two car attached garage (shown with optional windows on garage door)
- · Full basement
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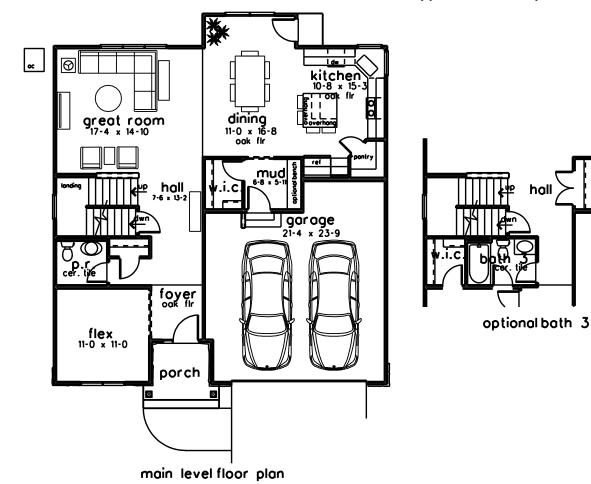


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upper level floor plan





Elevation A

Craftsman Elevation



The Sandhill

The Sandhill plan is open concept living at its best! This fantastic first floor master plan features comfortable open living spaces, 9' first floor ceilings, 4 bedrooms, 3 bathrooms, a spacious second floor loft, a full basement, and a dramatic kitchen dining/great room area. Stone or brick options are available with a spacious porch.

- **FEATURES:** Foyer: 9' ceiling, coat closet and a side light window
- Dining Room: open to kitchen and great room. This open layout Bedroom 3: generously sized second floor bedroom with a provides plenty of entertainment opportunities.
- Great Room: bright and open gathering space with triple window and views of breakfast, kitchen and dining rooms
- Kitchen: features an oversized island with eating bar, stainless steel appliances, custom cabinets, granite counters, wood floors and a pantry. The kitchen is open to both the great room and dining room.
- Bedroom 1: Spacious first floor master bedroom
- Master bathroom features a long vanity, shower, linen closet and a large walk-in closet
- Bedroom 2: first floor bedroom may also be used as a home office or flex room
- Bathroom 2: full bathroom with nearby linen closet

- · Laundry room on the first floor
- walk-in closet and nearby full bathroom and loft
- Bedroom 4: generously sized bedroom with a sitting/study area, walk-in closet and nearby full bathroom and loft
- Loft: located on the second floor makes a great family room
- Bathroom 3: full bathroom located on the second floor. A generous linen closet is located nearby.
- · Full basement
- Two car attached garage
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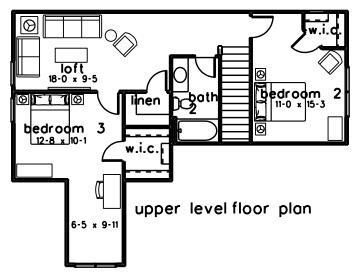


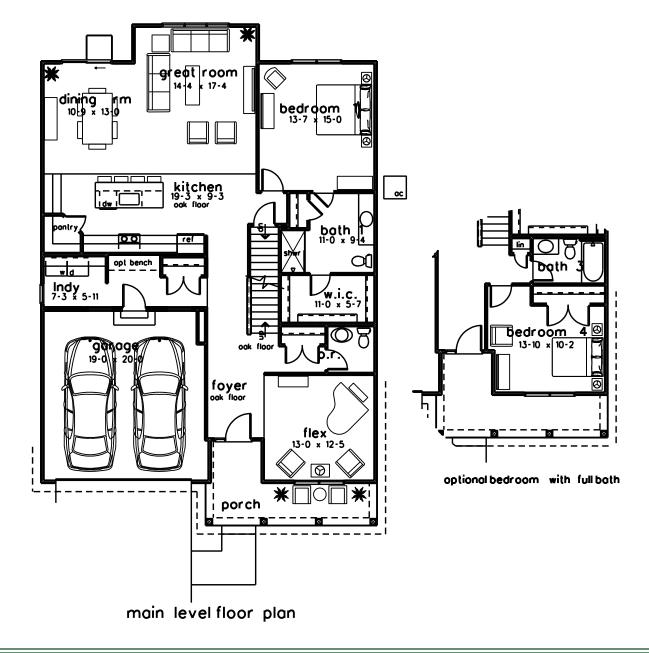
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The Rockwell Premier Series—2541

This striking modern traditional home features a covered front entry, an open floor plan boasting four bedrooms, 2½ bathrooms, a away room and an airy great room with tall windows.

FEATURES:

- · Foyer: spacious entry with wood floors
- Away Room: 9'3" x 10'2" off foyer featuring large double windows. Great for piano or home office
- Dining Room: 14'6" x 10'9" features large double windows
- Great Room: 18'4" x 16'2" boasts triple windows, which create a light and airy environment for family gatherings
- Kitchen: 17' 8" x 15'8" luxury kitchen showcases a large pantry, long center island with eating bar, custom crafted cabinetry, stainless steel appliances and oak floors.
- Powder Room: houses a pedestal sink
- Mud room: from garage to kitchen with coat closet
- Master Bedroom: 15'2" x 12'9" spacious bedroom retreat with adjoining private bath.

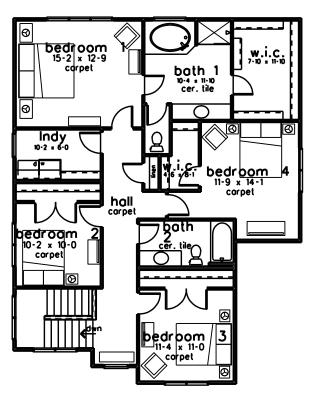
- Master Bath: includes an expansive walk-in closet, private water closet, soaking tub with a window set above. This allows bright light into the room and a separate shower.
- Bedroom 2: 10'0" x 10'2"
- Bedroom 3: 11'4" x 11'
- Bedroom 4: 14'1" x 11'9" features a walk-in closet
- Bath 2: full bath
- · Second floor laundry room
- · Full basement
- 2 car attached garage with extra storage area
- · Artist renderings and floor plans are for illustration purposes, changes or modifications may occur without notice. Please review the Construction Standards and Energy Features brochure and contract blueprint for detailed information.



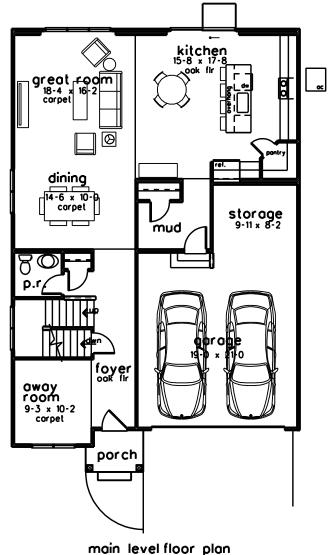


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upper level floor plan



The Larkspur



The Larkspur Premier Series—2774

The Larkspur is a spacious family home, including 9' first floor ceilings, 4 bedrooms, 2½ bathrooms, an expansive kitchen, and a great room. This home has the option to convert the first-floor study and dining room into a fifth bedroom with a full bathroom.

FEATURES:

- Foyer: covered entry leads to the open foyer, which features two sidelight windows and a 9' ceiling
- Study: 11' x 10' can easily be transformed into a fifth bedroom on the first floor, having a full private bathroom and a walk-in closet to replace the dining room. Bedroom 5 would be 11' x 15'6".
- Dining Room: 14' x 11' can be transformed into a full bathroom and closet if the study is converted into a fifth bedroom. Bedroom 5 would be 11' x 15'6".
- Great Room: 18'4" x 14'10" three windows, 9' ceiling, open to kitchen and breakfast room
- Kitchen: 15'3" x 8'4" features island with eating bar, stainless steel appliances, custom cabinets and a walk-in pantry. The kitchen is open to the great room/breakfast room. Sink is set on an angle under two corner windows.
- Breakfast Room: 12' x 14'1" open to the great room, features a sliding glass door to rear yard. This ample eating area can fit a large family table and hutch.
- Mudroom: located off of the garage entry, this room is perfect for coats and boots. Add optional coat hooks and sitting bench for a creative way to store the kids' backpacks and sports equipment.

- · Powder Room: first floor half-bath near the coat closet
- Bedroom 1: 16'6" x 15' spacious bedroom showcasing two windows with views of the backyard
- Master Bathroom: features a long vanity, shower, separate soaking tub, linen closet, and an oversized walk-in closet
- Bedroom 2: 13' x 12'7'
- Bedroom 3: 11' x 14'8" features a walk-in closet
- Bedroom 4: 12' x 15'7" features a walk-in closet
- Bathroom 2: full bathroom
- Laundry Room: second floor laundry
- Two car attached garage (shown with optional windows on garage door)
- · Full basement
- Artist renderings and floor plans are for illustration purposes, changes or modifications may occur without notice. Please review the Construction Standards and Energy Features brochure and contract blueprint for detailed information.



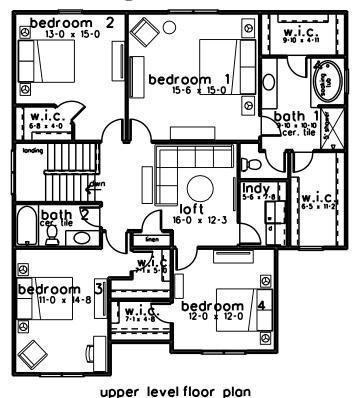


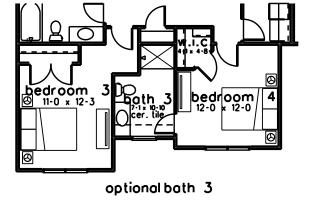
(630) 293-3000

www.AirhartConstruction.com







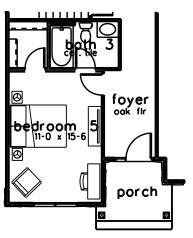


great room
17-4 x 14-10

dining room
17-4 x 11-0

study
11-0 x 10-0

main level floor plan



optional 5 bedroom/3 bath



The Maple Hill



The Maple Hill Premier Serfes—2945

The Maple Hill is a spacious family home, including 9' first floor ceilings, 4 bedrooms, 2½ bathrooms, an expansive kitchen, and a great room. This home has the option to convert the first-floor flex and powder room into a fifth bedroom with a full bathroom and walk-in closet.

FEATURES:

- Foyer: covered entry leads to the open foyer, which features two sidelight windows and a 9' ceiling
- Flex: 11' x 11' can be used as a study or living room or transformed into a fifth bedroom on the first floor, having a full private bathroom and a walk-in closet. Bedroom 5 would be 11' x 11'.
- Great Room: 18'4" x 15' three tall windows, 9' ceiling, open to the kitchen and dining room
- Kitchen: 15'2" x 15' features long island with eating bar, stainless steel appliances, custom cabinets and a spacious walk-in pantry 6' x 7'3". The kitchen is open to the great room/dining room.
- Dining Room: 14' x 10' open to the great room, features three windows and a swing door to the rear yard. This ample eating area can fit a large table and hutch.
- Mudroom: located off of the garage entry, this room is perfect for coats and boots. Add optional coat hooks, sitting bench, or countertops for a creative way to store the kids' backpacks, sports equipment, or add hobby center.
- · Powder Room: first floor half-bath near the coat closet

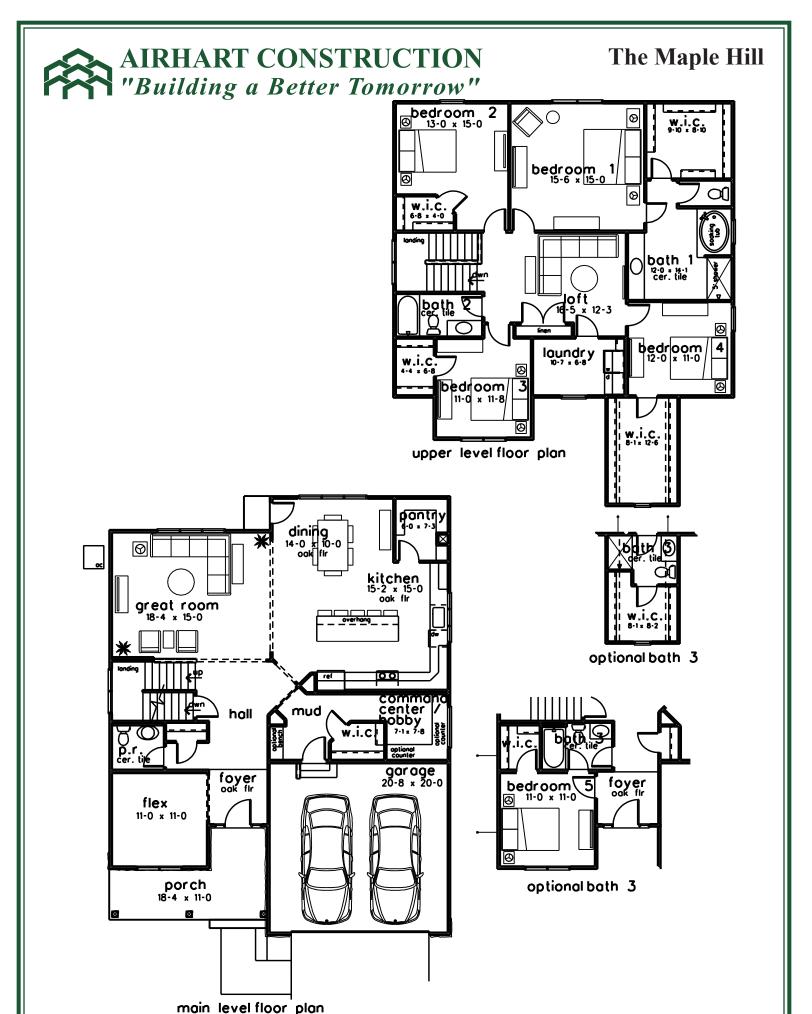
- Bedroom 1: 15'6" x 15' spacious bedroom showcasing two windows with views of the backyard
- Master Bathroom: features a long vanity, shower, separate soaking tub, linen closet, and an oversized 9'10" x 8'10" walk-in closet
- Bedroom 2: 13' x 15' features a walk-in closet
- Bedroom 3: 11' x 11'8" features a walk-in closet
- Bedroom 4: 12' x 11' features a walk-in closet and an optional private bath
- Bathroom 2: full bathroom
- Loft: 16'5" x 12'3" makes a great study or TV area for the family
- · Laundry Room: second floor laundry
- Two car attached garage (shown with optional windows on garage door)
- Full basement
- Artist renderings and floor plans are for illustration purposes, changes or modifications may occur without notice. Please review the Construction Standards and Energy Features brochure and contract blueprint for detailed information.





(630) 293-3000

www.AirhartConstruction.com



CITY OF ST. CHARLES

TWO EAST MAIN STREET ST. CHARLES, ILLINOIS 60174-1984



COMMUNITY DEVELOPMENT DIVISION

PHONE: (630) 377-4443 EMAIL: cd@stcharlesil.gov

ZONING MAP AMENDMENT APPLICATION

CITYVIEW

Project Name:

Minhall Glen

Project Number:

2020 -PR- 004

Cityview Project Number: PLMA 2020 00641

St. Charles. IL

AUG 1 1 2020

Planning-Division-

Instructions:

To request a zoning map amendment (rezoning) for a property, complete this application and submit it with all required attachments to the Planning Division.

City staff will review submittals for completeness and for compliance with applicable requirements prior to establishing a Plan Commission public hearing or meeting date.

The information you provide must be complete and accurate. If you have a question please call the Planning Division and we will be happy to assist you.

1.	Property Information: Location: Munhall Ave/Tyler Road, St. Charles				
***************************************		Parcel Number (s): 5 parcels: 09-26-376-003, 09-26-376-001, 09-26-376-005, 09-35-126-010 & 09-26-377-004,			
		Proposed PUD Name:			
		Munhall Glen			
2.	Applicant Information:	Name Airhart Construction Corp Court Airhart President Address	Phone 630-293-3000 ext. 145 Fax 630-293-3021 Email court@airhartconstruction.com		
3.	Record Owner	Name D. Four	Phone 630-879-3680		
	Information:	Address	Fax		
-		140 First Street			
		Batavia, IL 60510	Email austin@bataviaenterprises.com		

Zoning and Use Information:

Comprehensive Plan Land Use Designation of the property:	Single Family Detached Residential (eastern
Current zoning of the property: M-2 Limited Manufactu	parcels) and Industrial/Business Park (rear parcel ring
Is the property a designated Landmark or in a Historic Distri	ct? No
Current use of the property: Residential and farming	
Proposed zoning of the property: RS-4 under a PUD	
Proposed use of the property: Residential	
If the proposed Map Amendment is approved, what improve	ments or construction are planned? (An accurate site

Full development of the site for 50 residential lots under RS-4 PUD zoning with earth moving, tree removal, road construction, detention, sewer, water, gas, electric, communication and landscape installation ready for home construction.

plan may be required to establish that the proposed improvement can meet the minimum zoning requirements)

Attachment Checklist:

If multiple zoning or subdivision applications are being submitted concurrently, do not submit duplicate checklist items or plans. Fee must be paid for each application.

□ APPLICATION FEE:

Application fee in accordance with Appendix B of the Zoning Ordinance. (\$500)

□ REIMBURSEMENT OF FEES AGREEMENT:

An original, executed Reimbursement of Fees Agreement and deposit of funds in escrow with the City, as provided by Appendix B of the Zoning Ordinance.

□ REIMBURSEMENT OF FEES INITIAL DEPOSIT:

Deposit of funds in escrow with the City. Required deposit is based on review items (number of applications filed) and the size of the site:

Number of	I Indon & Assoc	5-15 Acres	1.6 75 A aura	Over 75 Acres
Review Items	Under 5 Acres	3-13 Acres	16-75 Acres	Over 75 Acres
1	\$1,000	\$2,000	\$3,000	\$4,000
2 or 3	\$2,000	\$4,000	\$5,000	\$7,000
4 or more	\$3,000	\$5,000	\$7,000	\$10,000

□ PROOF OF OWNERSHIP and DISCLOSURE:

- a) A current title policy report; or
- b) A deed and a current title search.

If the owner is not the applicant, an original letter of authorization from the owner permitting the applicant to act on his/her behalf is required. If the owner or applicant is a Trust, a disclosure of all beneficiaries; if the owner or applicant is a Partnership, a disclosure of all partners; if the owner or applicant is a Corporation, a disclosure of all owners with an interest of at least ten percent (10%).

NOTE: Private covenants and deed restrictions can limit private property rights with respect to the use of land even though the City's Zoning Ordinance may authorize the use or a less restrictive use. We strongly advise that you perform a title search on the property to determine if there any private covenants containing use restrictions or other deed restrictions. As those private covenants and deed restrictions may conflict with the City's Zoning Ordinance, it is further recommended that you consult with an attorney to obtain an opinion with respect to whether your intended use is compatible with those restrictions.

□ LEGAL DESCRIPTION: For entire subject property, on 8 ½ x 11 inch paper

□ PLAT OF SURVEY:

A current plat of survey for the Subject Realty showing all existing improvements on the property, prepared by a registered Illinois Professional Land Surveyor.

□ SITE PLAN:

Simple site plan drawn to scale to demonstrate that the property can meet the requirements of the proposed zoning district (parking requirements, setbacks, landscaping, etc.)

☐ FINDINGS OF FACT:

Fill out the attached form or submit responses on a separate sheet.

■ LIST OF PROPERTY OWNERS WITHIN 250 FT.

Fill out the attached form or submit on a separate sheet. The form or the list must be signed and notarized.

□ SOIL AND WATER CONSERVATION DISTRICT APPLICATION:

Copy of completed Land Use Opinion application as required by state law, as submitted to The Kane-Dupage Soil and Water Conservation District. http://www.kanedupageswcd.org/

<u>Submit the application form and fee directly to the Kane-DuPage Soil and Water Conservation District.</u> Provide a copy with this application.

□ ENDANGERED SPECIES REPORT:

Copy of Endangered Species Consultation Agency Action to be filed with the Illinois Department of Natural Resources. http://dnrecocat.state.il.us/ecopublic/

Fill out the online form, print the report and submit with this application.

I (we) certify that this application and the documents submitted with it are true and correct to the best of my (our) knowledge and belief.

Record Owner

Applicant or Authorized Agent

Date

Date

Legal Description

PARCEL 1:

THAT PART OF THE SOUTHWEST QUARTER OF SECTION 26, TOWNSHIP 40 NORTH, RANGE 8, EAST OF THE THIRD PRINCIPAL MERIDIAN, DESCRIBED AS FOLLOWS: COMMENCING AT THE SOUTHEAST CORNER OF SAID SOUTHWEST QUARTER; THENCE SOUTH 88 DEGREES 50 MINUTES 28 SECONDS WEST ALONG THE SOUTH LINE OF SAID SOUTH WEST QUARTER, 215.25 FEET TO THE CENTER LINE OF TYLER ROAD; THENCE NORTH 08 DEGREES 41 MINUTES 50 SECONDS EAST ALONG SAID CENTER LINE, 73.7 FEET FOR A POINT OF BEGINNING; THENCE SOUTH 89 DEGREES 23 MINUTES 00 SECONDS WEST 222.0 FEET; THENCE NORTH 08 DEGREES 41 MINUTES 50 SECONDS EAST PARALLEL WITH THE CENTER LINE OF SAID TYLER ROAD, 132.77 FEET; THENCE NORTH 85 DEGREES 33 MINUTES 00 SECONDS EAST 224.97 FEET TO THE CENTER LINE OF SAID TYLER ROAD; THENCE SOUTH 08 DEGREES 41 MINUTES 50 SECONDS WEST ALONG SAID CENTER LINE, 148.01 FEET TO THE POINT OF BEGINNING, IN THE CITY OF SAINT CHARLES, KANE COUNTY, ILLINOIS.

PARCEL 2:

THAT PART OF THE SOUTHWEST QUARTER OF SECTION 26, TOWNSHIP 40 NORTH, RANGE 8 EAST OF THE THIRD PRINCIPAL MERIDIAN, DESCRIBED AS FOLLOWS: COMMENCING ON THE SOUTH LINE OF THE RIGHT OF WAY OF THE CHICAGO AND GREAT WESTERN RAILROAD COMPANY AT A POINT 615.9 FEET WESTERLY FROM THE CENTER LINE OF A NORTH AND SOUTH ROAD IN SAID SOUTHWEST QUARTER, KNOWN AS TYLER'S ROAD, MEASURED ALONG THE SOUTHERLY LINE OF SAID RAILROAD RIGHT OF WAY; THENCE SOUTH 11 DEGREES 16 MINUTES EAST 895.5 FEET TO THE SOUTH LINE OF SAID SOUTHWEST QUARTER FOR A POINT OF BEGINNING; THENCE NORTH 11 DEGREES 16 MINUTES WEST 895.5 FEET TO THE SOUTH LINE OF SAID RAILROAD RIGHT OF WAY; THENCE WESTERLY ALONG THE SOUTHERLY LINE OF SAID RIGHT OF WAY 482 FEET; THENCE SOUTHERLY 914.3 FEET TO A POINT ON THE SOUTH LINE OF SAID SOUTHWEST QUARTER 1059.9 FEET WEST OF THE CENTER LINE OF SAID TYLER ROAD; THENCE EAST ALONG SAID SOUTH LINE 669 FEET TO THE POINT OF BEGINNING, IN THE CITY OF SAINT CHARLES, KANE COUNTY, ILLINOIS.

PARCEL 3:

THAT PART OF THE SOUTHWEST QUARTER OF SECTION 26, TOWNSHIP 40 NORTH, RANGE 8, EAST OF THE THIRD PRINCIPAL MERIDIAN DESCRIBED AS FOLLOWS: COMMENCING AT THE SOUTHEAST CORNER OF SAID QUARTER; THENCE SOUTH 88 DEGREES 50 MINUTES 28 SECONDS WEST ALONG THE SOUTH LINE OF SAID QUARTER 215.25 FEET TO THE ORIGINAL CENTER LINE OF MUNHALL AVENUE (FORMERLY TYLER ROAD); THENCE NORTH 8 DEGREES 41 MINUTES 50 SECONDS EAST ALONG SAID ORIGINAL CENTER LINE 221.71 FEET FOR A POINT OF BEGINNING; THENCE SOUTH 8 DEGREES 41 MINUTES 50 SECONDS WEST ALONG SAID ORIGINAL CENTER LINE 95.20 FEET; THENCE NORTHEASTERLY ALONG A CURVE TO THE RIGHT HAVING A RADIUS OF 233.0 FEET TANGENT TO A LINE DRAWN NORTH 39 DEGREES 33 MINUTES 44 SECONDS EAST FROM THE LAST DESCRIBED POINT 89.15 FEET TO A LINE

DRAWN CONCENTRIC WITH AND 40.0 FEET SOUTHWESTERLY OF THE PRESENT CENTER LINE OF TYLER ROAD; THENCE NORTHWESTERLY ALONG SAID CONCENTRIC LINE, BEING A CURVE TO THE RIGHT HAVE A RADIUS OF 1081.0 FEET; 43.34 FEET TO A LINE DRAWN NORTH 85 DEGREES 33 MINUTES 0 SECONDS EAST FROM THE POINT OF BEGINNING; THENCE SOUTH 85 DEGREES 33 MINUTES 0 SECONDS WEST 39.68 FEET TO THE POINT OF BEGINNING, IN THE CITY OF SAINT CHARLES, KANE COUNTY, ILLINOIS.

PARCEL 4:

THAT PART OF THE SOUTHWEST QUARTER OF SECTION 26 AND PART OF THE NORTHWEST QUARTER OF SECTION 35, TOWNSHIP 40 NORTH, RANGE 8, EAST OF THE THIRD PRINCIPAL MERIDIAN, DESCRIBED AS FOLLOWS: COMMENCING AT THE SOUTHEAST CORNER OF SAID SOUTHWEST QUARTER; THENCE SOUTH 88 DEGREES 50 MINUTES 28 SECONDS WEST ALONG THE SOUTH LINE OF SAID SOUTHWEST QUARTER, 215.25 FEET TO THE CENTER LINE OF TYLER ROAD FOR THE POINT OF BEGINNING; THENCE NORTH 08 DEGREES 41 MINUTES 50 SECONDS EAST ALONG SAID CENTER LINE, 73.7 FEET; THENCE SOUTH 89 DEGREES 23 MINUTES 00 SECONDS WEST 222.0 FEET; THENCE NORTH 08 DEGREES 41 MINUTES 50 SECONDS EAST PARALLEL WITH THE CENTER LINE OF SAID TYLER ROAD, 132.77 FEET; THENCE SOUTH 85 DEGREES 33 MINUTES 00 SECONDS WEST 237.82 FEET; THENCE SOUTH 10 DEGREES 39 MINUTES 00 SECONDS EAST 194.53 FEET TO THE NORTH EAST CORNER OF PHASE NO. 1 CAMBRIDGE, SAINT CHARLES, KANE COUNTY, ILLINOIS; THENCE SOUTH 17 DEGREES 28 MINUTES 03 SECONDS EAST ALONG A NORTHEASTERLY LINE OF SAID PHASE NO. 1, 253.08 FEET; THENCE NORTH 72 DEGREES 44 MINUTES 11 SECONDS EAST 305.44 FEET TO THE CENTER LINE OF SAID TYLER ROAD; THENCE NORTHERLY ALONG SAID CENTER LINE 160.54 FEET TO THE POINT OF BEGINNING IN THE CITY OF SAINT CHARLES, KANE COUNTY, ILLINOIS.

PARCEL 5:

THAT PART OF THE SOUTHWEST QUARTER OF SECTION 26, TOWNSHIP 40 NORTH, RANGE 8, EAST OF THE THIRD PRINCIPAL MERIDIAN, DESCRIBED AS FOLLOWS: COMMENCING AT THE SOUTHEAST CORNER OF SAID QUARTER; THENCE SOUTH 88 DEGREES 50 MINUTES 28 SECONDS WEST ALONG THE SOUTH LINE OF SAID QUARTER 215.25 FEET TO THE ORIGINAL CENTER LINE OF MUNHALL AVENUE (FORMERLY TYLER ROAD); THENCE NORTH 8 DEGREES 41 MINUTES 50 SECONDS EAST ALONG SAID ORIGINAL CENTER LINE 221.71 FEET FOR A POINT OF BEGINNING; THENCE SOUTH 08 DEGREES 41 MINUTES 50 SECONDS WEST ALONG SAID ORIGINAL CENTER LINE 95.20 FEET; THENCE SOUTHWESTERLY ALONG A CURVE TO THE LEFT HAVING A RADIUS OF 233.00 FEET TANGENT TO A LINE DRAWN SOUTH 39 DEGREES 33 MINUTES 44 SECONDS WEST FROM THE LAST DESCRIBED POINT 52.64 FEET; THENCE NORTH 08 DEGREES 48 MINUTES 33 SECONDS EAST 138.04 FEET TO A LINE DRAWN SOUTH 85 DEGREES 33 MINUTES 00 SECONDS WEST FROM THE POINT OF BEGINNING; THENCE NORTH 85 DEGREES 33 MINUTES 00 SECONDS EAST 22.0 FEET TO THE POINT OF BEGINNING, IN THE CITY OF SAINT CHARLES, KANE COUNTY, ILLINOIS.

FINDINGS OF FACT - MAPAMENDMENT

Munhall Glen

The St. Charles Zoning Ordinance requires the Plan Commission to consider factors listed below in making a recommendation to the City Council.



8/10/2020

As an applicant, the "burden of proof" is on you to show why the proposed zoning is more appropriate than the existing zoning. Therefore, you need to "make your case" by explaining how the following factors support your proposal. If a factor does not apply to the property in question, indicate "not applicable" and explain why it does not apply.

Pro	oject Name or Address Date
In a	om the Charles Zoning Ordinance, Section 17.04.320.D: making its recommendation to grant or deny an application for a Zoning Map Amendment, including mages to Zoning District and Overlay boundaries, the Plan Commission shall consider:
1.	The existing uses and zoning of nearby property. (Relate the proposed land use and zoning to the land use and zoning of other properties in the area) The property is currently zoned a mix of RS-4 and M-2. The property to the south is zoned RS-4 and so this property melds well to the existing residential. The property to the east is zoned M-2 and used as office space. The building have a residential feel and will work well with this property. The properties to the north and west are M-2 and St. Charles owned properties. The zoning change to this property will be a positive for this area and act well as a transition between the residential to the south and more commercial uses to the north.
2.	The extent to which property values are diminished by the existing zoning restrictions. (Compare the value of the subject property and nearby properties under the current zoning to their potential value under the proposed zoning.) The highest and best use of this property is residential under the RS-4 classification with a PUD. This property will fill a niche of unmet need of first floor master bedroom housing and is a much better use than the current M-2 zoning. The M-2 Zoning in this location is unneeded due to the properties to the east of this
3.	property toward the DuPage Airport and the route 64 corridor. The fact that this property has not been developed under M-2 and left as a field while every property around it has been developed is a testament to the fact. This property has been underperforming on the tax rolls as farm land and one residential property. The change in zoning will be a great benefit to the tax rolls and improve its value to the City of St. Charles. The extent to which the reduction of the property's value under the existing zoning restrictions
	promotes the health, safety, morals or general welfare of the public. (If the existing zoning decreases the value of the subject realty, does it also produce any perceptible public benefits? This is not applicable. The current zoning has no benefit to the health, safety, morals or general welfare of the public. In fact, the change in zoning will be a great benefit to the health, safety, morals and general welfare of the public by meeting housing needs and creating a much more orderly procession of development.

4.	The suitability of the property for the purposes for which it is presently zoned, i.e. the feasibility of developing the property for one or more of the uses permitted under the existing zoning classification. (Can the subject property reasonably be used for any of the uses currently permitted? Physical and market conditions may be considered.)		
	The property is not suitable for the purpose for which it is presently zoned. The traffic patterns into residential		
	area make commercial traffic an issue as well as the location being into a residential neighborhood. The fact		
	that it has sat for so long underutilized while every property around it many, many years ago had been		
	developed is testament to the fact that the value of the property is not M-2. There are many much better options		
	to the east for commercial usages and the best use of this property is rezoning to RS-4 under a PUD.		
5.	The length of time that the property has been vacant, as presently zoned, considered in the context of the land development in the area where the property is located. (If a property has been vacant longer than other similar properties in the area, it may be an indicator that the existing zoning is inappropriate.)		
	This property is the last property in this area for development. It has been many, many years since all the properties around		
	it have been developed and this property has sat underutilized. Due to the inadequacies for development as M-2 is the		
	reason it has been left behind as other properties have been developed. The change in zoning to RS-4 PUD residential usage		
	makes much more sense in this area.		
6.	The evidence, or lack of evidence, of the community's need for the uses permitted under the proposed district. (Development trends, market forces, and the Comprehensive Plan may be considered.) The rezoning of the property to RS-4 PUD will meet a substantial need of single level and first floor master bedroom homes in St. Charles. The demographics show that this is a substantial void in the market. The change in zoning allowing housing to be built on this property will allow those individuals who need first floor master bedrooms in St. Charles to stay in St. Charles rather than moving away from their community. It will create better mix of housing in St. Charles meeting the needs of more residents and creating a greater mix of housing in the community.		
7.	The consistency of the proposed amendment with the City's Comprehensive Plan.		
	The change in zoning to RS-4 PUD conforms to the purposes and intent of the Comprehensive Plan		
	by promoting development within the current boundaries of the City. It focuses development on an underutilized property thereby enhancing the tax base, utilizing surrounding infrastructure instead of needlessly extending infrastructure past undeveloped properties. It provides housing close to shopping districts and the downtown areas as well as promoting development in an area with significant road and transportation corridors promoting orderly and efficient development.		
8.	Whether the proposed amendment corrects an error or omission in the Zoning Map.		
	Not Applicable – There is not an error or omission in the Zoning Map.		

(extent to which the proposed amendment creates nonconformities. (Generally it is not ropriate to rezone a property unless it can comply with the requirements of the new zoning.)
	The rezoning of this property will not create any nonconformities.
	trend of development, if any, in the general area of the property in question. (New development, evelopment, changes in use, or other changes in the area may help to justify a change in zoning.)
	The trend in development for M-2 is to be in better transportation corridors specializing in ease of
	ommercial traffic flow for large trucks. This property does not meet that need. The need is for
	esidential in this area and this change in zoning will allow housing for those looking for single level
	iving or first floor master bedrooms. Changing to RS-4 PUD allows for housing and will be a much

Plan Commission recommendation shall be based upon the preponderance of the evidence presented and the Commission shall not be required to find each Finding of Fact in the affirmative to recommend approval of an application for Map Amendment.

CITY OF ST. CHARLES

TWO EAST MAIN STREET ST. CHARLES, ILLINOIS 60174-1984



COMMUNITY DEVELOPMENT DIVISION

PHONE: (630) 377-4443 EMAIL: cd@stcharlesil.gov

SPECIAL USE APPLICATION

(To request a Special Use or Amendment, or a Special Use for PUD or Amendment)

For City Use
Project Name: Munhall Gen
Project Number: 2020 -PR-004
Cityview Project Number: PLSU20200034

RECEIVED ate
St. Charles, IL

JUL 3 0 2020

CDD

Planning Division

To request a Special Use for a property, or to request to amend an existing Special Use Ordinance for a property, complete this application and submit it with all required attachments to the Planning Division.

City staff will review submittals for completeness and for compliance with applicable requirements prior to establishing a public hearing date for an application.

The information you provide must be complete and accurate. If you have a question please call the Planning Division and we will be happy to assist you.

1. Property Location: Munhall Ave./Tyler Road, St. Charles				
Info	rmation:	House on property has address 872 Munhall Ave		
	76-003, 09-26-376-005			
		Proposed Name: Munhall Glen		
2. App	olicant ormation:	Name Airhart Construction Corp - Court Airhart President	Phone 630-293-3000 ext. 145	
		Address 500 E. Roosevelt Road	Fax 630-293-3021	
		West Chicago, IL 60185	Email court@airhartconstruction.com	
3. Reco		Name D. Four LLC	Phone 630-879-3680	
Info	ormation:	Address 140 First Street	Fax	
		Batavia, IL 60510	Email austin@bataviaenterprises.co	

<u>Please</u>	check the type of application:	
X	Special Use for Planned Unit Development - PUD Name: New PUD Amendment to existing PUD- Ordinance #: PUD Preliminary Plan filed concurrently	Munhall Glen
	Other Special Use (from list in the Zoning Ordinance): Newly established Special Use Amendment to an existing Special Use Ordinance #:	
<u>Inforn</u>	nation Regarding Special Use:	
	If the proposed Special Use is approved, what improvements of 51 new single family homes along with construction sanitary sewer, water, and storm sewer	NO I and farming r construction are planned?
For S	Why is the proposed change necessary?	
	What are the proposed amendments? (Attach proposed language	ge if necessary)

Note for existing buildings:

If your project involves using an existing building, whether you plan to alter it or not, please contact the St. Charles Fire Department (630-377-4458) and the Building and Code Enforcement Division (630-377-4406) for information on building, life safety and other code requirements. Depending on the proposed use, size of structure and type of construction, these requirements can result in substantial costs.

Attachment Checklist:

If multiple zoning or subdivision applications will be submitted concurrently, do not submit duplicate checklist items or plans. Fee must be paid for each application.

△ APPLICATION FEE:

Application fee in accordance with Appendix B of the Zoning Ordinance. (Special Use for PUD \$1,000; all other Special Use requests \$750)

REIMBURSEMENT OF FEES AGREEMENT:

An original, executed Reimbursement of Fees Agreement and deposit of funds in escrow with the City, as provided by Appendix B of the Zoning Ordinance.

REIMBURSEMENT OF FEES INITIAL DEPOSIT:

Deposit of funds in escrow with the City. Required deposit is based on review items (number of applications filed) and the size of the site:

Number of Review Items	Under 5 Acres	5-15 Acres	16-75 Acres	Over 75 Acres
1	\$1,000	\$2,000	\$3,000	\$4,000
2 or 3	\$2,000	\$4,000	\$5,000	\$7,000
4 or more	\$3,000	\$5,000	\$7,000	\$10,000

PROOF OF OWNERSHIP and DISCLOSURE:

- a) A current title policy report; or
- b) A deed and a current title search.

If the owner is not the applicant, an original letter of authorization from the owner permitting the applicant to act on his/her behalf is required. If the owner or applicant is a Trust, a disclosure of all beneficiaries; if the owner or applicant is a Partnership, a disclosure of all partners; if the owner or applicant is a Corporation, a disclosure of all owners with an interest of at least ten percent (10%).

NOTE: Private covenants and deed restrictions can limit private property rights with respect to the use of land even though the City's Zoning Ordinance may authorize the use or a less restrictive use. We strongly advise that you perform a title search on the property to determine if there any private covenants containing use restrictions or other deed restrictions. As those private covenants and deed restrictions may conflict with the City's Zoning Ordinance, it is further recommended that you consult with an attorney to obtain an opinion with respect to whether your intended use is compatible with those restrictions.

LEGAL DESCRIPTION: For entire subject property, on 8 1/2 x 11 inch paper

2 PLAT OF SURVEY:

A current plat of survey for the Subject Realty showing all existing improvements on the property, prepared by a registered Illinois Professional Land Surveyor.

FINDINGS OF FACT:

Fill out the attached forms or submit responses on a separate sheet (Submit "Criteria for PUD" for any PUD application; "Findings for Special Use" for all other Special Use applications.)

LIST OF PROPERTY OWNERS WITHIN 250 FT.:

Fill out the attached form or submit on a separate sheet. The form or the list must be signed and notarized.

2/SOIL AND WATER CONSERVATION DISTRICT APPLICATION:

Copy of completed Land Use Opinion application as required by state law, as submitted to The Kane-Dupage Soil and Water Conservation District. http://www.kanedupageswcd.org/

<u>Submit the application form and fee directly to the Kane-DuPage Soil and Water Conservation District.</u> Provide a copy with this application.

/ ENDANGERED SPECIES REPORT:

Copy of Endangered Species Consultation Agency Action to be filed with the Illinois Department of Natural Resources. http://dnr.illinois.gov/EcoPublic/

Fill out the online form, print the report and submit with this application.

TRAFFIC STUDY: If requested by the Director of Community Development.

Staff will advise you whether a traffic study is recommended based on the project. Regardless, the Plan Commission or City Council may request a traffic study as a part of the review process.

d PLANS:

All required plans shall be drawn on sheets no larger than 24" x 36", unless the Director of Community Development permits a larger size when necessary to show a more comprehensive view of the project. All required plans shall show north arrow and scale, and shall be drawn at the same scale (except that a different scale may be used to show details or specific features). All plans shall include the name of the project, developer or owner of site, person or firm preparing the plan, and the date of plan preparation and all revisions.

Copies of Plans:

Initial Submittal - Ten (10) full size copies, Three (3) 11" by 17", and a PDF electronic file (On a CD-ROM or may be emailed to the Project Manager). For subsequent submittals, please contact the Project Manager to determine how many copies are required.

SITE PLAN (Note: For a Special Use for PUD, submit PUD Preliminary Plan Application in lieu of Site Plan)

A plan or plans showing the following information:

- 1. Accurate boundary lines with dimensions
- 2. Streets on and adjacent to the tract: Name and right-of-way width
- 3. Location, size, shape, height, and use of existing and proposed structures
- 4. Location and description of streets, sidewalks, and fences
- 5. Surrounding land uses
- 6. Date, north point, and scale
- 7. Ground elevation contour lines
- 8. Building/use setback lines
- 9. Location of any significant natural features
- 10. Location of any 100-year recurrence interval floodplain and floodway boundaries
- 11. Location and classification of wetland areas as delineated in the National Wetlands Inventory
- 12. Existing zoning classification of property
- 13. Existing and proposed land use
- 14. Area of property in square feet and acres
- 15. Proposed off-street parking and loading areas
- 16. Number of parking spaces provided, and number required by ordinance
- 17. Angle of parking spaces
- 18. Parking space dimensions and aisle widths
- 19. Driveway radii at the street curb line
- 20. Width of driveways at sidewalk and street curb line

- 21. Provision of handicapped parking spaces
- 22. Dimensions of handicapped parking spaces
- 23. Depressed ramps available to handicapped parking spaces
- 24. Location, dimensions and elevations of freestanding signs
- 25. Location and elevations of trash enclosures
- 26. Provision for required screening, if applicable
- 27. Exterior lighting plans showing:
 - a. Location, height, intensity and fixture type of all proposed exterior lighting
 - b. Photometric information pertaining to locations of proposed lighting fixtures

I (we) certify that this application and the documents s	submitted with it are true and correct to the best of my (our)
knowledge and belief.	07/14/2020
Record Owner	Date
4	7/14/2020
Applicant or Authorized Agent	Date

July 10th, 2020

Russell Colby
Assistant Director
Community & Economic Development
City of St. Charles
2 E. Main Street
St. Charles, IL 60174

Re: Special Use Application – Munhall Glen

Dear Russell,

I am the managing member of DKIDS, LLC, now known as D. Four, LLC. We own the property listed below. I give my permission to Court Airhart, Airhart Construction Corp. and members of their team to act on our behalf and file the documents, speak at hearings, or whatever is needed to move the development of this property forward. The property is a combination of 5 properties with PINS:

PIN #:

09-26-376-001 - 11.96 acres

09-26-376-003 – 0.66 acres 09-26-376-004 – 0.12 acres 09-26-376-005 – 1.25 acres 09-35-126-010 – 1.47 acres

Please contact me if you have any further questions at (630) 879-3680.

Sincerely,

Austin Dempsey

Manager- D. Four, LLC

OWNERSHIP DISCLOSURE FORM LIMITED LIABILITY COMPANY (L.L.C.)

STATE OF ILLINOIS) SS.		
KANE COUNTY)		
I, AUSTIN M. DEMPSEY, being first duly s	sworn on oath depose and say that I am	
1	, an Illinois Limited Liability	
Company (L.L.C.), and that the following persons are		
Austin DEMPSEY	BRIAN DEMPSEY Ashley Hicks	
Austin Dempsey BRENT Dempsey	Ashley Hicks	
By: Manager		
Subscribed and Sworn before me this 2nd	day of	
<u>april</u> , 20 <u>20</u> .	KAY M LEFEVE OFFICIAL SEA Notary Public, State of	R L Illinois
han M Lebever	My Commission Ex September 28, 20	pires
Notary Jublic		

CRITERIA FOR PLANNED UNIT DEVELOPMENTS (PUDS)

For Special Use for PUD or PUD Amendment applications.

The St. Charles Zoning Ordinance requires the Plan Commission to consider the criteria listed below in making a recommendation to the City Council on whether a proposed Planned Unit Development is in the public interest.



As the applicant, the "burden of proof" is on you to provide information that addresses the criteria below in order to demonstrate that the project is in the public interest.

(You may utilize this form or provide the responses on another sheet.)

Munhall Glen	6/24/2020
PUD Name	Date

From the St. Charles Zoning Ordinance, Section 17.04.410.3:

The Plan Commission shall not favorably recommend, and the City Council shall not approve, a Special Use for a PUD or an amendment to a Special Use for a PUD unless they each make findings of fact based on the application and the evidence presented at the public hearing that the PUD is in the public interest, based on the following criteria:

i. The proposed PUD advances one or more of the purposes of the Planned Unit Development procedure stated in Section 17.04.400.A:

- 1. To promote a creative approach to site improvements and building design that results in a distinctive, attractive development that has a strong sense of place, yet becomes an integral part of the community.
- 2. To create places oriented to the pedestrian that promote physical activity and social interaction, including but not limited to walkable neighborhoods, usable open space and recreational facilities for the enjoyment of all.
- 3. To encourage a harmonious mix of land uses and a variety of housing types and prices.
- 4. To preserve native vegetation, topographic and geological features, and environmentally sensitive areas.
- 5. To promote the economical development and efficient use of land, utilities, street improvements, drainage facilities, structures and other facilities.
- 6. To encourage redevelopment of sites containing obsolete or inappropriate buildings or uses.
- 7. To encourage a collaborative process among developers, neighboring property owners and residents, governmental bodies and the community

The proposed PUD promotes a creative solution to an unmet growing housing need of single story and first floor master bedroom housing. This housing will allow for longtime community residents, business people and leaders in St. Charles to remain in St. Charles as their housing needs change. The PUD zoning promotes creative housing and provides attractive streetscapes that incentivizes porches and pedestrian friendly neighborhoods. It promotes social interaction by providing sidewalks, paths, neighborhood connection areas as well as connection to a potential linear park to the north of the property. The PUD provides a harmonious usage of the property by changing a potential heavy commercial use of the property to residential use more in scale with the residential use to the south and the less intense commercial use to the east. The development of this property will promote higher levels of landscaping and higher quality trees than currently exist on the property as uncontrolled Buckthorn,

Honeysuckle, Mulberry, Box Elder, etc. and other invasive landscaping is allowed to multiply. The installation of detention areas with natural landscaping will enhance water quality and native plants. Munhall Glen will be a benefit to future residents, the surrounding neighbors, local businesses and the City of St. Charles.

- ii. The proposed PUD and PUD Preliminary Plans conform to the requirements of the underlying zoning district or districts in which the PUD is located and to the applicable Design Review Standards contained in Chapter 17.06, except where:
 - A. Conforming to the requirements would inhibit creative design that serves community goals, or
 - B. Conforming to the requirements would be impractical and the proposed PUD will provide benefits that outweigh those that would have been realized by conforming to the applicable requirements.

Factors listed in Section 17.04.400.B shall be used to justify the relief from requirements:

- 1. The PUD will provide community amenities beyond those required by ordinance, such as recreational facilities, public plazas, gardens, public areas, pedestrian and transit facilities.
- 2. The PUD will preserve open space, natural beauty and critical environmental areas in excess of what is required by ordinance or other regulation.
- 3. The PUD will provide superior landscaping, buffering or screening.
- 4. The buildings within the PUD offer high quality architectural design.
- 5. The PUD provides for energy efficient building and site design.
- 6. The PUD provides for the use of innovative stormwater management techniques.
- 7. The PUD provides accessible dwelling units in numbers or with features beyond what is required by the Americans with Disabilities Act (ADA) or other applicable codes.
- 8. The PUD provides affordable dwelling units in conformance with, or in excess of, City policies and ordinances.
- 9. The PUD preserves historic buildings, sites or neighborhoods.

The proposed RS4-PUD and PUD Preliminary Plans provide a significantly more harmonious usage of the property than the current more intrusive usage and negative impact on the surrounding properties of the existing M-2 (industrial usage) zoning classification currently in place. By allowing for the PUD the property will be able to meet a significant housing need in the community of single level and first floor master bedroom housing. This housing would not be allowed in the M-2 zoning classification. By changing the zoning and allowing the PUD, storm water facilities, open space, and sidewalks and paths will allow for residents to enjoy the open space and property as well as provide for a pedestrian connection to the potential linear park to the north. The planned landscaping will be a significant improvement to the invasive species currently on the property and the trees planted as a part of the development will be a significant improvement. Due to the size and shape of the property by downzoning the property to an RS4-PUD it allows for implementation of a variety of lot sizes which provides for varied architecture, improved rear yard setbacks and a unique streetscape that incentivizes porches and neighbor interaction than the current M-2 zoning classification allows. The development will provide stormwater facilities with native species enhancing the water quality where currently no storm water facilities exist. The RS4-PUD enhance the opportunity for single level living while not infringing on the size of the homes for those requiring single level living. The PUD promotes quality residential development and provides good transitional zoning to benefit those properties that currently surround it. The development will be a benefit to the City of St. Charles housing and significant increase in tax base.

iii. The proposed PUD conforms with the standards applicable to Special Uses (section 17.04.330.C.2):

A. Public Convenience: The Special Use will serve the public convenience at the proposed location.

The Special Use will serve the public convenience at Munhall Glen by filling a housing void in the market of single story and first floor master bedroom homes. This void in the market is causing those needing this type of housing to look outside of St. Charles even though they have been long time residents. In addition, by providing housing in this location it helps support area businesses and supports good planning putting residential housing close to both public and private amenities.

B. Sufficient Infrastructure: That adequate utilities, access roads, drainage and/or necessary facilities have been, or are being, provided.

There is sufficient infrastructure and utilities in this area to support the development. There is a major sanitary sewer main on the north end of the property installed for the future development of this property. The utility infrastructure installed on this property will help with the connectivity of utilities, specifically water main, which will help "loop" the water system in the area and provide for better servicing and water circulation. The installation of storm water controls and Best Management Practices on this property will provide storm water detention where no storm water controls currently exist. The traffic pattern will provide excellent vehicular movement because Munhall Glen exits onto a Major Collector, Tyler Road, which links to Principal Arterials of E. Main Street and Kirk Road providing for safe and efficient vehicular movement.

C. Effect on Nearby Property: That the Special Use will not be injurious to the use and enjoyment of other property in the immediate vicinity for the purposes already permitted, nor substantially diminish or impair property values within the neighborhood.

Munhall Glen will not be injurious to the use and enjoyment of surrounding properties and it will act as an excellent transition from the commercial to the east and west and the residential to the south. By approving this Special Use, the downzoning of this property from M-2, Limited Manufacturing to RS4-PUD will ensure a more harmonious residential usage of the property and ensure Munhall Ave. stays primarily a residential street rather than negatively impacted by commercial heavy trucking transportation uses.

D. Effect on Development of Surrounding Property: That the establishment of the Special Use will not impede the normal and orderly development and improvement of the surrounding property for uses permitted in the district.

The establishment of the Special Use will not impede the normal and orderly development and improvement of the surrounding properties due to the fact that all surrounding properties are currently developed. As the last piece of property in this area for development the approval of the special use will in fact promote a harmonious interconnecting and buffer for the surrounding properties.

E. Effect on General Welfare: That the establishment, maintenance or operation of the Special Use will not be detrimental to or endanger the public health, safety, comfort or general welfare.

The approval of the Special Use will not be detrimental to or endanger the public health, safety, comfort or general welfare. As planned, the properties development will provide buffering between different property usages, will extend and improve municipal infrastructure, and will provide

housing	needed	in	the	city	of St	Charles.
Housing	necueu	111	uic	CILV	ບເວເ	Charles.

F. Conformance with Codes: That the proposed Special Use conforms to all applicable provisions of the St. Charles Municipal Code and meets or exceeds all applicable provisions of this Title, except as may be varied pursuant to a Special Use for Planned Unit Development.

The proposed Special Use conforms to all applicable provisions of the St. Charles Municipal Code and meets or exceeds all applicable provisions of this Title, except as varied pursuant to the Special Use for the Planned Unit Development. The Special Use and PUD zoning allows for a more inventive design, the average lot sizes are significantly larger than the minimum requirements, and the housing will be constructed at or above current codes and energy requirements. The Special Use and minor changes to the zoning requirements allows for an inventive solution for meeting a needed housing niche within the St. Charles housing stock.

iv. The proposed PUD will be beneficial to the physical development, diversity, tax base and economic well-being of the City.

The proposed PUD will be beneficial to the physical development, diversity, tax base and economic well-being of the City. The development of this property will improve connections of the water and sewer systems and provide storm water management facilities where none currently exist. It will provide a diversity of housing by providing single story and first floor master bedroom housing providing solutions for current St. Charles residents whose housing needs have changed over time and want to stay in the City due to civic, cultural, social and religious activities that they have long time connections. The PUD will substantially increase the tax base for the City, School District, Park District, etc. over the current use in perpetuity benefiting many taxing bodies. In addition, it provides housing close to many commercial districts benefiting many surrounding businesses and the economic wellbeing of the City

v. The proposed PUD conforms to the purposes and intent of the Comprehensive Plan.

The proposed PUD conforms to the purposes and intent of the Comprehensive Plan by promoting development within the current boundaries of the City. It focuses development on an underutilized property thereby enhancing the tax base, utilizing surrounding infrastructure instead of needlessly extending infrastructure past undeveloped properties. It provides housing close to shopping districts and the downtown area as well as promoting development in an area with significant road and transportation corridors promoting orderly and efficient development.

CITY OF ST. CHARLES

TWO EAST MAIN STREET ST. CHARLES, ILLINOIS 60174-1984



COMMUNITY DEVELOPMENT DIVISION

PHONE: (630) 377-4443 EMAIL: cd@stcharlesil.gov

PUD PRELIMINARY PLAN APPLICATION

Project Number:

Project Number:

Project Number:

Project Number:

PL PUD 20200036

Received Date
RECEIVED
St. Charles, IL

JUL 3 0 2020

CDD
Planning Division

To request approval of a PUD Preliminary Plan, complete this application and submit it with all required plans and attachments to the Planning Division. Normally this application will track with an application for Special Use for a PUD, unless a Special Use for a PUD has previously been granted and no amendment is necessary.

When the application is complete staff will distribute the plans to other City departments for review. When the staff has determined that the plans are ready for Plan Commission review, we will place the PUD Preliminary Plan on a Plan Commission meeting agenda.

The information you provide must be complete and accurate. If you have a question please call the Planning Division and we will be happy to assist you.

1.	Property Information:	Location: Munhall Ave./Tyler Road, St. Charles					
2.	Applicant Information:	Name Airhart Construction Corp Court Airhart President Address 500 E. Roosevelt Road West Chicago, IL 60185	Phone 630-293-3000 ext. 145 Fax 630-293-3021 Email court@airhartconstruction.com				
3.	Record Owner Information:	Name D. Four Address 140 First Street Batavia, IL 60510	Phone 630-879-3680 Fax Email austin@bataviaenterprises.com				

Please check the type of application:

X	New proposed PUD- Planned Unit Development (Special Use Application filed concurrently)						
	Existing PUD-Planned Unit Development						
	PUD Amendment Required for proposed plan (Special Use Application filed concurrently						
Subdiv	rision of land:						
	Proposed lot has already been platted and a new subdivision is not required.						
	New subdivision of property required:						
	Final Plat of Subdivision Application filed concurrently						
	Final Plat of Subdivision Application to be filed later						

Attachment Checklist:

If multiple zoning or subdivision applications are being submitted concurrently, do not submit duplicate checklist items or plans. Fee must be paid for each application.

Note: The City Staff, Plan Commission, or City Council, may request other pertinent information during the review process.

APPLICATION FEE: Application fee in accordance with Appendix B of the Zoning Ordinance. (\$500)

REIMBURSEMENT OF FEES AGREEMENT:

An original, executed Reimbursement of Fees Agreement and deposit of funds in escrow with the City, as provided by Appendix B of the Zoning Ordinance.

REIMBURSEMENT OF FEES INITIAL DEPOSIT:

Deposit of funds in escrow with the City. Required deposit is based on review items (number of applications filed) and the size of the site:

Number of Review Items	Under 5 Acres	5-15 Acres	16-75 Acres	Over 75 Acres
1	\$1,000	\$2,000	\$3,000	\$4,000
2 or 3	\$2,000	\$4,000	\$5,000	\$7,000
4 or more	\$3,000	\$5,000	\$7,000	\$10,000

PROOF OF OWNERSHIP and DISCLOSURE:

- a) a current title policy report; or
- b) a deed and a current title search.

If the owner is not the applicant, an original letter of authorization from the owner permitting the applicant to act on his/her behalf is required. If the owner or applicant is a Trust, a disclosure of all beneficiaries; if the owner or applicant is a Partnership, a disclosure of all partners; if the owner or applicant is a Corporation, a disclosure of all owners with an interest of at least ten percent (10%).

NOTE: Private covenants and deed restrictions can limit private property rights with respect to the use of land even though the City's Zoning Ordinance may authorize the use or a less restrictive use. We strongly advise that you perform a title search on the property to determine if there any private covenants containing use restrictions or other deed restrictions. As those private covenants and deed restrictions may conflict with the City's Zoning Ordinance, it is further recommended that you consult with an attorney to obtain an opinion with respect to whether your intended use is compatible with those restrictions.

LEGAL DESCRIPTION: For entire subject property, on 8 ½ x 11 inch paper

☑ PLAT OF SURVEY:

A current plat of survey for the Subject Realty showing all existing improvements on the property, prepared by a registered Illinois Professional Land Surveyor.

SOIL AND WATER CONSERVATION DISTRICT APPLICATION:

Copy of completed Land Use Opinion application as required by state law, as submitted to The Kane-Dupage Soil and Water Conservation District. http://www.kanedupageswcd.org/

Submit the application form and fee directly to the Kane-DuPage Soil and Water Conservation District. Provide a copy with this application.

Z ENDANGERED SPECIES REPORT:

Copy of Endangered Species Consultation Agency Action to be filed with the Illinois Department of Natural Resources. http://dnr.illinois.gov/EcoPublic/

Fill out the online form, print the report and submit with this application.

p PLANS:

All required plans shall be drawn on sheets no larger than 24" x 36", unless the Director of Community Development permits a larger size when necessary to show a more comprehensive view of the project. All required plans shall show north arrow and scale, and shall be drawn at the same scale (except that a different scale may be used to show details or specific features). All plans shall include the name of the project, developer or owner of site, person or firm preparing the plan, and the date of plan preparation and all revisions.

Copies of Plans:

Initial Submittal - Ten (10) full size copies for non-residential projects OR Twelve (12) full size copies for residential projects; Three (3) 11" by 17"; and a PDF electronic file (On a CD-ROM or may be emailed to the Project Manager). For subsequent submittals, please contact the Project Manager to determine how many copies are required.

☑ SITE/ENGINEERING PLAN:

PRELIMINARY ENGINNERING PLANS - DRAWING REQUIREMENTS/CHECKLIST:

Complete the attached checklist and ensure that all required information is included on the Preliminary Engineering Plans:

- 1. Accurate boundary lines with dimensions
- 2. Existing and proposed easements: location, width, purpose
- 3. Streets on and adjacent to the tract: Name and right-of-way width, center line elevation, and culverts
- 4. Location, size, shape, height, and use of existing and proposed structures
- 5. Location and description of streets, sidewalks, and fences
- 6. Surrounding land uses
- 7. Legal and common description
- 8. Date, north point, and scale
- 9. Existing and proposed topography
- 10. All parcels of land intended to be dedicated for public use or reserved for the use of all property owners with

the proposal indicated

- 11. Location of utilities
- 12. Building/use setback lines
- 13. Location of any significant natural features
- 14. Location of any 100-year recurrence interval floodplain and floodway boundaries
- 15. Location and classification of wetland areas as delineated in the National Wetlands Inventory
- 16. Existing zoning classification of property
- 17. Existing and proposed land use
- 18. Area of property in square feet and acres
- 19. Proposed off-street parking and loading areas
- 20. Number of parking spaces provided, and number required by ordinance
- 21. Angle of parking spaces
- 22. Parking space dimensions and aisle widths
- 23. Driveway radii at the street curb line
- 24. Width of driveways at sidewalk and street curb line
- 25. Provision of handicapped parking spaces
- 26. Dimensions of handicapped parking spaces
- 27. Depressed ramps available to handicapped parking spaces
- 28. Location, dimensions and elevations of freestanding signs
- 29. Location and elevations of trash enclosures
- 30. Provision for required screening, if applicable
- 31. Provision for required public sidewalks
- 32. Certification of site plan by a registered land surveyor or professional engineer
- 33. Geometric plan showing all necessary geometric data required for accurate layout of the site
- 34. Grading plans showing paving design, all storm sewers, and detention/retention facilities including detention/retention calculations) and erosion control measures
- 35. Utility plans showing all storm sewers, sanitary sewers, watermains, and appropriate appurtenant structures
- 36. Exterior lighting plans showing:
 - Location, height, intensity and fixture type of all proposed exterior lighting
 - Photometric information pertaining to locations of proposed lighting fixtures
- 37. Typical construction details and specifications
- 38. Certification of site engineering plans by a registered professional engineer
- 39. Proof of application for Stormwater Management Permit

$1//_{\Omega}$ SKETCH PLAN FOR LATER PHASES OF PUD:

For phased PUD's, where a sketch plan is permitted, it shall include, at minimum, the following:

- General location of arterial and collector streets
- Location of any required landscape buffers
- Location of proposed access to the site from public streets
- Maximum number of square feet of floor area for nonresidential development
- Maximum number of dwelling units for residential development
- Open space and storm water management land

N/4 D ARCHITECTURAL PLANS:

Architectural plans and data for all principal buildings shall be submitted in sufficient detail to permit an understanding of the exterior appearance and architectural style of the proposed buildings, the number, size and type of dwelling units, the proposed uses of nonresidential and mixed use buildings, total floor area and total building coverage of each building.

TREE PRESERVATION PLAN:

Tree Preservation Plan when required in accordance with Chapter 8.30 of the St. Charles Municipal Code. The information required for this plan may be included as part of the Landscape Plan set. See attachment, "Tree Preservation Requirements for Preliminary Plans".

LANDSCAPE PLAN:

Landscape Plan showing the following information:

- 1. Delineation of the buildings, structures, and paved surfaces situated on the site and/or contemplated to be built thereon
- 2. Delineation of all areas to be graded and limits of land disturbance, including proposed contours as shown on the Site/Engineering Plan.
- 3. Accurate property boundary lines
- 4. Accurate location of proposed structures and other improvements, including paved areas, berms, lights, retention and detention areas, and landscaping
- 5. Site area proposed to be landscaped in square feet and as a percentage of the total site area
- 6. Percent of landscaped area provided as per code requirement
- 7. Dimensions of landscape islands
- 8. Setbacks of proposed impervious surfaces from property lines, street rights-of-way, and private drives
- 9. Location and identification of all planting beds and plant materials
- 10. Planting list including species of all plants, installation size (caliper, height, or spread as appropriate) and quantity of plants by species
- 11. Landscaping of ground signs and screening of dumpsters and other equipment

STORMWATER MANAGEMENT:

Written information (reports, calculations, etc.) as described in the Stormwater Management Requirements for Preliminary Plans (attached)

SUBDIVISION PLAT DRAWING REQUIREMENTS/CHECKLIST:

If the PUD Preliminary Plan involves the subdivision of land, a completed Subdivision Plat Drawing Requirements Checklist must be submitted.

PUBLIC BENEFITS, DEPARTURES FROM CODE:

A description of how the PUD meets the purposes and requirements set out in Section 17.04.400 of the Zoning Ordinance. Any requests for departures from the requirements of Title 16, "Subdivisions and Land Improvement," and Title 17, "Zoning," shall be listed and reasons for requesting each departure shall be given.

SCHEDULE: Construction schedule indicating:

- a. Phases in which the project will be built with emphasis on area, density, use and public facilities, such as open space, to be developed with each phase. Overall design of each phase shall be shown on the plat and through supporting material.
- b. Approximate dates for beginning and completion of each phase.
- c. If different land use types are to be included within the PUD, the schedule must include the mix of uses to be built in each phase.

☑ PARK AND SCHOOL LAND/CASH WORKSHEETS

For residential developments, Park and School land/cash worksheets in accordance with Title 16 of the St. Charles Municipal Code with population projections establishing anticipated population and student yields.

INCLUSIONARY HOUSING SUMMARY

For residential developments, submit information describing how the development will comply with the requirements of Title 19, "Inclusionary Housing" of the St. Charles Municipal Code.

I (we) certify that this application and the documents submitted with it are true and correct to the best of my (our) knowledge and belief.

Record Owner

Applicant or Authorized Agent

Date

AZMAY Consympton Coar Miglzoz

MUNHALL GELN - PUBLIC BENEFITS, DEPARTURE FROM CODE

PROJECT CONCEPT

The development of this site will include 50 residential single family residences as well as open space and stormwater detention. The location of the property is excellent for residential construction. The property is located off a main collector road with great access from Tyler to the North Ave. shopping corridor and downtown St. Charles. It is excellent for commuting as well with its east side location.

Munhall Glen is designed with smaller lots and extremely livable low maintenance homes for downsizers and those looking for "right sized" homes. Due to the shape of the property we are creating a mix of lot widths and depths. This will allow us to create larger back yards in strategic locations that we believe will add to the character of the development and provides a great solution for the layout of this property.

The home designs are primarily focused on single level living with most homes having first floor master bedrooms with secondary bedrooms or loft space on the second floor. There will be some homes with second floor master bedrooms.

Open space is designed to the north of the project which connects to what will hopefully be a future linear park. This will create great access to the park system in St. Charles and beyond for walkers, runners and bikers. Additional open area will be at the entry which includes open space and a second detention basin. The neighborhood will have sidewalks and three pocket parks for social interaction designed into the neighborhood. The first will be at the entry and the other two will be overlooking the natural basin to the north. These areas allow walkers a place to rest or gather with friends.

In addition, the near town location creates easy access to the many parks and the Fox River path system which gives its residents amazing outdoor opportunities.

Architecture:

The homes include a variety of exterior styles that blend together to create a unique and interesting street scape from traditional to midcentury modern. Homes may feature a front porch to increase neighbor interaction as well as stone or brick accents mixed with siding. The garage faces will be set back from the front of the homes to lessen the impact of the garage doors.

These homes will primarily have first floor master bedrooms designed for owners looking for single level living. The interiors will focus on open concept living with dual use spaces for efficient living. These homes have smaller private yards and include patios or decks on the rear of the homes for outdoor enjoyment.

The finished living space will range from 1,300 to 3,000 square feet, 2-4 bedrooms and 2-3 ½ baths and include a two car attached garage with 2 additional parking spaces. The exteriors will be a combination of low maintenance materials including architectural grade shingles, fiber cement siding, aluminum soffits and fascia, concrete porches and options to include cultured stone or brick accents.

While the homes are not attached, they draw buyers looking for the low maintenance of attached homes, without the drawback of having attached walls.

Departures from Code

Because of our focus on single level living and first floor master bedroom homes the straight RS-4 zoning negatively impacts the pursuit of supplying that housing. In order to meet the stated goals above we are requesting a few departures from code to provide this housing. Not all are departures from code, in fact some are more restrictive, the items are listed together to be comprehensive in review.

Due to the shape of the property 2 lot sizes will be utilized to promote a variety of housing and create better opportunities for expanded rear yards. Following is minimum lot size, setbacks and lot coverage in each category.

53 foot wide lots (Premier Homes)

Minimum Lot Area:	6,307 square feet	Departure from Code
Minimum Lot Width:	53 feet	Departure from Code
Maximum Building Coverage:	37.5%	Departure from Code
Maximum Building Height:	34 feet or 2 stories	Equal to Code

(whichever is less)

Setbacks:

Front yard:

Front Porch: 14 feet More Restrictive than Code

Living space: 20 feet (Enclosed & heated) Equal to Code

Garage 25 feet More Restrictive than Code Interior Side yard: 6 feet More Restrictive than Code

Between Homes: Minimum 12 feet Departure from Code

Exterior Side Yard: 15 feet Equal to Code Rear yard: 30 feet Equal to Code

74 foot wide lots (Garden Homes)

Minimum Lot Area:7,030 square feetMore Restrictive than CodeMinimum Lot Width:74 feetMore Restrictive than CodeMaximum Building Coverage:37.5%Departure from Code

Maximum Building Height: 34 feet or 2 stories Equal to Code

(whichever is less)

Setbacks:

Front yard:

Front Porch:

14 feet

More Restrictive than Code

Living space:

20 feet (Enclosed & heated) Equal to Code

Garage

25 feet

More Restrictive than Code

Interior Side yard:

6 feet

More Restrictive than Code

Between Homes: Exterior Side Yard: Minimum 12 feet

Departure from Code Equal to Code

Rear yard:

15 feet 30 feet

Equal to Code

Munhall Glen Construction Schedule:

Upon Municipal Approval and weather permitting:

Site Development will take approximately 6 months and will include:

- 1) Site Mobilization
- 2) Tree Removal
- 3) Silt fence and BMP protections
- 4) Mass Grading
 - a. Structural grading of house sites
 - b. Structural grading of roads
 - c. Grading of storm water basins
- 5) Utility installation
 - a. Sanitary Sewer
 - b. Storm Sewer
 - c. Water main
 - d. Conduits for electric mains and structures
- 6) Road Construction
- 7) Landscape installation
 - a. Installation of storm basin landscaping
 - b. Installation of paths and pocket park amenities
 - c. Tree installation
- 8) Model Home and spec home construction will occur during Site Development, but occupancy will not be allowed until utility and emergency access for vehicles are complete.

Home Construction:

Once site development is complete home construction will begin and we estimate the buildout for the site to take approximately 48-60 months.

PARK LAND/CASH WORKSHEET

City of St. Charles, Illinois

Name of Development
Date Submitted:
Prepared by:

Munhall Glen
July 27, 2020
Court Airhart



8	50 isting residential lots wi	ithin the proposed subdivisio	n shall be granted. Deduct on	ne (1) unit per existing lot.
	Units w/ Deduction:			(a) that f is a second of
*If the propose		n existing residential dwelling	unit, a credit calculated as a	reduction of the estimated population for the
Estimated Population	Yield:			
Type of Dwelling	# Dwelling Units (DU)	Population Generation per Unit	Estimated Population	
Detached Single Famil	ly 2 Bedroom: 5 units	DU x 2.017	= 10.085	
> 3 Bedroom	44	DU x 2.899	= 127.556	
> 4 Bedroom		DU x 3.764	=	
> 5 Bedroom		DU x 3.770	=	
Attached Single Famil	У			
➤ 1 Bedroom		DU x 1.193	=	
➤ 2 Bedroom		DU x 1.990	=	
➤ 3 Bedroom	,	DU x 2.392	=	
> 4 Bedroom		DU x 3.145	=	
Apartments				300-400-00-1
> Efficiency	w holder (100 feet and 100 feet	DU x 1.294		
➤ 1 Bedroom		DU x 1.758		
➤ 2 Bedroom		DU x 1.914	=	
➤ 3 Bedroom		DU x 3.053	=	
Totals	49		137.641	
	Total Dwelling Units with deduction, if applica	able)	Estimated Total Populatio	n
Park Site Requiremen	ts:			
Estimated Total Populat	tion 137.641 x .01	10 Acres per capita = 1.376	4 Acres	
Cash in lieu of require	ements:			
Total Site Acres 1.37	764 x \$240	,500 (Fair Market Value per Ir	nproved Land) = \$ 33	1,026.61

SCHOOL LAND/CASH WORKSHEET

City of St. Charles. Illinois

Name of Development Date Submitted: Prepared by: Munhall Glen
July 27th, 2020
Court Airhart



Total Dwelling	Units:	50	

A credit for existing residential lots within the proposed subdivision shall be granted. Deduct one (1) unit per existing lot.

Total Dwelling Units w/ Deduction: 49

*If the proposed subdivision contains an existing dwelling unit, a credit is available calculated as a reduction of the estimated population for the dwelling. Please request a worksheet from the City.

Estimated Student Yield by Grades:

Type of Dwelling # of dwelling		Ele	mentary	M	<i>fiddle</i>	High		
2) P + 4) = 11 + 11 11 18	Units (DU)		des K to 5)	(Graa	les 6 to 8)	(Grade	es 9 to 12)	
Detached Single Family	2 Bedroom: 5 units	DU x .136	= .68	DU x .048	= .24	DU x .020	= .1	
> 3 Bedroom	44	DU x .369	= 16.236	DU x .173	= 7.612	DU x .184	= 8.096	
> 4 Bedroom		DU x .530	=	DU x .298	=	DU x .360	=	
> 5 Bedroom		DU x .345		DU x .248	=	DU x .300	=	
Attached Single Family								
> 1 Bedroom		DU x .000	=	DU x .000	=	DU x .000	=	
> 2 Bedroom		DU x .088	=	DU x .048	=	DU x .038	=	
> 3 Bedroom		DU x .234	=	DU x .058	=	DU x .059	=	
> 4 Bedroom		DU x .322		DU x .154	=	DU x .173	=	
Apartments	20/2000 (11 11 11 11 12 11 11 11							
> Efficiency		DU x .000	=	DU x .000	=	DU x .000	=	
> 1 Bedroom		DU x .002	_	DU x .001	=	DU x .001	=	
> 2 Bedroom		DU x .086	=	DU x .042	=	DU x .046	=	
> 3 Bedroom		DU x .234	_	DU x .123	=	DU x .118	_	

 Totals
 49
 TDU
 16.916
 TE
 7.852
 TM
 8.196
 TH

 (with deduction, if applicable)
 TH
 16.916
 TH
 TH
 16.916

School Site Requirements:

Elementary (TE) 16.916 x .025 Middle (TM) 7.852 x .0389	
Middle (TM) 7.852 x .0389	= .4229
	= .3054
High (TH) 8.196 x.072	= .5901

Total Site Acres 1.3184

Cash in lieu of requirement	n lieu of requiremen	ents
-----------------------------	----------------------	------

INCLUSIONARY HOUSING WORKSHEET

Name of Development	Munhall Glen
Date Submitted:	July 27, 2020
Prepared by:	Court Airhart



Use this worksheet to determine the affordable unit requirement for the proposed development and to propose how the development will meet the Inclusionary Housing requirements of Title 19.

Calculate the number of affordable units required:

Unit Count Range	# of Units Proposed in Development		% of Affordable Units Required		# of Affordable Units Required
1 to 15 Units		X	5%	=	
More than 15 Units	49	X	10%	=	4.9

How will the Inclusionary Housing requirement be met?

	Provid	e on-site affordable units
X	Pay a	fee in-lieu of providing affordable units (calculate fee in-lieu below)
	Provid	e a mixture of affordable units and fee in-lieu
	0	# of affordable units to be provided:
	0	Amount of fee in-lieu to be paid (calculate below).

Fee In-Lieu Payment Calculation

# of Affordable Units Required	# of Affordable Units Proposed to Pay the Fee-In- Lieu		Fee-In-Lieu Amount Per Unit		Total Fee-In-Lieu Amount
4.9		X	\$39,665.75	=	\$194,362.18



Hampton, Lenzini and Renwick, Inc.

Civil Engineering • Structural Engineering • Environmental Services • Land Surveying www.hlrengineering.com

September 4, 2020

Attn: Ellen Johnson City of St. Charles 2 E. Main Street St. Charles, IL 60174

Re:

Munhall Glen Residential Development

St. Charles, IL

Traffic Impact Study

Dear Ms. Johnson:

Per your request we reviewed the revised Traffic Study and response letter submitted by Gewalt Hamilton Associates, Inc. (GHA) on September 2, 2020 for the referenced project. We concur with the findings of the study with one minor comment below.

1. On Exhibit 6B (2027 Total Traffic), show the traffic volume at the open access of the site at South Avenue.

If you have any questions or need additional information please contact HLR at 847-697-6700.

Yours truly,

HAMPTON, LENZINI AND RENWICK, INC.

Bv:

Callie Allbright, PE, PTOE

Calle ally

Traffic Engineer

Amy McSwane, PE, PTOE

Preliminary/Traffic Engineering Manager

Memorandum

To:

Mr. Court Airhart

Airhart Construction

From: Lynn M. Means, P.E., PTOE

Senior Transportation Engineer

Date: July 10, 2020

Updated September 2, 2020

Subject: Munhall Glen Residential Development

Munhall Avenue and Tyler Road

St. Charles, Illinois



625 Forest Edge Drive, Vernon Hills, IL 60061
Tel 847.478.9700 ■ Fax 847.478.9701

www.gha-engineers.com

Part I. Introduction and Project Context

Gewalt Hamilton Associates, Inc. (GHA) has conducted a Traffic Summary for the proposed Munhall Glen residential development. The site is located on the northwest side of Munhall Avenue, west of Tyler Road, in St. Charles, Illinois. It currently contains a single-family residence and vacant land, accessed via two driveways on Munhall Avenue. As currently proposed, the project includes redeveloping the site to provide 50 age-targeted (not-restricted), single-family homes. Access to the site is proposed via one access on Munhall Avenue. Secondary, emergency access, is also planned via a connection to South Avenue. This also provides an opportunity to permit an open (full access) connection to South Avenue.

The following provides a summary of site traffic characteristics and the analysis conducted, which includes an analysis of the development's impact on the surrounding roadway network. *Exhibits* and *Appendices* referenced are in the Technical Addendum at the end of this document.

Part II. Background Information

Site Location Map and Roadway Inventory

Exhibit 1 provides a site location map. **Exhibit 2** depicts traffic operations on the roadways serving the site, including the lane geometry, traffic control (traffic signal and stop control) and posted speed limits. All roadways within the study area are under local, City of St. Charles jurisdiction except Main Street (IL Route 64) is under the jurisdiction of the Illinois Department of Transportation (IDOT). **Appendix A** provides a photo inventory of current traffic operations.

Area Land Uses

- The site currently contains a single-family residence with two driveways on Munhall Avenue.
- The site is bound by Tyler Ridge Business Park to the north and east, commercial/industrial uses (including Tanglewood Marine, Bluegrass, CrossFit) to the west and single family residential to the south.

Pedestrian Facilities

A sidewalk is generally provided along both sides of the study area roadways. There are a few areas of disconnect along the east side of 7th Avenue, primarily adjacent to vacant parcels. Pedestrian signals are provided at the Main Street intersections with 7th Avenue and Tyler Road. Crosswalks are also provided on all approaches of these signalized intersections, as well as several of the minor street approaches (see *Exhibit 2*).

Existing Traffic

Exhibit 3 summarizes the existing weekday morning and evening peak hour traffic volumes. Peak period count data along Tyler Road, Madison Avenue, Main Street and 7th Street was obtained from IDOT's Traffic Count Database System from year 2018, as well as along Main Street from year 2019. The IDOT data was supplemented with turning movement count conducted by GHA on Tuesday, August 25, 2020 from 7:00 to 9:00 AM and 4:00 to 6:00 PM at the intersections of Tyler Road at Munhall Avenue and Indiana Avenue at 13th Avenue.

The observed weekday morning and evening peak hours generally occurred from 7:00 to 8:00 AM and 5:00 to 6:00 PM, respectively. The peak hour for each individual intersection / road segment was used in the analysis to provide a conservative analysis scenario.

Exhibit 3 also provides the Annual Average Daily Traffic (AADT) along Main Street (year 2019) and Tyler Road, Madison Avenue and 7th Street (year 2018) obtained from IDOT's website: www.gettingaroundillinois.com. Summaries of the IDOT traffic counts can be found in **Appendix B** and the intersection traffic counts in **Appendix C**

- Notes: 1. The IDOT traffic volumes presented in Appendix B represent "raw", unadjusted data. These volumes are adjusted based on day of week and month of year factors, resulting in an AADT which is lower than the total.
 - 2. The Tyler Road and Munhall Avenue intersection volumes were balanced with the IDOT segment volumes to account for abnormal traffic conditions within the study area associated with school and business closures due to COVID-19.
 - 3. The 2020 Tyler Road and Munhall Avenue intersection volumes were approximately 15 to 25 percent lower than the IDOT roadway segment peak hour volumes. Accordingly, to provide a conservative analysis scenario, the 2020 peak hour volumes at the intersection of Indiana Avenue and 13th Avenue were increased by 25 percent.

Crash Analysis

Crash data was obtained from the IDOT Division of Transportation and Safety for the last five calendar years, 2014 through 2018. A summary of the crash data is provided in *Table 1* with the locations mapped on the exhibit contained in *Appendix D*. There were no reported fatalities or crashes within the five-year analysis period that involved a bicyclist or pedestrian.

As shown in Table 1, the intersection of Tyler Road at Madison Avenue and Wallace Avenue has experienced the highest number of crashes within the study area over the five-year analyses period, with an average of approximately 1.5 crashes per year. Approximately 86 percent (6 of 7) of the crashes involved property damage only and approximately 71 percent (5 of 7) were cross-movement/angle type collisions.

Complete year 2019, as well as year 2020 crash data was not available from IDOT at the time of this study.

Table 1: Crash Summary (2014 – 2018)

			Severity ^A				Crash Type ^c					Percent	
Location	No. of			ЫB									During
Location	Crashes	PD	A	В	С	F	CM	RE	НО	FO	Ped	Bike	Wet/Icy Conditions
Intersections D													
Tyler Rd & Munhall Ave	3	3	-	-	-	-	2	-	-	1	-	-	67%
Tyler Rd & Madison Ave/Wallace Ave	7	6	1	-	-	-	5	2	-	-	-	-	0%
Madison Ave & Independence Ave	3	1	-	1	1	-	1	1	-	1	-	-	0%
Madison Ave & 7th Ave	1	1	-	-	-	-	-	1	-	-	-	-	0%
7th Ave & Indiana Ave	5	5	-	-	-	-	4	-	-	1	-	-	0%
Indiana Ave & 14th Ave	1	1	-	-	-	-	-	-	-	1	-	-	0%
Segments													
Tyler Rd: Munhall Ave and Main St	3	3	-	-	-	-	-	1	-	2	-	-	0%
Madison Ave: Tyler Rd and 7th Ave	3	3	-	-	-	-	1	-	-	2	-	-	0%
7th Ave: Madison Ave and Indiana Ave	4	2	-	2	-	-	1	-	-	3	-	-	25%
Indiana Ave: 7th Ave and 14th Ave	3	3	-	-	-	-	3	-	-	-	-	-	33%
Total (2014-18)	33	28	1	3	1	0	17	5	0	11	0	0	9%

A PD = property damage only; PI = personal injury; F = fatality.

No-Build Traffic

Exhibit 4 summarizes the 2027 No-Build weekday morning and evening peak hour traffic volumes. Traffic growth in the area is a function of expected land development in the region. Future traffic volume conditions were developed for the year 2027, build-out year of the development plus five years. Based on a review of historical traffic volumes and the Chicago Metropolitan Agency for Planning (CMAP) 2050 projections (see **Appendix E**), traffic volumes within the study area are assumed to experience a compounded growth rate of approximately 0.2 to 1.4 percent per year. However, to provide a conservative analysis scenario, a 0.5 to 1.5 percent per year compounded growth rate was applied (0.5% along 7th Avenue, Indiana Avenue, 13th Avenue and Tyler Road between Main Street and Madison Avenue; 1% along Main Street; and 1.5% along Madison Avenue and Tyler Road between Madison Avenue and Kirk Road).

Part III. Project Traffic Characteristics

Proposed Development

The development consists of redeveloping the site to include 50 age-targeted (not-restricted), single-family homes. Access to the site is proposed via one access on Munhall Avenue. Secondary, emergency access, is also planned via a connection to South Avenue. This also provides an opportunity to permit an open (full access) connection to South Avenue.

Trip Generation and Directional Distribution

Trip generation rates published by the Institute of Transportation Engineers (ITE) in the 10th Edition of the Manual *Trip Generation* were used to determine the anticipated traffic from the proposed residential use (see *Appendix F*). The number of vehicle trips anticipated during the weekday morning peak hour (one hour between 7:00 and 9:00 AM) and weekday evening peak hour (one hour between 4:00 and 6:00 PM), as well as on a weekday daily (24-hour) basis is displayed in *Table 2* and includes all traffic attributed to the proposed residential development (residents, visitors, deliveries, etc.).

^B Type A (incapacitating injury); Type B (non-incapacitating injury); Type C (possible injury).

^C CM = cross movement/angle; RE = rear end; HO = head on; FO = fixed object; Ped = pedestrian.

^D Crashes within 200 feet of an intersection.

Table 2 also presents a comparison between trip generation estimates based on traditional single-family homes and senior housing rates.

Table 2: Estimated Trip Generation

•		ITE Land	AM	Peak	Hour ¹	PM	Peak I	Hour ²	Week	day Dai	ily (24-hr)
Land Use	Size	Use Code	ln	Out	Total	In	Out	Total	In	Out	Total
Single Family Homes	50 Units	210	10	30	40	33	19	52	275	275	550
Senior Adult Housing		251	8	16	24	17	11	28	153	153	306
Trip Generation Comparison ³			-2	-14	-16	-16	-8	-24	-122	-122	-244

One hour between 7:00 and 9:00 AM. One hour between 4:00 and 6:00 PM.

As shown in Table 2, it is expected that the proposed residential development will generate between 306 and 550 total trips on a typical weekday for senior housing and traditional (non, age-restricted) single family uses, respectively. During the peak hours, the development is expected to generate between 24 and 40 vehicle trips during the weekday AM and between 28 and 52 trips during the weekday PM, again, for senior housing (lower value) and traditional single-family housing (higher value).

- Notes: 1. The existing site formerly contained a single-family residence.
 - 2. The development is anticipated to be age-targeted (not restricted). As shown in Table 2, the trip generation estimates based on senior housing use (ITE LUC 251) are approximately 40 to 45 percent lower during the weekday morning and evening peak hours, respectively, as well as approximately 45 percent lower on a daily basis.
 - 3. Accordingly, the new trips as presented in Table 2 (highlighted in green and assumed in this study) provides a conservative analysis scenario.

The anticipated trip distribution of site traffic is summarized in *Table 3*. This was based on current travel patterns, the operational characteristics of the street system and site access.

Table 3: Trip Distribution

Route & Direction	Percent Route To/From Site
North Avenue	
West of 7th Avenue	20%
East of Tyler Road	35%
Tyler Road	
South of Madison Avenue	25%
7 th Avenue	
South of Madison Avenue	20%
Totals =	100%

Site and Total Traffic Assignments

Exhibit 5A illustrates the site traffic assignment for the proposed development's trips, which is based on the traffic characteristics summarized in *Tables 2 and 3* (traffic generation and trip distribution) and assigned to the area roadways, assuming a gated, emergency access at South Avenue. **Exhibit 5B** illustrates the site traffic assignment, assuming an open (full access) connection to South Avenue. The site traffic (*Exhibit 5*) and 2027 No-Build traffic (*Exhibit 4*) were combined to produce the 2027 Total traffic, which is illustrated on **Exhibits 6A and 6B**, with a gated connection to South Avenue and an open connection, respectively.

³ Senior housing (age-restricted) trip generation estimates less traditional (non-restricted), single-family homes.

Traffic Increases

As shown on *Exhibit 5*, the total (including both entering and exiting traffic) weekday AM and PM peak hour between 8 and 17 vehicle trips are expected on the roadways leading beyond the study area, or one additional vehicle every 3 to 7 minutes. *Accordingly, the amount of site-generated traffic is expected to have minimal effects on the operations of the external street network. The gated or open connection to South Avenue has minimal impact on roadway operations.*

Part IV. Traffic Evaluation

Capacity Analysis

Capacity analyses are a standard measurement that identifies how an intersection operates. They are measured in terms of Level of Service (LOS). The concept of LOS is defined as a qualitative measure describing operational conditions within a traffic stream and their perception by motorists and/or passengers. A level-of-service definition provides an index to quality of traffic flow in terms of such factors as speed, travel time, freedom to maneuver, traffic interruptions, comfort, convenience, and safety.

Six Levels of Service are defined for each type of facility. They are given letter designations from A to F, with LOS A representing the best operating conditions and LOS F the worst. LOS C is often considered acceptable for design purposes and LOS D is usually considered as providing the lower threshold of acceptable operations. Since the level of service is a function of the traffic flows placed upon it, the facility may operate at a wide range of levels of service, depending on the time of day, day of week or period of year. A description of the operating condition under each level of service, based on the analysis parameters as published in the Transportation Research Board's (TRB) Highway Capacity Manual (HCM), Sixth Edition, is provided in **Table 4**.

Table 4: Level of Service (LOS) Summary

	, , , , , , , , , , , , , , , , , , , ,	Delay (s	sec/veh)
LOS	Description	Traffic Signal	Stop Sign
Α	Describes conditions with little to no delay to motorists.	<10	< 10
В	Represents a desirable level with relatively low delay to motorists.	>10 and < 20	>10 and < 15
С	Describes conditions with average delays to motorists.	>20 and < 35	>15 and < 25
D	Describes operations where the influence of congestion becomes more		
D	noticeable. Delays are still within an acceptable range.	>35 and < 55	>25 and < 35
	Represents operating conditions with high delay values. This level is often		
E	considered within urban settings or for minor streets intersecting major		
	arterial roadways to be the limit of acceptable delay.	>55 and < 80	>35 and < 50
_	Is unacceptable to most drivers with high delay values that often occur, when		
Г	arrival flow rates exceed the capacity of the intersection.	>80	>50

Capacity analyses were performed for four scenarios:

- Existing Traffic Existing traffic,
- No-Build Traffic Future (non-site, year 2027) traffic with background growth (assumed at 0.5 to 1.5 percent compounded per year) and
- Total Traffic, Gated Access Future No-Build traffic volumes (year 2027) plus the addition of site generated traffic, with a gate at South Avenue.
- Total Traffic, Open Access Total Traffic with open site access at South Avenue.

Table 5 summarizes the intersection capacity and queue analysis results. Capacity analysis summary printouts are provided in **Appendix G**.

Table 5: Level-of-Service Summary

	5. Level-of-octvice outlinary		Movement Group By Approach													
Intersection / Timeframe		Roadway Conditions														
				astbou	nd	We	stbou	ınd	No	rthbou	nd	Southbound				
1. Tyler Rd at Munhall Ave		TWSC - EB Stops	LT	TH	RT	LT	ΤH	RT	LT	TH	RT	LT	TH	RT		
-		• LOS	>	В	<	-	-	-	>	Α	-	-	-	•		
	A Evicting (See Evhibit 2)	 Delay 	-	13.3	-	-	-	-	-	8.0	-	-	-	-		
	A.Existing (See Exhibit 3)	 95th Queue Length (ft) 	-	5	-	-	-	-	-	-	-	-	-	-		
		 Approach LOS (Delay) 		B (13.3)			-			•			-			
		• LOS	>	В	<	-	-	-	>	Α	-	-	-	-		
	D 2027 No Build (See Eyhibit 4)	• Delay	-	13.7	-	-	-	-	-	8.0	-	-	-	-		
	B. 2027 No-Build (See Exhibit 4)	 95th Queue Length (ft) 	-	5	-	-	-	-	-	-	-	-	-	-		
AM		 Approach LOS (Delay) 		B (13.7)			-			-			-			
Peak		• LOS	>	В	<	-	-	-	>	Α	-	-	-	-		
	C. 2027 Total - Gated (See Exhibit 6A)	 Delay 	-	13.6	-	-	-	-	-	8.0	-	-	-	-		
		 95th Queue Length (ft) 	-	10	-	-	-	-	-	-	-	-	-	-		
		 Approach LOS (Delay) 		B (13.6)			-			-			-			
	D. 2027 Total - Open (See Exhibit 6B)	• LOS	>	В	<	-	-	-	>	Α	-	-	-	-		
		• Delay	-	13.4	-	-	-	-	-	8.0	-	-	-	-		
		 95th Queue Length (ft) 	-	8	-	-	-	-	-	-	-	-	-	-		
		 Approach LOS (Delay) 		B (13.4)			-			•			-			
	A.Existing (See Exhibit 3)	• LOS	>	В	<	-	-	-	>	Α	-	-	-	-		
		 Delay 	-	13.7	-	-	-	-	-	8.1	-	-	-	-		
		 95th Queue Length (ft) 	-	5	-	-	-	-	-	-	-	-	-	-		
		 Approach LOS (Delay) 		B (13.7)			-			-			-			
		• LOS	>	В	<	-	-	-	>	Α	-	-	-	-		
	B. 2027 No-Build (See Exhibit 4)	 Delay 	-	14.0	-	-	-	-	-	8.1	-	-	-	-		
	B. 2027 NO-Bulla (See Exhibit 4)	 95th Queue Length (ft) 	-	5	-	-	-	-	-	-	-	-	-	-		
PM		 Approach LOS (Delay) 		B (14.0)			-			•			-			
Peak		• LOS	>	В	<	-	-	-	>	Α	-	-	-	-		
	C 2027 Total - Gated (See Evhibit 6A)	 Delay 	-	14.4	-	-	-	-	-	8.2	-	-	-	-		
	C. 2027 Total - Gated (See Exhibit 6A)	 95th Queue Length (ft) 	-	8	-	-	-	-	-	-	-	-	-	-		
		 Approach LOS (Delay) 		B (14.4)			-			-			-			
		• LOS	>	В	<	-	-	-	>	Α	-	-	-	-		
	D 2027 Total - Open (See Exhibit 6B)	 Delay 	-	14.3	-	-	-	-	-	8.2	-	-	-	-		
	D. 2027 Total - Open (See Exhibit 6B)	 95th Queue Length (ft) 	-	8	-	-	-	-	-	-	-	-	-	-		
		 Approach LOS (Delay) 		B (14.3)			-			-			-			

Table 5: Level-of-Service Summary (cont.)

Tuble 3. Level-of-service summary (cont.)				Movement Group By Approach												
	Intersection / Timeframe	Roadway Conditions	> =	= Shar	red La	ne -	= No	n Cr	itical o	or not	Allov	ved N	loveme	ent		
		-	E	astbou	ınd	We	stbou	und	Noi	rthbou	ind	Southbound				
2. Indi	ana Ave at 13th Ave (N Leg)	TWSC - NB Stops	LT	TH	RT	LT	TH	RT	LT	TH	RT	LT	TH	RT		
		• LOS	>	Α	•	-	-	-	-	-	-	>	Α	<		
	A.Existing (See Exhibit 3)	• Delay	-	7.4	-	-	-	-	-	-	-	-	9.2	-		
	A.Existing (See Exhibit 3)	 95th Queue Length (ft) 	-	3	-	-	-	-	-	-	-	-	5	-		
		Approach LOS (Delay)		-			-			-			A (9.2)			
		• LOS	>	Α	-	-	-	-	•	-	-	>	Α	<		
	D. 2027 No. Build (Con Fubility 4)	• Delay	-	7.4	-	-	-	-	-	-	-	-	9.3	-		
	B. 2027 No-Build (See Exhibit 4)	95th Queue Length (ft)	-	3	-	-	-	-	-	-	-	-	5	-		
AM		Approach LOS (Delay)		-			-			-			A (9.3)			
Peak		• LOS	>	Α	-	-	-	-	=	-	-	>	Α	<		
	C. 2027 Total - Gated (See Exhibit 6A)	• Delay	-	7.4	-	-	-	-	-	-	-	-	9.3	-		
		95th Queue Length (ft)	-	3	-	-	-	-	-	-	-	-	5	-		
		Approach LOS (Delay)		-			-			-			A (9.3)			
	D. 2027 Total - Open (See Exhibit 6B)	• LOS	>	Α	-	-	-	-	-	-	-	>	Α	<		
		• Delay	-	7.4	-	-	-	-	-	-	-	-	9.3	-		
		95th Queue Length (ft)	-	3	-	-	-	-	-	-	-	-	5	-		
		Approach LOS (Delay)		-			-			-			A (9.3)			
	A.Existing (See Exhibit 3)	• LOS	>	Α	-	-	-	-	-	-	-	>	Α	<		
		• Delay	-	7.4	-	-	-	-	-	-	-	-	9.3	-		
		95th Queue Length (ft)	-	-	-	-	-	-	-	-	-	-	8	-		
		Approach LOS (Delay)		-			-			-			A (9.3)			
		• LOS	>	Α	-	-	-	-	=	-	-	>	Α	<		
	D. 2027 No. Build (Con Fubility 4)	• Delay	-	7.4	-	-	-	-	-	-	-	-	9.3	-		
	B. 2027 No-Build (See Exhibit 4)	95th Queue Length (ft)	-	-	-	-	-	-	-	-	-	-	8	-		
PM		Approach LOS (Delay)		-			-			-			A (9.3)			
Peak		• LOS	>	Α	-	-	-	-	-	-	-	>	Α	<		
	C 2027 Total Catad (See Eyelibit CA)	 Delay 	-	7.4	-	-	-	-	-	-	-	-	9.3	-		
	C. 2027 Total - Gated (See Exhibit 6A)	95th Queue Length (ft)	-	-	-	-	-	-	-	-	-	-	8	-		
		Approach LOS (Delay)		-			-			-			A (9.3)			
		• LOS	>	Α	-	-	-	-	-	-	-	>	A	<		
	D 2027 Total Open (See Eyhibit CD)	 Delay 	-	7.4	-	-	-	-	-	-	-	-	9.3	-		
	D. 2027 Total - Open (See Exhibit 6B)	95th Queue Length (ft)	-	-	-	-	-	-	-	-	-	-	8	-		
		Approach LOS (Delay)		-			-			-			A (9.3)			

Table 5: Level-of-Service Summary (cont.)

	o. Level-or-octvice duminary (ed															
	Intersection / Timeframe	Roadway Conditions														
				astbou	ınd	We	stbou	ınd	No	orthbou	nd	Southbound				
3. Indi	ana Ave at 13th Ave (S Leg)	TWSC - NB Stops	LT	TH	RT	LT	TH	RT	LT	TH	RT	LT	TH	RT		
		• LOS	-	-	•	>	Α	-	>	Α	<	-	-	-		
	A Eviating (See Evhibit 2)	• Delay	-	-	-	-	7.3	-	-	9.1	-	-	-	-		
	A.Existing (See Exhibit 3)	 95th Queue Length (ft) 	-	-	-	-	-	-	-	3	-	-	-	-		
		 Approach LOS (Delay) 					-			A (9.1)			-			
		• LOS	-	-	-	>	Α	-	>	Α	<	-	-	-		
	D 2027 No Build (See Eyhibit 4)	• Delay	-	-	-	-	7.3	-	-	9.1	-	-	-	-		
	B. 2027 No-Build (See Exhibit 4)	95th Queue Length (ft)	-	-	-	-	-	-	-	3	-	-	-	-		
AM		Approach LOS (Delay)		-			-			A (9.1)			-			
Peak		• LOS	-	-	-	>	Α	-	>	Α	<	-	-	-		
	C. 2027 Total - Gated (See Exhibit 6A)	• Delay	-	-	-	-	7.3	-	-	9.1	-	-	-	-		
		95th Queue Length (ft)	-	-	-	-	-	-	-	3	-	-	-	-		
		Approach LOS (Delay)					-			A (9.1)						
	D. 2027 Total - Open (See Exhibit 6B)	• LOS	-	-	-	>	Α	-	>	À	<	-	-	-		
		• Delay	-	-	-	-	7.3	-	-	9.2	-	-	-	-		
		95th Queue Length (ft)	-	-	-	-	-	-	-	3	-	-	-	-		
		Approach LOS (Delay)		-			-			A (9.2)			-			
	A.Existing (See Exhibit 3)	• LOS	-	-	•	>	Α	-	>	Α	<	-	-	-		
		• Delay	-	-	-	-	7.4	-	-	9.1	-	-	-	-		
		95th Queue Length (ft)	-	-	-	-	-	-	-	3	-	-	-	-		
		Approach LOS (Delay)		-			-			A (9.1)			-			
		• LOS	-	-	-	>	Α	-	>	Α	<	-	-	-		
	D 2027 No Build (See Eyhibit 4)	• Delay	-	-	-	-	7.4	-	-	9.2	-	-	-	-		
	B. 2027 No-Build (See Exhibit 4)	95th Queue Length (ft)	-	-	-	-	-	-	-	3	-	-	-	-		
PM		 Approach LOS (Delay) 		-			-			A (9.2)			-			
Peak		• LOS	-	-	-	>	Α	-	>	Α	<	-	-	-		
	C 2027 Total Catad (San Eyhibit 6A)	• Delay	-	-	-	-	7.4	-	-	9.2	-	-	-	-		
	C. 2027 Total - Gated (See Exhibit 6A)	 95th Queue Length (ft) 	-	-	-	-	-	-	-	3	-	-	-	-		
		Approach LOS (Delay)		-			-			A (9.2)			-			
		• LOS	-	-	-	>	Α	-	>	À	<	-	-	-		
	D 2027 Total Onen (See Eyhibit CD)	 Delay 	-	-	-	-	7.4	-	-	9.2	-	-	-	-		
	D. 2027 Total - Open (See Exhibit 6B)	95th Queue Length (ft)	-	-	-	-	-	-	-	3	-	-	-	-		
		Approach LOS (Delay)		-			-			A (9.2)			-			

Table 5: Level-of-Service Summary (cont.)

	,	Movement Group By Approach													
	Intersection / Timeframe	Roadway Conditions													
			Е	astbou	nd	We	stbou	ınd	Noi	rthbou	nd	Southbound			
4. Mun	hall Ave at Site Access	TWSC - EB Stops	LT	TH	RT	LT	TH	RT	LT	TH	RT	LT	TH	RT	
		• LOS	>	Α	<	-	-	-	Α	-	-	-	-	-	
	C. 2027 Total - Gated (See Exhibit 6A)	 Delay 	-	8.7	-	-	-	-	7.3	-	-	-	-	-	
AM Peak	C. 2027 Total - Gateu (See Exhibit OA)	 95th Queue Length (ft) 	-	3	-	-	-	-	-	-	-	-	-	-	
		 Approach LOS (Delay) 		A (8.7)			-			-			-		
	D. 2027 Total - Open (See Exhibit 6B)	• LOS	>	Α	<	-	-	-	Α	-		-	-	-	
		 Delay 	-	8.7	-	-	-	-	7.2	-	-	-	-	-	
		 95th Queue Length (ft) 	-	3	-	-	-	-	-	-	-	-	-	-	
		 Approach LOS (Delay) 		A (8.7)			-			-			-		
		• LOS	>	Α	<	-	-	-	Α	-	-	-	-	-	
	C. 2027 Total - Gated (See Exhibit 6A)	 Delay 	-	9	-	-	-	-	7.4	-	-	-	-	-	
	C. 2027 Total - Gated (See Exhibit OA)	 95th Queue Length (ft) 	-	3	-	-	-	-	-	-	-	-	-	-	
PM		#REF!		A (9.0)			-			-			-		
Peak		• LOS	>	Α	<	-	-	-	Α	-	-	-	-	-	
	D. 2027 Total - Open (See Exhibit 6B)	 Delay 	-	8.9	-	-	-	-	7.3	-	-	-	-	-	
		 95th Queue Length (ft) 	-	3	-	-	-	-	-	-	-	-	-	-	
		 Approach LOS (Delay) 		A (8.9)			-			-			-		

As shown in Table 5, under both existing and future conditions (no-build and total, with and without gated access at South Avenue), all movements at the study area intersections operate at desirable Levels of Service (at LOS "B" or better) during both peak hours studied. The 95th percentile queue lengths for exiting movements, as well as left-turn entering movements are not anticipated to exceed one vehicle, which will not impact operations on or off-site. **The gated or open connection to South Avenue has minimal impact on intersection operations.**

Part V. Recommendations and Conclusions

Analyses have been conducted to determine the impact from the proposed residential development on the adjacent roadway network. Overall, the development is anticipated to have a minimal effect on the existing traffic operations of the area street network.

In addition, the following recommendations should be considered to facilitate traffic both on and off site:

- The site access driveway should provide one inbound lane and one outbound lane, operating under Stop sign control.
- Secondary, emergency access should be provided via a connection to South Avenue. This access could permit for a future, full (open) access to South Avenue.

Part VI. Technical Addendum

The following *Exhibits* and *Appendices* were previously referenced. They provide technical support for our observations, findings and recommendations discussed in the text.

Exhibits

- 1. Location Map
- 2. Existing Operations
- 3. Existing Traffic
- 4. 2027 No-Build Traffic
- 5. Site Traffic
 - a. Gated Access at South Avenue
 - b. Open Access at South Avenue
- 6. 2027 Total Traffic
 - a. Gated Access at South Avenue
 - b. Open Access at South Avenue

Appendices

- A. Photo Inventory
- B. IDOT Traffic Count Summaries
- C. Intersection Traffic Count Summaries
- D. Crash Summary Map
- E. CMAP Traffic Volume Projections
- F. ITE Trip Generation Excerpts
- G. Capacity Analysis Worksheets

Technical Addendum



Exhibits



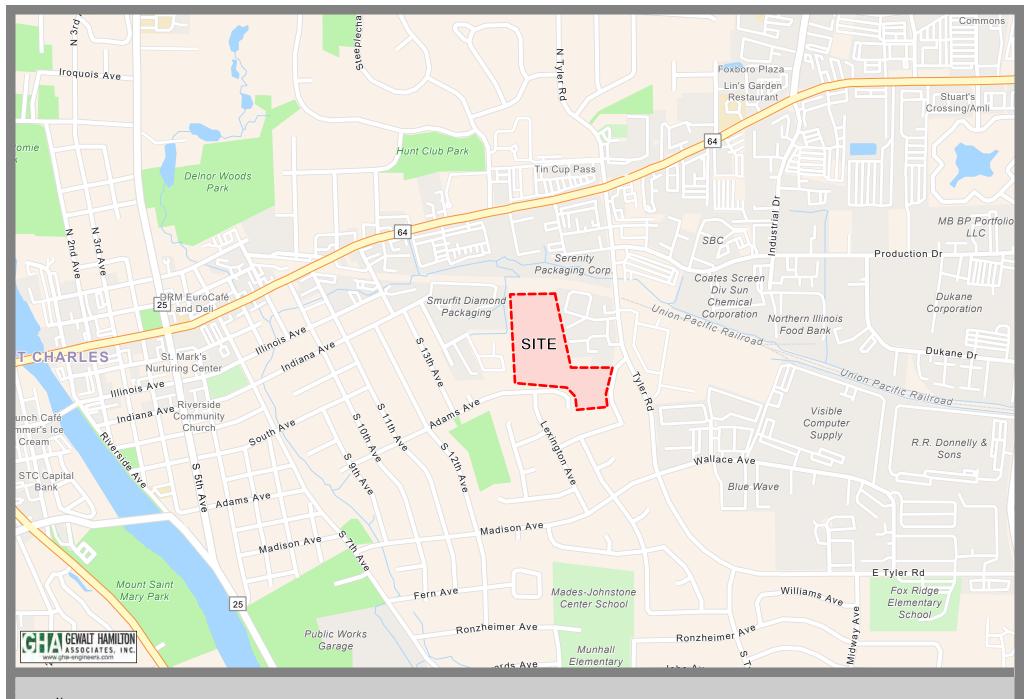
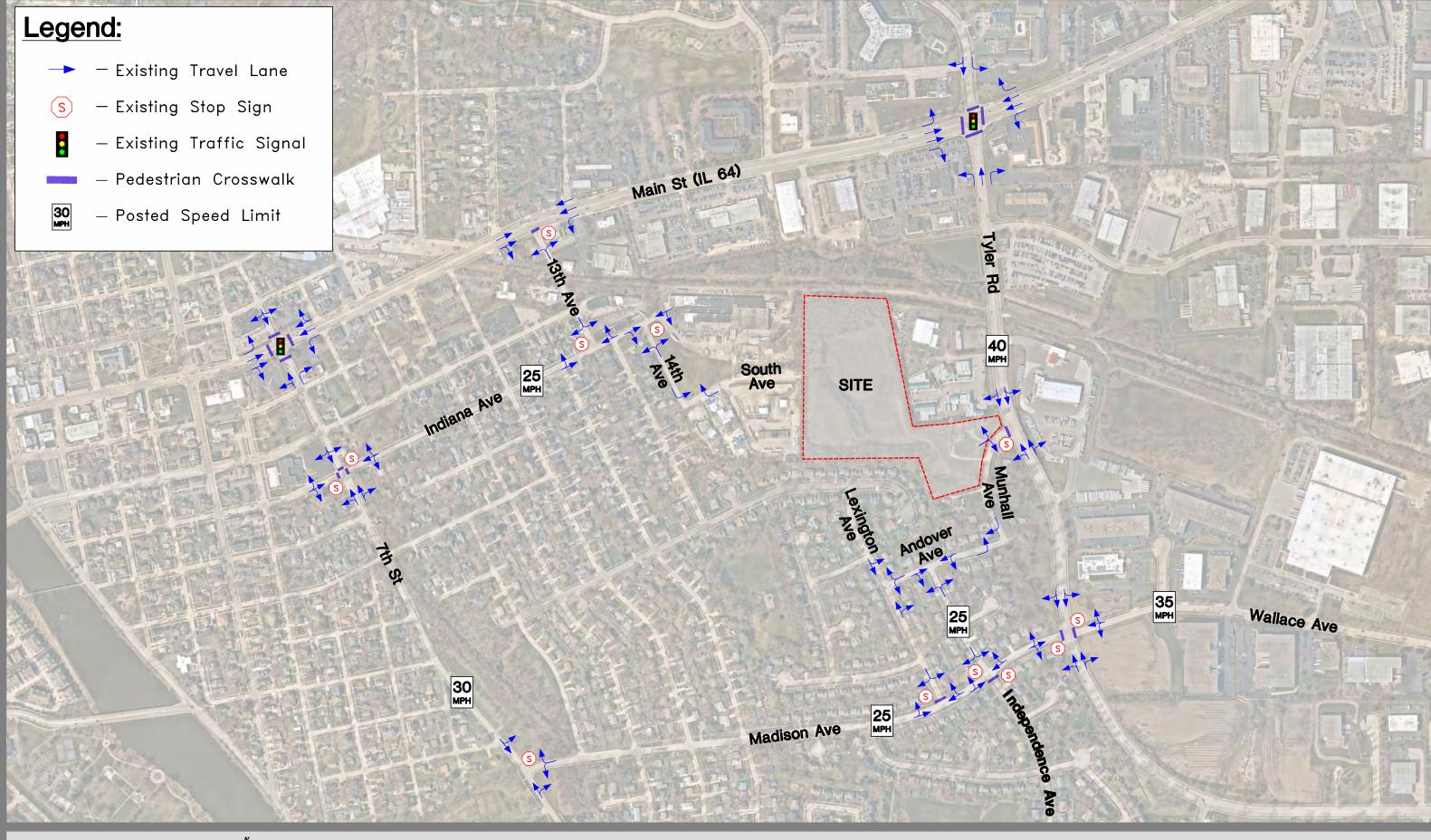




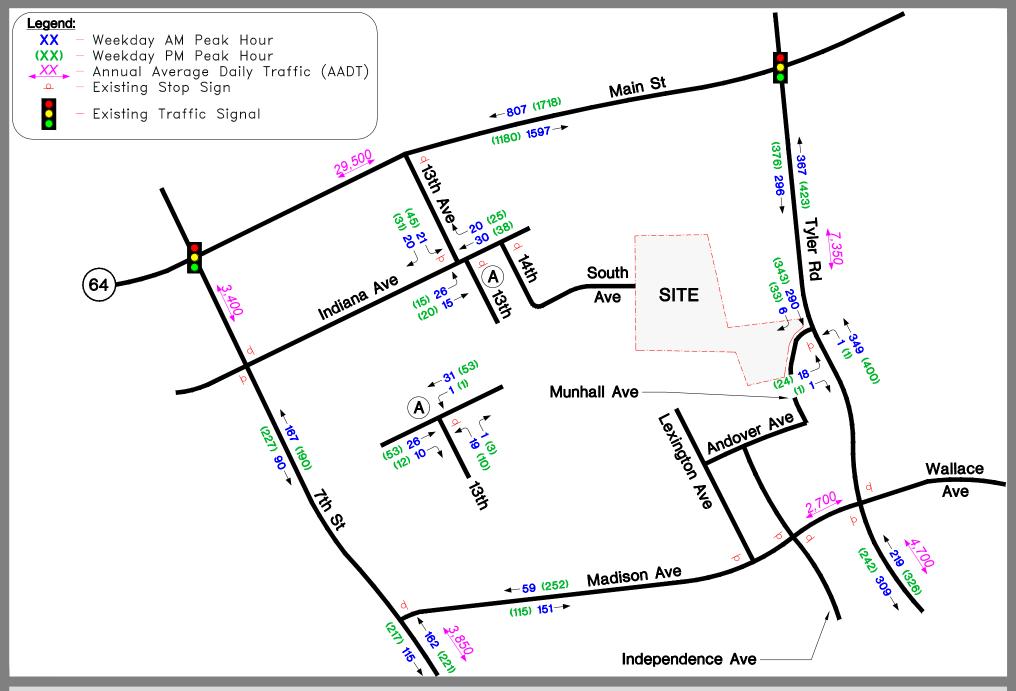
Exhibit 1 - Location Map

Proposed Residential Development St. Charles, IL



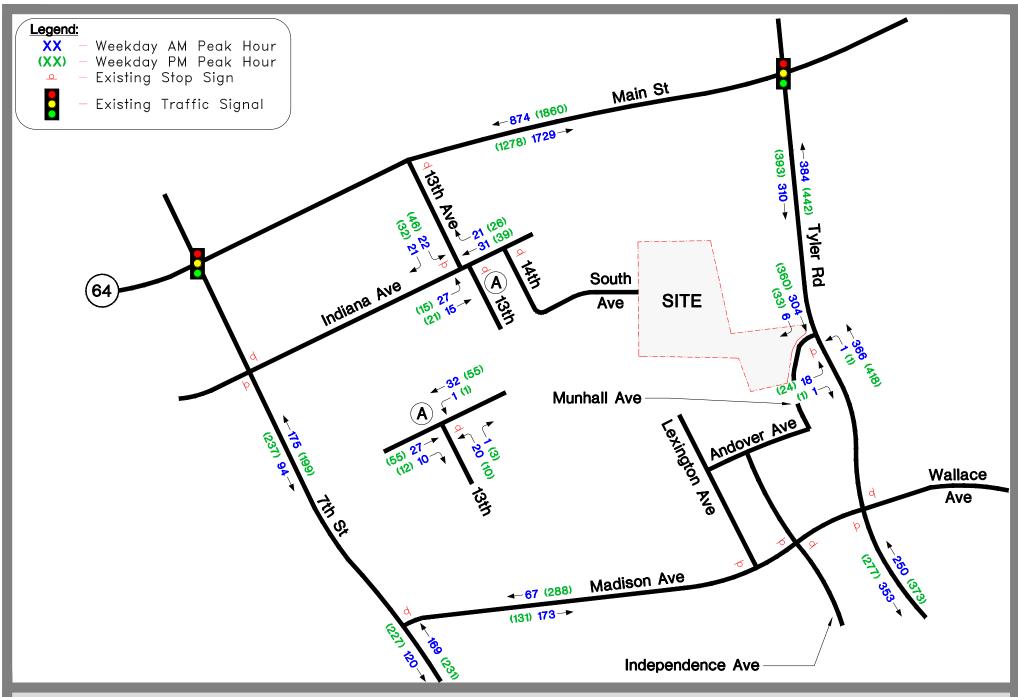
















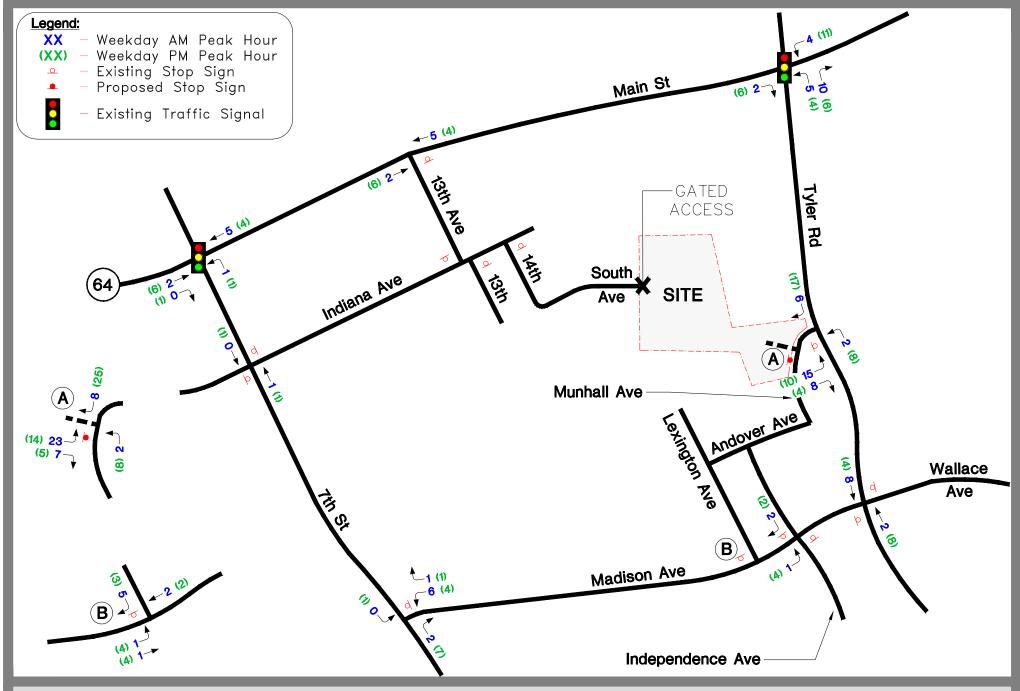






Exhibit 5A Site Traffic (Gated access at South Ave)

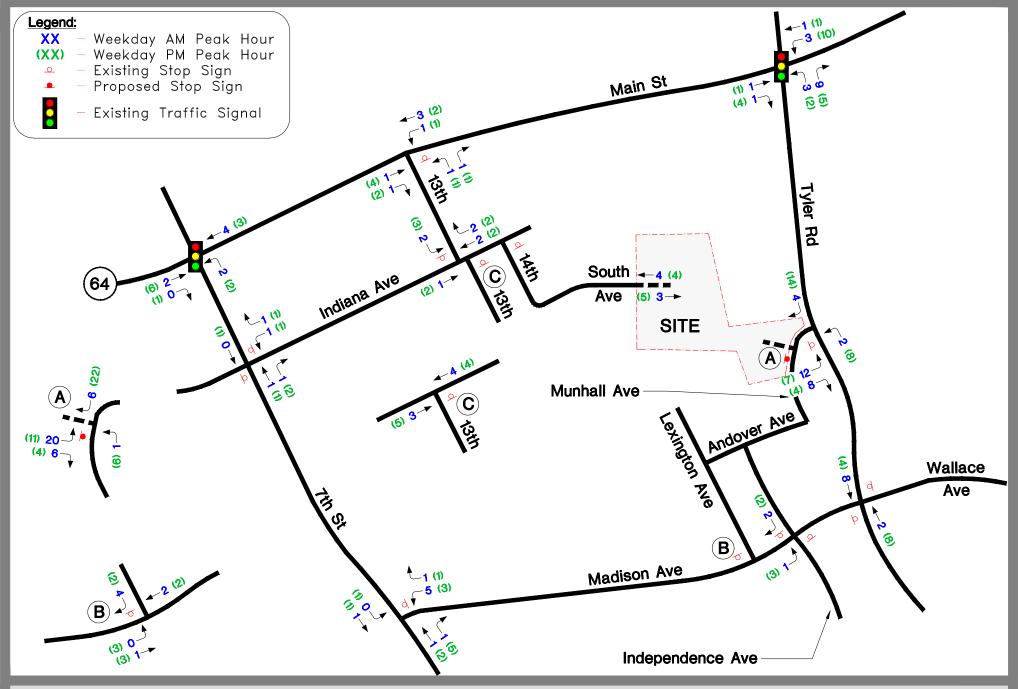






Exhibit 5B Site Traffic (Open access at South Ave)

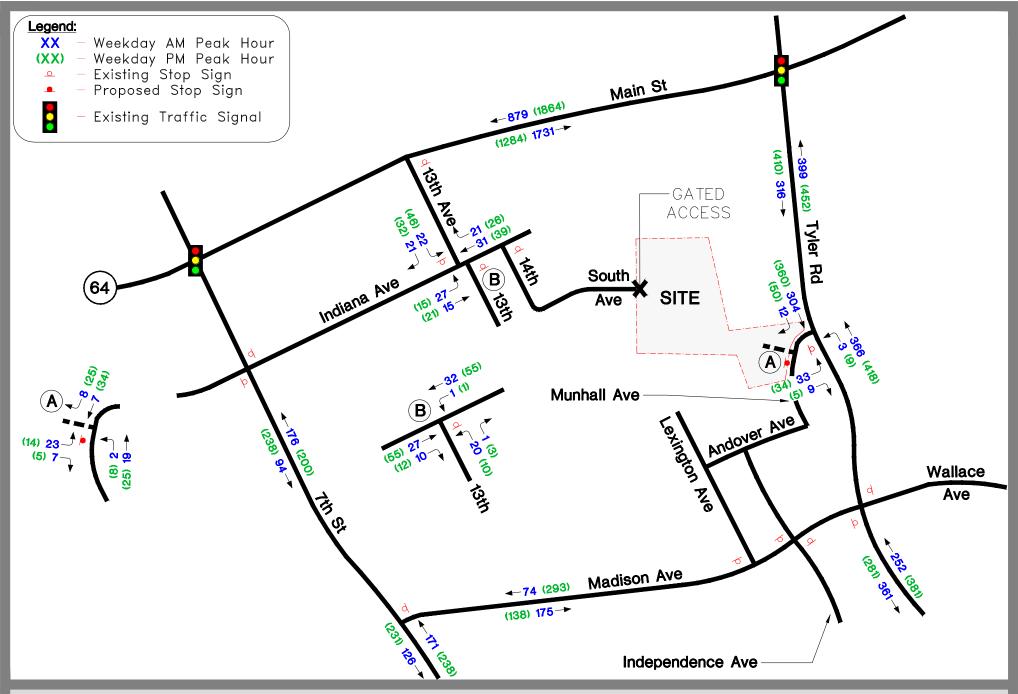






Exhibit 6A 2027 Total Traffic (Gated access at South Ave)

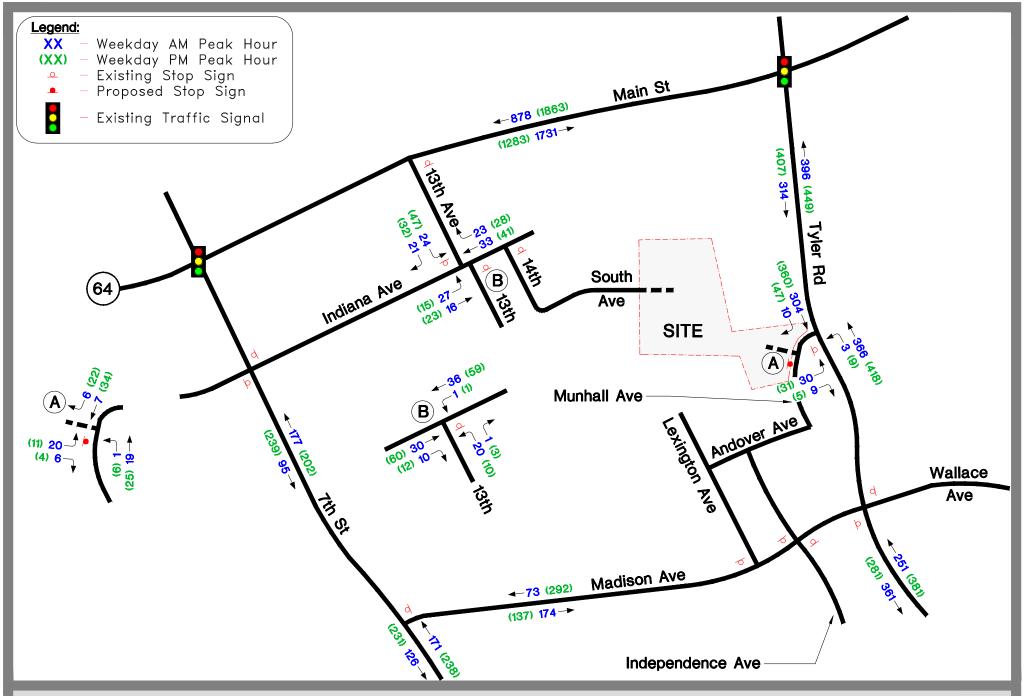






Exhibit 6B 2027 Total Traffic (Open access at South Ave)

Appendices



Appendix A Photo Inventory





Looking east along Munhall Ave at Tyler Rd



Looking north along Tyler Rd at Munhall Ave



Looking south along Tyler Rd at Munhall Ave



Looking south along Munhall Ave at Site Access





Looking north along Munhall Ave at Site Access



Looking south along Tyler Rd at Madison Ave/Wallace Ave



Looking east along Madison Ave at Tyler Rd



Looking east along Wallace Ave at Tyler Rd





Looking north along Tyler Rd at Madison Ave/Wallace Ave



Looking east along South Ave at Site Access



Looking west along Madison Ave at 7th St



Looking west along South Ave from Site Access

Appendix B IDOT Traffic Count Summaries





LOCATION INF	0
Location ID	045 0011
Туре	LINK
Fnct'l Class	3
Located On	Main St
From Road	7TH AVE
To Road	Kirk Rd
Direction	2-WAY
County	Kane
Community	ST CHARLES
MPO ID	
HPMS ID	
Agency	Illinois DOT

COUNT DATA INFO	
Count Status	Accepted
Start Date	Wed 8/28/2019
End Date	Thu 8/29/2019
Start Time	12:00:00 PM
End Time	12:00:00 PM
Direction	2-WAY
Notes	
Station	IL 64
Study	
Speed Limit	
Description	
Sensor Type	
Source	CombineVolumeCountsIncremental
Latitude,Longitude	

INTERVAL:60-MIN	
Time	Hourly Count
0:00-1:00	134
1:00-2:00	97
2:00-3:00	84
3:00-4:00	93
4:00-5:00	260
5:00-6:00	688
6:00-7:00	1,557
7:00-8:00	2,404
8:00-9:00	2,259
9:00-10:00	1,768
10:00-11:00	1,742
11:00-12:00 📵	2,048
12:00-13:00	2,281
13:00-14:00	2,137
14:00-15:00	2,256
15:00-16:00	2,428
16:00-17:00	2,744
17:00-18:00	2,898
18:00-19:00	2,348
19:00-20:00	1,657
20:00-21:00	1,411
21:00-22:00	865
22:00-23:00	495
23:00-24:00	277
Total	34,931
AM Peak	07:00-08:00 2,404
PM Peak	17:00-18:00 2,898



LOCATION INF	FO
Location ID	045 0011_EB
Туре	LINK
Fnct'l Class	3
Located On	Main St
From Road	7TH AVE
To Road	Kirk Rd
Direction	EB
County	Kane
Community	ST CHARLES
MPO ID	
HPMS ID	
Agency	Illinois DOT

COUNT DATA INFO	
Count Status	Accepted
Start Date	Wed 8/28/2019
End Date	Thu 8/29/2019
Start Time	12:00:00 PM
End Time	12:00:00 PM
Direction	EB
Notes	
Station	IL 64
Study	
Speed Limit	
Description	
Sensor Type	
Source	CombineVolumeCountsIncremental
Latitude,Longitude	

INTERVAL:60-MIN	
Time	Hourly Count
0:00-1:00	47
1:00-2:00	39
2:00-3:00	46
3:00-4:00	64
4:00-5:00	175
5:00-6:00	495
6:00-7:00	1,076
7:00-8:00	1,597
8:00-9:00	1,369
9:00-10:00	917
10:00-11:00	947
11:00-12:00 📵	1,047
12:00-13:00	1,144
13:00-14:00	1,036
14:00-15:00	1,057
15:00-16:00	1,082
16:00-17:00	1,106
17:00-18:00	1,180
18:00-19:00	958
19:00-20:00	742
20:00-21:00	673
21:00-22:00	382
22:00-23:00	226
23:00-24:00	121
Total	17,526
AM Peak	07:00-08:00 1,597
PM Peak	17:00-18:00 1,180



LOCATION INF	FO
Location ID	045 0011_WB
Туре	LINK
Fnct'l Class	3
Located On	Main St
From Road	7TH AVE
To Road	Kirk Rd
Direction	WB
County	Kane
Community	ST CHARLES
MPO ID	
HPMS ID	
Agency	Illinois DOT

COUNT DATA INFO	
Count Status	Accepted
Start Date	Wed 8/28/2019
End Date	Thu 8/29/2019
Start Time	12:00:00 PM
End Time	12:00:00 PM
Direction	WB
Notes	
Station	IL 64
Study	
Speed Limit	
Description	
Sensor Type	
Source	CombineVolumeCountsIncremental
Latitude,Longitude	

INTERVAL:60-MIN	
Time	Hourly Count
0:00-1:00	87
1:00-2:00	58
2:00-3:00	38
3:00-4:00	29
4:00-5:00	85
5:00-6:00	193
6:00-7:00	481
7:00-8:00	807
8:00-9:00	890
9:00-10:00	851
10:00-11:00	795
11:00-12:00	1,001
12:00-13:00	1,137
13:00-14:00	1,101
14:00-15:00	1,199
15:00-16:00	1,346
16:00-17:00	1,638
17:00-18:00	1,718
18:00-19:00	1,390
19:00-20:00	915
20:00-21:00	738
21:00-22:00	483
22:00-23:00	269
23:00-24:00	156
Total	17,405
AM Peak	11:00-12:00 1,001
PM Peak	17:00-18:00 1,718



LOCATION INF	FO
Location ID	045 3792
Туре	LINK
Fnct'l Class	5
Located On	Tyler Rd
From Road	Main St
To Road	MADISON AVE
Direction	2-WAY
County	Kane
Community	ST CHARLES
MPO ID	
HPMS ID	
Agency	Illinois DOT

COUNT DATA INFO	
Count Status	Accepted
Start Date	Tue 6/26/2018
End Date	Wed 6/27/2018
Start Time	2:00:00 PM
End Time	2:00:00 PM
Direction	2-WAY
Notes	
Station	TYLER RD
Study	
Speed Limit	
Description	
Sensor Type	
Source	CombineVolumeCountsIncremental
Latitude,Longitude	

INTERVAL:60-MIN	
Time	Hourly Count
0:00-1:00	45
1:00-2:00	20
2:00-3:00	13
3:00-4:00	15
4:00-5:00	36
5:00-6:00	230
6:00-7:00	407
7:00-8:00	637
8:00-9:00	604
9:00-10:00	473
10:00-11:00	490
11:00-12:00	537
12:00-13:00	605
13:00-14:00 📵	581
14:00-15:00	602
15:00-16:00	615
16:00-17:00	799
17:00-18:00	712
18:00-19:00	532
19:00-20:00	362
20:00-21:00	240
21:00-22:00	199
22:00-23:00	127
23:00-24:00	59
Total	8,940
AM Peak	07:00-08:00 637
PM Peak	16:00-17:00 799



LOCATION INFO	
Location ID	045 3792_NB
Туре	LINK
Fnct'l Class	5
Located On	Tyler Rd
From Road	Main St
To Road	MADISON AVE
Direction	NB
County	Kane
Community	ST CHARLES
MPO ID	
HPMS ID	
Agency	Illinois DOT

COUNT DATA INFO	
Count Status	Accepted
Start Date	Tue 6/26/2018
End Date	Wed 6/27/2018
Start Time	2:00:00 PM
End Time	2:00:00 PM
Direction	NB
Notes	
Station	TYLER RD
Study	
Speed Limit	
Description	
Sensor Type	
Source	CombineVolumeCountsIncremental
Latitude,Longitude	

INTERVAL:60-MIN	
Time	Hourly Count
0:00-1:00	21
1:00-2:00	15
2:00-3:00	7
3:00-4:00	6
4:00-5:00	17
5:00-6:00	113
6:00-7:00	191
7:00-8:00	367
8:00-9:00	316
9:00-10:00	222
10:00-11:00	246
11:00-12:00	261
12:00-13:00	310
13:00-14:00 📵	311
14:00-15:00	292
15:00-16:00	279
16:00-17:00	423
17:00-18:00	364
18:00-19:00	291
19:00-20:00	194
20:00-21:00	133
21:00-22:00	105
22:00-23:00	70
23:00-24:00	34
Total	4,588
AM Peak	07:00-08:00 367
PM Peak	16:00-17:00 423



LOCATION INFO	
Location ID	045 3792_SB
Туре	LINK
Fnct'l Class	5
Located On	Tyler Rd
From Road	Main St
To Road	MADISON AVE
Direction	SB
County	Kane
Community	ST CHARLES
MPO ID	
HPMS ID	
Agency	Illinois DOT

COUNT DATA INFO	
Count Status	Accepted
Start Date	Tue 6/26/2018
End Date	Wed 6/27/2018
Start Time	2:00:00 PM
End Time	2:00:00 PM
Direction	SB
Notes	
Station	TYLER RD
Study	
Speed Limit	
Description	
Sensor Type	
Source	CombineVolumeCountsIncremental
Latitude,Longitude	

INTERVAL:60-MIN	
Time	Hourly Count
0:00-1:00	24
1:00-2:00	5
2:00-3:00	6
3:00-4:00	9
4:00-5:00	19
5:00-6:00	117
6:00-7:00	216
7:00-8:00	270
8:00-9:00	288
9:00-10:00	251
10:00-11:00	244
11:00-12:00	276
12:00-13:00	295
13:00-14:00 📵	270
14:00-15:00	310
15:00-16:00	336
16:00-17:00	376
17:00-18:00	348
18:00-19:00	241
19:00-20:00	168
20:00-21:00	107
21:00-22:00	94
22:00-23:00	57
23:00-24:00	25
Total	4,352
AM Peak	08:00-09:00 288
PM Peak	16:00-17:00 376



LOCATION INF	0
Location ID	045 3793
Туре	LINK
Fnct'l Class	5
Located On	Tyler Rd
From Road	MADISON AVE
To Road	Kirk Rd
Direction	2-WAY
County	Kane
Community	ST CHARLES
MPO ID	
HPMS ID	
Agency	Illinois DOT

COUNT DATA INFO	
Count Status	Accepted
Start Date	Thu 7/19/2018
End Date	Fri 7/20/2018
Start Time	12:00:00 AM
End Time	12:00:00 AM
Direction	2-WAY
Notes	
Station	TYLER RD
Study	
Speed Limit	
Description	
Sensor Type	
Source	CombineVolumeCountsIncremental
Latitude,Longitude	

INTERVAL:60-MIN	
INTERVAL.00-IVI	Hourly
Time	Count
0:00-1:00	35
1:00-2:00	13
2:00-3:00	8
3:00-4:00	10
4:00-5:00	43
5:00-6:00	167
6:00-7:00	319
7:00-8:00	528
8:00-9:00	491
9:00-10:00	339
10:00-11:00	355
11:00-12:00	316
12:00-13:00	370
13:00-14:00	334
14:00-15:00	397
15:00-16:00	442
16:00-17:00	532
17:00-18:00	568
18:00-19:00	350
19:00-20:00	272
20:00-21:00	221
21:00-22:00	189
22:00-23:00	138
23:00-24:00 📵	74
Total	6,511
AM Peak	07:00-08:00 528
PM Peak	17:00-18:00 568



LOCATION INFO	
Location ID	045 3793_NB
Туре	LINK
Fnct'l Class	5
Located On	Tyler Rd
From Road	MADISON AVE
To Road	Kirk Rd
Direction	NB
County	Kane
Community	ST CHARLES
MPO ID	
HPMS ID	
Agency	Illinois DOT

COUNT DATA INFO	
Count Status	Accepted
Start Date	Thu 7/19/2018
End Date	Fri 7/20/2018
Start Time	12:00:00 AM
End Time	12:00:00 AM
Direction	NB
Notes	
Station	TYLER RD
Study	
Speed Limit	
Description	
Sensor Type	_
Source	CombineVolumeCountsIncremental
Latitude,Longitude	_

INTERVAL:60-MIN	
Time	Hourly Count
0:00-1:00	14
1:00-2:00	9
2:00-3:00	1
3:00-4:00	4
4:00-5:00	10
5:00-6:00	68
6:00-7:00	132
7:00-8:00	219
8:00-9:00	220
9:00-10:00	184
10:00-11:00	187
11:00-12:00	176
12:00-13:00	200
13:00-14:00	188
14:00-15:00	228
15:00-16:00	261
16:00-17:00	327
17:00-18:00	326
18:00-19:00	198
19:00-20:00	157
20:00-21:00	123
21:00-22:00	109
22:00-23:00	86
23:00-24:00 📵	42
Total	3,469
AM Peak	08:00-09:00 220
PM Peak	16:00-17:00 327



LOCATION INFO	
Location ID	045 3793_SB
Туре	LINK
Fnct'l Class	5
Located On	Tyler Rd
From Road	MADISON AVE
To Road	Kirk Rd
Direction	SB
County	Kane
Community	ST CHARLES
MPO ID	
HPMS ID	
Agency	Illinois DOT

COUNT DATA INFO	
Count Status	Accepted
Start Date	Thu 7/19/2018
End Date	Fri 7/20/2018
Start Time	12:00:00 AM
End Time	12:00:00 AM
Direction	SB
Notes	
Station	TYLER RD
Study	
Speed Limit	
Description	
Sensor Type	
Source	CombineVolumeCountsIncremental
Latitude,Longitude	

INTERVAL:60-MIN	
Time	Hourly Count
0:00-1:00	21
1:00-2:00	4
2:00-3:00	7
3:00-4:00	6
4:00-5:00	33
5:00-6:00	99
6:00-7:00	187
7:00-8:00	309
8:00-9:00	271
9:00-10:00	155
10:00-11:00	168
11:00-12:00	140
12:00-13:00	170
13:00-14:00	146
14:00-15:00	169
15:00-16:00	181
16:00-17:00	205
17:00-18:00	242
18:00-19:00	152
19:00-20:00	115
20:00-21:00	98
21:00-22:00	80
22:00-23:00	52
23:00-24:00 📵	32
Total	3,042
AM Peak	07:00-08:00 309
PM Peak	17:00-18:00 242



LOCATION INFO	
Location ID	045 3640
Туре	LINK
Fnct'l Class	5
Located On	MADISON AVE
From Road	7TH AVE
To Road	Tyler Rd
Direction	2-WAY
County	Kane
Community	ST CHARLES
MPO ID	
HPMS ID	
Agency	Illinois DOT

COUNT DATA INFO	
Count Status	Accepted
Start Date	Thu 7/19/2018
End Date	Fri 7/20/2018
Start Time	12:00:00 AM
End Time	12:00:00 AM
Direction	2-WAY
Notes	
Station	MADISON AVE
Study	
Speed Limit	
Description	
Sensor Type	
Source	CombineVolumeCountsIncremental
Latitude,Longitude	

INTERVAL:60-MIN	
Time	Hourly Count
0:00-1:00	14
1:00-2:00	10
2:00-3:00	7
3:00-4:00	11
4:00-5:00	17
5:00-6:00	55
6:00-7:00	129
7:00-8:00	207
8:00-9:00	210
9:00-10:00	126
10:00-11:00	150
11:00-12:00	181
12:00-13:00	218
13:00-14:00	160
14:00-15:00	178
15:00-16:00	230
16:00-17:00	275
17:00-18:00	367
18:00-19:00	231
19:00-20:00	138
20:00-21:00	109
21:00-22:00	103
22:00-23:00	69
23:00-24:00	39
Total	3,234
AM Peak	08:00-09:00 210
PM Peak	17:00-18:00 367



LOCATION INFO	
Location ID	045 3640_EB
Туре	LINK
Fnct'l Class	5
Located On	MADISON AVE
From Road	7TH AVE
To Road	Tyler Rd
Direction	EB
County	Kane
Community	ST CHARLES
MPO ID	
HPMS ID	
Agency	Illinois DOT

COUNT DATA INFO	
Count Status	Accepted
Start Date	Thu 7/19/2018
End Date	Fri 7/20/2018
Start Time	12:00:00 AM
End Time	12:00:00 AM
Direction	EB
Notes	
Station	MADISON AVE
Study	
Speed Limit	
Description	
Sensor Type	
Source	CombineVolumeCountsIncremental
Latitude,Longitude	

INTERVAL:60-MIN	
Time	Hourly Count
0:00-1:00	5
1:00-2:00	2
2:00-3:00	6
3:00-4:00	5
4:00-5:00	15
5:00-6:00	50
6:00-7:00	101
7:00-8:00	166
8:00-9:00	151
9:00-10:00	78
10:00-11:00	76
11:00-12:00	78
12:00-13:00	100
13:00-14:00	83
14:00-15:00	76
15:00-16:00	68
16:00-17:00	77
17:00-18:00	115
18:00-19:00	100
19:00-20:00	68
20:00-21:00	46
21:00-22:00	40
22:00-23:00	28
23:00-24:00 📵	7
Total	1,541
AM Peak	07:00-08:00 166
PM Peak	17:00-18:00 115



LOCATION INFO	
Location ID	045 3640_WB
Туре	LINK
Fnct'l Class	5
Located On	MADISON AVE
From Road	7TH AVE
To Road	Tyler Rd
Direction	WB
County	Kane
Community	ST CHARLES
MPO ID	
HPMS ID	
Agency	Illinois DOT

COUNT DATA INFO	
Count Status	Accepted
Start Date	Thu 7/19/2018
End Date	Fri 7/20/2018
Start Time	12:00:00 AM
End Time	12:00:00 AM
Direction	WB
Notes	
Station	MADISON AVE
Study	
Speed Limit	
Description	
Sensor Type	
Source	CombineVolumeCountsIncremental
Latitude,Longitude	

INTERVAL:60-MIN	
Time	Hourly Count
0:00-1:00	9
1:00-2:00	8
2:00-3:00	1
3:00-4:00	6
4:00-5:00	2
5:00-6:00	5
6:00-7:00	28
7:00-8:00	41
8:00-9:00	59
9:00-10:00	48
10:00-11:00	74
11:00-12:00	103
12:00-13:00	118
13:00-14:00	77
14:00-15:00	102
15:00-16:00	162
16:00-17:00	198
17:00-18:00	252
18:00-19:00	131
19:00-20:00	70
20:00-21:00	63
21:00-22:00	63
22:00-23:00	41
23:00-24:00 📵	32
Total	1,693
AM Peak	11:00-12:00 103
PM Peak	17:00-18:00 252



LOCATION INF	0
Location ID	045 3794
Туре	LINK
Fnct'l Class	5
Located On	7TH AVE
From Road	Main St
To Road	MADISON AVE
Direction	2-WAY
County	Kane
Community	ST CHARLES
MPO ID	
HPMS ID	
Agency	Illinois DOT

COUNT DATA INFO	
Count Status	Accepted
Start Date	Tue 6/26/2018
End Date	Wed 6/27/2018
Start Time	2:00:00 PM
End Time	2:00:00 PM
Direction	2-WAY
Notes	
Station	7TH AVE
Study	
Speed Limit	
Description	
Sensor Type	
Source	CombineVolumeCountsIncremental
Latitude,Longitude	

INTERVAL:60-MIN	
Time	Hourly Count
0:00-1:00	17
1:00-2:00	11
2:00-3:00	8
3:00-4:00	10
4:00-5:00	14
5:00-6:00	57
6:00-7:00	148
7:00-8:00	257
8:00-9:00	255
9:00-10:00	257
10:00-11:00	279
11:00-12:00	287
12:00-13:00	292
13:00-14:00 📵	286
14:00-15:00	263
15:00-16:00	320
16:00-17:00	395
17:00-18:00	417
18:00-19:00	296
19:00-20:00	194
20:00-21:00	116
21:00-22:00	89
22:00-23:00	62
23:00-24:00	33
Total	4,363
AM Peak	11:00-12:00 287
PM Peak	17:00-18:00 417



LOCATION INF	FO
Location ID	045 3794_NB
Туре	LINK
Fnct'l Class	5
Located On	7TH AVE
From Road	Main St
To Road	MADISON AVE
Direction	NB
County	Kane
Community	ST CHARLES
MPO ID	
HPMS ID	
Agency	Illinois DOT

COUNT DATA INFO	
Count Status	Accepted
Start Date	Tue 6/26/2018
End Date	Wed 6/27/2018
Start Time	2:00:00 PM
End Time	2:00:00 PM
Direction	NB
Notes	
Station	7TH AVE
Study	
Speed Limit	
Description	
Sensor Type	
Source	CombineVolumeCountsIncremental
Latitude,Longitude	

INTERVAL:60-MIN	
Time	Hourly Count
0:00-1:00	6
1:00-2:00	3
2:00-3:00	4
3:00-4:00	5
4:00-5:00	9
5:00-6:00	37
6:00-7:00	89
7:00-8:00	167
8:00-9:00	152
9:00-10:00	125
10:00-11:00	146
11:00-12:00	146
12:00-13:00	140
13:00-14:00 📵	126
14:00-15:00	115
15:00-16:00	146
16:00-17:00	162
17:00-18:00	190
18:00-19:00	118
19:00-20:00	71
20:00-21:00	39
21:00-22:00	25
22:00-23:00	24
23:00-24:00	13
Total	2,058
AM Peak	07:00-08:00 167
PM Peak	17:00-18:00 190



LOCATION INF	FO
Location ID	045 3794_SB
Туре	LINK
Fnct'l Class	5
Located On	7TH AVE
From Road	Main St
To Road	MADISON AVE
Direction	SB
County	Kane
Community	ST CHARLES
MPO ID	
HPMS ID	
Agency	Illinois DOT

COUNT DATA INFO	
Count Status	Accepted
Start Date	Tue 6/26/2018
End Date	Wed 6/27/2018
Start Time	2:00:00 PM
End Time	2:00:00 PM
Direction	SB
Notes	
Station	7TH AVE
Study	
Speed Limit	
Description	
Sensor Type	
Source	CombineVolumeCountsIncremental
Latitude,Longitude	

INTERVAL:60-MIN	
Time	Hourly Count
0:00-1:00	11
1:00-2:00	8
2:00-3:00	4
3:00-4:00	5
4:00-5:00	5
5:00-6:00	20
6:00-7:00	59
7:00-8:00	90
8:00-9:00	103
9:00-10:00	132
10:00-11:00	133
11:00-12:00	141
12:00-13:00	152
13:00-14:00 📵	160
14:00-15:00	148
15:00-16:00	174
16:00-17:00	233
17:00-18:00	227
18:00-19:00	178
19:00-20:00	123
20:00-21:00	77
21:00-22:00	64
22:00-23:00	38
23:00-24:00	20
Total	2,305
AM Peak	11:00-12:00 141
PM Peak	16:00-17:00 233



LOCATION INF	0
Location ID	045 3795
Туре	LINK
Fnct'l Class	5
Located On	7TH AVE
From Road	MADISON AVE
To Road	State St
Direction	2-WAY
County	Kane
Community	ST CHARLES
MPO ID	
HPMS ID	
Agency	Illinois DOT

COUNT DATA INFO	
Count Status	Accepted
Start Date	Tue 6/26/2018
End Date	Wed 6/27/2018
Start Time	2:00:00 PM
End Time	2:00:00 PM
Direction	2-WAY
Notes	
Station	7TH AVE
Study	
Speed Limit	
Description	
Sensor Type	
Source	CombineVolumeCountsIncremental
Latitude,Longitude	

INTERVAL:60-MIN	
Time	Hourly Count
0:00-1:00	17
1:00-2:00	12
2:00-3:00	8
3:00-4:00	17
4:00-5:00	15
5:00-6:00	69
6:00-7:00	158
7:00-8:00	269
8:00-9:00	277
9:00-10:00	238
10:00-11:00	266
11:00-12:00	304
12:00-13:00	315
13:00-14:00 📵	311
14:00-15:00	265
15:00-16:00	349
16:00-17:00	438
17:00-18:00	429
18:00-19:00	318
19:00-20:00	208
20:00-21:00	124
21:00-22:00	107
22:00-23:00	65
23:00-24:00	48
Total	4,627
AM Peak	11:00-12:00 304
PM Peak	16:00-17:00 438



LOCATION INF	0
Location ID	045 3795_NB
Туре	LINK
Fnct'l Class	5
Located On	7TH AVE
From Road	MADISON AVE
To Road	State St
Direction	NB
County	Kane
Community	ST CHARLES
MPO ID	
HPMS ID	
Agency	Illinois DOT

COUNT DATA INF	-O
Count Status	Accepted
Start Date	Tue 6/26/2018
End Date	Wed 6/27/2018
Start Time	2:00:00 PM
End Time	2:00:00 PM
Direction	NB
Notes	
Station	7TH AVE
Study	
Speed Limit	
Description	
Sensor Type	
Source	CombineVolumeCountsIncremental
Latitude,Longitude	

INTERVAL:60-M	IN
Time	Hourly Count
0:00-1:00	12
1:00-2:00	7
2:00-3:00	6
3:00-4:00	12
4:00-5:00	7
5:00-6:00	39
6:00-7:00	89
7:00-8:00	146
8:00-9:00	162
9:00-10:00	117
10:00-11:00	123
11:00-12:00	170
12:00-13:00	173
13:00-14:00 📵	168
14:00-15:00	133
15:00-16:00	183
16:00-17:00	221
17:00-18:00	236
18:00-19:00	163
19:00-20:00	105
20:00-21:00	72
21:00-22:00	49
22:00-23:00	35
23:00-24:00	25
Total	2,453
AM Peak	11:00-12:00 170
PM Peak	17:00-18:00 236



LOCATION INF	FO
Location ID	045 3795_SB
Туре	LINK
Fnct'l Class	5
Located On	7TH AVE
From Road	MADISON AVE
To Road	State St
Direction	SB
County	Kane
Community	ST CHARLES
MPO ID	
HPMS ID	
Agency	Illinois DOT

COUNT DATA INF	0
Count Status	Accepted
Start Date	Tue 6/26/2018
End Date	Wed 6/27/2018
Start Time	2:00:00 PM
End Time	2:00:00 PM
Direction	SB
Notes	
Station	7TH AVE
Study	
Speed Limit	
Description	
Sensor Type	_
Source	CombineVolumeCountsIncremental
Latitude,Longitude	_

INTERVAL:60-M	IN
Time	Hourly Count
0:00-1:00	5
1:00-2:00	5
2:00-3:00	2
3:00-4:00	5
4:00-5:00	8
5:00-6:00	30
6:00-7:00	69
7:00-8:00	123
8:00-9:00	115
9:00-10:00	121
10:00-11:00	143
11:00-12:00	134
12:00-13:00	142
13:00-14:00 📵	143
14:00-15:00	132
15:00-16:00	166
16:00-17:00	217
17:00-18:00	193
18:00-19:00	155
19:00-20:00	103
20:00-21:00	52
21:00-22:00	58
22:00-23:00	30
23:00-24:00	23
Total	2,174
AM Peak	10:00-11:00 143
PM Peak	16:00-17:00 217

Appendix CIntersection Traffic Count Summaries



Tyler Rd / Munhall Ave - TMC

Tue Aug 25, 2020 Full Length (7 AM-9 AM, 4 PM-6 PM)

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks,

Pedestrians)

All Movements

ID: 776044, Location: 41.913573, -88.291841



Provided by: Gewalt Hamilton Associates Inc. 625 Forest Edge Drive, Vernon Hills, IL, 60061, US

Leg	Munhall					Tyler					Tyler					
Dire ction	Eastbou	nd				Northb	ound				Southbo	und				
Time	L	R	U	App	Ped*	L	T	U	App	Pe d*	T	R	U	App	Pe d*	Int
2020-08-25 7:00A	1 7	0	0	7	0	0	48	0	48	0	48	2	0	50	0	105
7:15A	1 7	0	0	7	0	0	49	0	49	0	63	3	0	66	0	122
7:30AI	4 6	0	0	6	0	0	54	0	54	0	62	4	0	66	0	126
7:45A	1 1	0	0	1	0	0	84	0	84	0	83	0	0	83	0	168
Hourly Tota	l 21	0	0	21	0	0	235	0	235	0	256	9	0	265	0	521
8:00Al	4 3	0	0	3	0	0	66	0	66	0	68	1	1	70	0	139
8:15A	4 3	0	0	3	0	0	48	0	48	0	77	1	0	78	0	129
8:30AI	1 3	1	0	4	0	0	54	0	54	0	60	4	0	64	0	122
8:45AI	1 2	0	0	2	0	0	73	0	73	0	63	1	0	64	0	139
Hourly Tota	l 11	1	0	12	0	0	241	0	241	0	268	7	1	276	0	529
4:00PI	1 4	0	0	4	0	1	77	0	78	0	91	10	0	101	0	183
4:15Pi	1 2	0	0	2	0	0	61	0	61	0	72	6	0	78	0	141
4:30Pi	1 1	0	0	1	0	1	74	0	75	0	76	10	0	86	0	162
4:45Pi	1 5	0	0	5	0	0	52	0	52	1	87	7	0	94	0	151
Hourly Tota	l 12	0	0	12	0	2	264	0	266	1	326	33	0	359	0	637
5:00PI	1 4	0	0	4	1	0	73	0	73	0	84	7	0	91	0	168
5:15Pi	4 6	0	0	6	0	0	73	0	73	0	89	8	0	97	0	176
5:30PI	1 6	0	0	6	0	1	56	0	57	0	79	8	0	87	0	150
5:45Pi	4 6	0	0	6	0	0	59	0	59	0	54	6	0	60	0	125
Hourly Tota	1 22	0	0	22	1	1	261	0	262	0	306	29	0	335	0	619
Tota	l 66	1	0	67	1	3	1001	0	1004	1	1156	78	1	1235	0	2306
% Approac	h 98.5%	1.5%	0%	-	-	0.3%	99.7%	0%	-	-	93.6%	6.3%	0.1%	-	-	-
% Tota	1 2.9%	0%	0%	2.9%	-	0.1%	43.4%	0%	43.5%	-	50.1%	3.4%	0%	53.6%	-	-
Light	s 65	1	0	66	-	3	982	0	985	-	1122	77	1	1200	-	2251
% Light	s 98.5%	100%	0%	98.5%	-	100%	98.1%	0%	98.1%	-	97.1%	98.7%	100%	97.2%	-	97.6%
Articulated Truck	s 0	0	0	0	-	0	2	0	2	-	6	0	0	6	-	8
% Articulated Truck	s 0%	0%	0%	0 %	-	0%	0.2%	0%	0.2%	-	0.5%	0%	0%	0.5%	-	0.3%
Buses and Single-Unit Truck	s 1	0	0	1	-	0	17	0	17	-	28	1	0	29	-	47
% Buses and Single-Unit Truck	s 1.5%	0%	0%	1.5 %	-	0%	1.7%	0%	1.7 %	-	2.4%	1.3%	0%	2.3%	-	2.0%
Pedestrian	s -	-	-	-	1	-	-	-	-	1	-	-	-	-	0	i e
% Pedestrian	s -	-	-	_	100%	-	-	_	-	100%	-	-	-	-	-	-

^{*}Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Tyler Rd / Munhall Ave - TMC

Tue Aug 25, 2020 AM Peak (7:30 AM - 8:30 AM)

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks,

Pedestrians) All Movements

ID: 776044, Location: 41.913573, -88.291841



Provided by: Gewalt Hamilton Associates Inc. 625 Forest Edge Drive, Vernon Hills, IL, 60061, US

Leg	Munhal	l				Tyle	r				Tyler					
Dire ction	Eastbou	nd				Nort	hbound				Southbo	und				
Time	L	R	U	App	Ped*	L	T	U	App	Pe d*	T	R	U	App	Pe d*	Int
2020-08-25 7:30AM	6	0	0	6	0	0	54	0	54	0	62	4	0	66	0	126
7:45AM	1	0	0	1	0	0	84	0	84	0	83	0	0	83	0	168
8:00AM	3	0	0	3	0	0	66	0	66	0	68	1	1	70	0	139
8:15AM	3	0	0	3	0	0	48	0	48	0	77	1	0	78	0	129
Total	13	0	0	13	0	0	252	0	252	0	290	6	1	297	0	562
% Approach	100%	0%	0%	-	-	0%	100%	0%	-	-	97.6%	2.0%	0.3%	-	-	-
% Total	2.3%	0%	0%	2.3%	-	0%	44.8%	0%	44.8%	-	51.6%	1.1%	0.2%	52.8%	-	-
PHF	0.542	-	-	0.542	-	-	0.750	-	0.750	-	0.873	0.375	0.250	0.895	-	0.836
Lights	12	0	0	12	-	0	245	0	245	-	280	6	1	287	-	544
% Lights	92.3%	0%	0%	92.3%	-	0%	97.2%	0%	97.2%	-	96.6%	100%	100%	96.6%	-	96.8%
Articulated Trucks	0	0	0	0	-	0	0	0	0	-	3	0	0	3	-	3
% Articulated Trucks	0%	0%	0%	0%	-	0%	0%	0%	0 %	-	1.0%	0%	0%	1.0 %	-	0.5%
Buses and Single-Unit Trucks	1	0	0	1	-	0	7	0	7	-	7	0	0	7	-	15
% Buses and Single-Unit Trucks	7.7%	0%	0%	7.7%	-	0%	2.8%	0%	2.8%	-	2.4%	0%	0%	2.4 %	-	2.7%
Pedestrians	-	-	-	-	0	-	-	-	-	0	-	-	-	-	0	
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-

^{*}Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Tyler Rd / Munhall Ave - TMC

Tue Aug 25, 2020 PM Peak (4:30 PM - 5:30 PM) - Overall Peak Hour All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks, Pedestrians) All Movements



Provided by: Gewalt Hamilton Associates Inc. 625 Forest Edge Drive, Vernon Hills, IL, 60061, US

ID: 776044, Location: 41.913573, -88.291841

Leg	Munha	11				Tyler					Tyler					
Dire ction	Eastbo	und				Northbo	ound				Southbo	und				
Time	L	R	U	App	Pe d*	L	T	U	App	Pe d*	T	R	U	App	Pe d*	Int
2020-08-25 4:30PM	1	0	0	1	0	1	74	0	75	0	76	10	0	86	0	162
4:45PM	5	0	0	5	0	0	52	0	52	1	87	7	0	94	0	151
5:00PM	4	0	0	4	1	0	73	0	73	0	84	7	0	91	0	168
5:15PM	6	0	0	6	0	0	73	0	73	0	89	8	0	97	0	176
Total	16	0	0	16	1	1	272	0	273	1	336	32	0	368	0	657
% Approach	100%	0%	0%	-	-	0.4%	99.6%	0%	-	-	91.3%	8.7%	0%	-	-	-
% Total	2.4%	0%	0%	2.4 %	-	0.2%	41.4%	0%	4 1.6 %	-	51.1%	4.9%	0%	56.0%	-	-
PHF	0.667	-	-	0.667	-	0.250	0.919	-	0.910	-	0.944	0.800	-	0.948	-	0.933
Lights	16	0	0	16	-	1	267	0	268	-	332	32	0	364	-	648
% Lights	100%	0%	0%	100%	-	100%	98.2%	0%	98.2%	-	98.8%	100%	0%	98.9%	-	98.6%
Articulate d Trucks	0	0	0	0	-	0	0	0	0	-	0	0	0	0	-	0
% Articulated Trucks	0%	0%	0%	0 %	-	0%	0%	0%	0 %	-	0%	0%	0%	0 %	-	0%
Buses and Single-Unit Trucks	0	0	0	0	-	0	5	0	5	-	4	0	0	4	-	9
% Buses and Single-Unit Trucks	0%	0%	0%	0 %	-	0%	1.8%	0%	1.8 %	-	1.2%	0%	0%	1.1%	-	1.4%
Pedestrians	-	-	-	-	1	-	-	-	-	1	-	-	-	-	0	
% Pedestrians	-	-	-	-	100%	-	-	-	-	100%	-	-	-	-	-	-

^{*}Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Indiana Ave / 13th Ave (N Leg) - TMC

Tue Aug 25, 2020

Full Length (7 AM-9 AM, 4 PM-6 PM)

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks, Pedestrians)

All Movements

ID: 776045, Location: 41.914851, -88.300412



Provided by: Gewalt Hamilton Associates Inc. 625 Forest Edge Drive, Vernon Hills, IL, 60061, US

Leg	Indiana	Ave				Indiana	Ave				13th Ave	,				
Dire ction	Eastbou	nd				Westbo	und				Southbo	und				
Time	L	T	U	App	Pe d*	T	R	U	App P	e d*	L	R	U	App	Pe d*	Int
2020-08-25 7:00AM	6	4	0	10	0	3	1	0	4	0	5	1	0	6	0	20
7:15AM	4	3	0	7	0	8	2	0	10	0	3	2	0	5	0	22
7:30AM	4	2	0	6	0	7	5	0	12	0	3	5	0	8	0	26
7:45AM	7	3	0	10	0	3	6	0	9	0	6	8	0	14	0	33
Hourly Total	21	12	0	33	0	21	14	0	35	0	17	16	0	33	0	101
8:00AM	5	2	0	7	0	7	2	0	9	0	4	0	0	4	0	20
8:15AM	3	1	0	4	0	1	3	0	4	0	2	0	0	2	0	10
8:30AM	3	3	0	6	0	4	2	0	6	0	2	1	0	3	0	15
8:45AM	0	3	0	3	0	2	3	0	5	0	3	0	0	3	0	11
Hourly Total	11	9	0	20	0	14	10	0	24	0	11	1	0	12	0	56
4:00PM	3	6	0	9	0	6	3	0	9	0	10	6	0	16	0	34
4:15PM	1	2	0	3	0	3	5	0	8	0	6	7	0	13	0	24
4:30PM	2	2	0	4	0	2	5	0	7	0	5	10	0	15	0	26
4:45PM	4	5	0	9	0	9	5	0	14	0	12	4	0	16	0	39
Hourly Total	10	15	0	25	0	20	18	0	38	0	33	27	0	60	0	123
5:00PM	2	4	0	6	0	13	7	0	20	0	7	7	0	14	0	40
5:15PM	4	5	0	9	0	6	3	0	9	0	12	4	0	16	0	34
5:30PM	1	2	0	3	0	3	6	0	9	0	3	5	0	8	0	20
5:45PM	6	5	0	11	0	6	4	0	10	0	6	3	0	9	0	30
Hourly Total	13	16	0	29	0	28	20	0	48	0	28	19	0	47	0	124
Total	55	52	0	107	0	83	62	0	145	0	89	63	0	152	0	404
% Approach	51.4%	48.6%	0%	_	-	57.2%	42.8%	0%	-	-	58.6%	41.4%	0%	-	-	-
% Total	13.6%	12.9%	0%	26.5%	-	20.5%	15.3%	0%	35.9%	-	22.0%	15.6%	0%	37.6%	-	-
Lights	54	48	0	102	-	75	59	0	134	_	87	62	0	149	-	385
% Lights	98.2%	92.3%	0%	95.3%	-	90.4%	95.2%	0%	92.4 %	-	97.8%	98.4%	0%	98.0%	-	95.3%
Artic ulate d Truc ks	1	1	0	2	-	3	0	0	3	_	0	0	0	0	-	5
% Articulated Trucks	1.8%	1.9%	0%	1.9 %	-	3.6%	0%	0%	2.1%	-	0%	0%	0%	0 %	-	1.2%
Buses and Single-Unit Trucks	0	3	0	3	-	5	3	0	8	-	2	1	0	3	-	14
% Buses and Single-Unit Trucks	0%	5.8%	0%	2.8%	-	6.0%	4.8%	0%	5.5%	-	2.2%	1.6%	0%	2.0%	-	3.5%
Pedestrians	-	-	-	-	0	-	-	-	-	0	-	-	-	-	0	
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
*D 1		1 7 7				_					•					-

^{*}Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Indiana Ave / 13th Ave (N Leg) - TMC

Tue Aug 25, 2020 AM Peak (7 AM - 8 AM)

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks,

Pedestrians) All Movements

ID: 776045, Location: 41.914851, -88.300412



Provided by: Gewalt Hamilton Associates Inc. 625 Forest Edge Drive, Vernon Hills, IL, 60061, US

Leg	Indiana	Ave				Indiana	Ave				13th Av	2				
Dire ction	Eastbou	nd				Westbo	und				Southbo	und				
Time	L	T	U	App	Ped*	T	R	U	App	Pe d*	L	R	U	App	Pe d*	Int
2020-08-25 7:00AM	6	4	0	10	0	3	1	0	4	0	5	1	0	6	0	20
7:15AM	4	3	0	7	0	8	2	0	10	0	3	2	0	5	0	22
7:30AM	4	2	0	6	0	7	5	0	12	0	3	5	0	8	0	26
7:45AM	7	3	0	10	0	3	6	0	9	0	6	8	0	14	0	33
Total	21	12	0	33	0	21	14	0	35	0	17	16	0	33	0	101
% Approach	63.6%	36.4%	0%	-	-	60.0%	40.0%	0%	-	-	51.5%	48.5%	0%	-	-	-
% Total	20.8%	11.9%	0%	32.7%	-	20.8%	13.9%	0%	34.7%	-	16.8%	15.8%	0%	32.7%	-	-
PHF	0.750	0.750	-	0.825	-	0.656	0.583	-	0.729	-	0.708	0.500	-	0.589	-	0.765
Lights	20	10	0	30	-	17	11	0	28	-	16	15	0	31	-	89
% Lights	95.2%	83.3%	0%	90.9%	-	81.0%	78.6%	0%	80.0%	-	94.1%	93.8%	0%	93.9%	-	88.1%
Articulate d Trucks	1	0	0	1	-	1	0	0	1	-	0	0	0	0	-	2
% Articulated Trucks	4.8%	0%	0%	3.0%	-	4.8%	0%	0%	2.9%	-	0%	0%	0%	0 %	-	2.0%
Buses and Single-Unit Trucks	0	2	0	2	-	3	3	0	6	-	1	1	0	2	-	10
% Buses and Single-Unit Trucks	0%	16.7%	0%	6.1%	-	14.3%	21.4%	0%	17.1%	-	5.9%	6.3%	0%	6.1%	-	9.9%
Pe de strians	-	-	-	-	0	-	-	-	-	0	-	-	-	-	0	
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-

^{*}Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Indiana Ave / 13th Ave (N Leg) - TMC

Tue Aug 25, 2020 PM Peak (4:30 PM - 5:30 PM) - Overall Peak Hour All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks, Pedestrians) All Movements



Provided by: Gewalt Hamilton Associates Inc. 625 Forest Edge Drive, Vernon Hills, IL, 60061, US

ID: 776045, Location: 41.914851, -88.300412

Leg	Indiana	Ave				Indiana	Ave				13th Ave	2				
Dire ction	Eastbou	nd				Westbo	und				Southbo	und				
Time	L	T	U	App	Ped*	T	R	U	App	Pe d*	L	R	U	App	Ped*	Int
2020-08-25 4:30PM	2	2	0	4	0	2	5	0	7	0	5	10	0	15	0	26
4:45PM	4	5	0	9	0	9	5	0	14	0	12	4	0	16	0	39
5:00PM	2	4	0	6	0	13	7	0	20	0	7	7	0	14	0	4 0
5:15PM	4	5	0	9	0	6	3	0	9	0	12	4	0	16	0	34
Total	12	16	0	28	0	30	20	0	50	0	36	25	0	61	0	139
% Approach	42.9%	57.1%	0%	-	-	60.0%	40.0%	0%	-	-	59.0%	41.0%	0%	-	-	-
% Total	8.6%	11.5%	0%	20.1%	-	21.6%	14.4%	0%	36.0%	-	25.9%	18.0%	0%	43.9%	-	-
PHF	0.750	0.800	-	0.778	-	0.577	0.714	-	0.625	-	0.750	0.625	-	0.953	-	0.869
Lights	12	15	0	27	-	30	20	0	50	-	35	25	0	60	-	137
% Lights	100%	93.8%	0%	96.4 %	-	100%	100%	0%	100%	-	97.2%	100%	0%	98.4 %	-	98.6%
Artic ulate d Truc ks	0	1	0	1	-	0	0	0	0	-	0	0	0	0	-	1
% Articulated Trucks	0%	6.3%	0%	3.6%	-	0%	0%	0%	0 %	-	0%	0%	0%	0 %	-	0.7%
Buses and Single-Unit Trucks	0	0	0	0	-	0	0	0	0	-	1	0	0	1	-	1
% Buses and Single-Unit Trucks	0%	0%	0%	0%	-	0%	0%	0%	0 %	-	2.8%	0%	0%	1.6%	-	0.7%
Pe de strians	-	-	-	-	0	-	-	-	-	0	-	-	-	-	0	
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-

^{*}Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Indiana Ave / 13th Ave (S Leg) - TMC

Tue Aug 25, 2020

Full Length (7 AM-9 AM, 4 PM-6 PM)

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks, Pedestrians)

All Movements

ID: 776046, Location: 41.914947, -88.30001



Provided by: Gewalt Hamilton Associates Inc. 625 Forest Edge Drive, Vernon Hills, IL, 60061, US

Leg	Indiana					Indiana					13th					
Dire ction	Eastbou	nd				Westbo	ound				Northbo	und				
Time	Т	R	U	App	Ped*	L	T	U	App	Ped*	L	R	U	App	Ped*	Int
2020-08-25 7:00AM	8	1	0	9	0	0	2	0	2	0	2	0	0	2	0	13
7:15AM	5	1	0	6	0	1	5	0	6	0	5	0	0	5	0	17
7:30AM	4	1	0	5	0	0	10	0	10	0	2	1	0	3	3	18
7:45AM	6	3	0	9	0	0	6	0	6	0	3	0	0	3	0	18
Hourly Total	23	6	0	29	0	1	23	0	24	0	12	1	0	13	3	66
8:00AM	4	2	0	6	0	0	4	0	4	0	5	0	0	5	0	15
8:15AM	2	1	0	3	0	0	1	0	1	0	3	0	0	3	0	7
8:30AM	4	1	0	5	0	0	2	0	2	0	4	0	0	4	0	11
8:45AM	5	1	0	6	0	0	2	0	2	0	3	0	0	3	0	11
Hourly Total	15	5	0	20	0	0	9	0	9	0	15	0	0	15	0	44
4:00PM	12	4	1	17	0	0	7	0	7	0	1	0	0	1	0	25
4:15PM	5	3	0	8	0	0	6	0	6	0	2	0	0	2	0	16
4:30PM	5	2	0	7	0	0	6	0	6	0	1	0	0	1	0	14
4:45PM	15	2	0	17	0	1	12	0	13	0	2	0	0	2	0	32
Hourly Total	37	11	1	49	0	1	31	0	32	0	6	0	0	6	0	87
5:00PM	11	0	0	11	0	0	17	0	17	0	3	0	0	3	0	31
5:15PM	11	6	0	17	0	0	7	0	7	0	2	2	0	4	0	28
5:30PM	2	3	0	5	0	0	5	0	5	0	4	0	0	4	0	14
5:45PM	9	2	0	11	0	0	8	0	8	0	2	0	0	2	0	21
Hourly Total	33	11	0	44	0	0	37	0	37	0	11	2	0	13	0	94
Total	108	33	1	14 2	0	2	100	0	102	0	44	3	0	47	3	291
% Approach	76.1%	23.2%	0.7%	-	-	2.0%	98.0%	0%	-	-	93.6%	6.4%	0%	-	-	-
% Total	37.1%	11.3%	0.3%	48.8%	-	0.7%	34.4%	0%	35.1%	-	15.1%	1.0%	0%	16.2%	-	-
Lights	103	32	1	136	-	2	93	0	95	-	41	3	0	44	-	275
% Lights	95.4%	97.0%	100%	95.8%	-	100%	93.0%	0%	93.1%	-	93.2%	100%	0%	93.6%	-	94.5%
Articulated Trucks	1	0	0	1	-	0	3	0	3	-	0	0	0	0	-	4
% Articulated Trucks	0.9%	0%	0%	0.7%	-	0%	3.0%	0%	2.9%	-	0%	0%	0%	0%	-	1.4%
Buses and Single-Unit Trucks	4	1	0	5	-	0	4	0	4	-	3	0	0	3	-	12
% Buses and Single-Unit Trucks	3.7%	3.0%	0%	3.5%	-	0%	4.0%	0%	3.9%	-	6.8%	0%	0%	6.4 %	-	4.1%
Pedestrians	-	-	-	-	0	-	-	-	-	0	-	-	-	-	3	
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	100%	-

^{*}Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Indiana Ave / 13th Ave (S Leg) - TMC

Tue Aug 25, 2020 AM Peak (7:15 AM - 8:15 AM)

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks, Pedestrians)

All Movements

ID: 776046, Location: 41.914947, -88.30001



Provided by: Gewalt Hamilton Associates Inc. 625 Forest Edge Drive, Vernon Hills, IL, 60061, US

Leg	Indiana					Indiana	l				13th					
Dire ction	Eastbou	nd				Westbo	ound				Northbo	und				
Time	Т	R	U	App	Pe d*	L	T	U	App	Ped*	L	R	U	Арр	Pe d*	Int
2020-08-25 7:15AM	5	1	0	6	0	1	5	0	6	0	5	0	0	5	0	17
7:30AM	4	1	0	5	0	0	10	0	10	0	2	1	0	3	3	18
7:45AM	6	3	0	9	0	0	6	0	6	0	3	0	0	3	0	18
8:00AM	4	2	0	6	0	0	4	0	4	0	5	0	0	5	0	15
Total	19	7	0	26	0	1	25	0	26	0	15	1	0	16	3	68
% Approach	73.1%	26.9%	0%	-	-	3.8%	96.2%	0%	-	-	93.8%	6.3%	0%	-	-	-
% Total	27.9%	10.3%	0%	38.2%	-	1.5%	36.8%	0%	38.2%	-	22.1%	1.5%	0%	23.5%	-	-
PHF	0.792	0.583	-	0.722	-	0.250	0.625	-	0.650	-	0.750	0.250	-	0.800	-	0.944
Lights	17	6	0	23	-	1	20	0	21	-	12	1	0	13	-	57
% Lights	89.5%	85.7%	0%	88.5%	-	100%	80.0%	0%	80.8%	-	80.0%	100%	0%	81.3%	-	83.8%
Artic ulate d Truc ks	0	0	0	0	-	0	2	0	2	-	0	0	0	0	-	2
% Articulated Trucks	0%	0%	0%	0 %	-	0%	8.0%	0%	7.7%	-	0%	0%	0%	0 %	-	2.9%
Buses and Single-Unit Trucks	2	1	0	3	-	0	3	0	3	-	3	0	0	3	-	9
% Buses and Single-Unit Trucks	10.5%	14.3%	0%	11.5%	-	0%	12.0%	0%	11.5%	-	20.0%	0%	0%	18.8%	-	13.2%
Pe de strians	-	-	-	-	0	-	-	-	-	0	-	-	-	-	3	
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	100%	-

^{*}Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Indiana Ave / 13th Ave (S Leg) - TMC

Tue Aug 25, 2020 PM Peak (4:30 PM - 5:30 PM) - Overall Peak Hour All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks, Pedestrians) All Movements



Provided by: Gewalt Hamilton Associates Inc. 625 Forest Edge Drive, Vernon Hills, IL, 60061, US

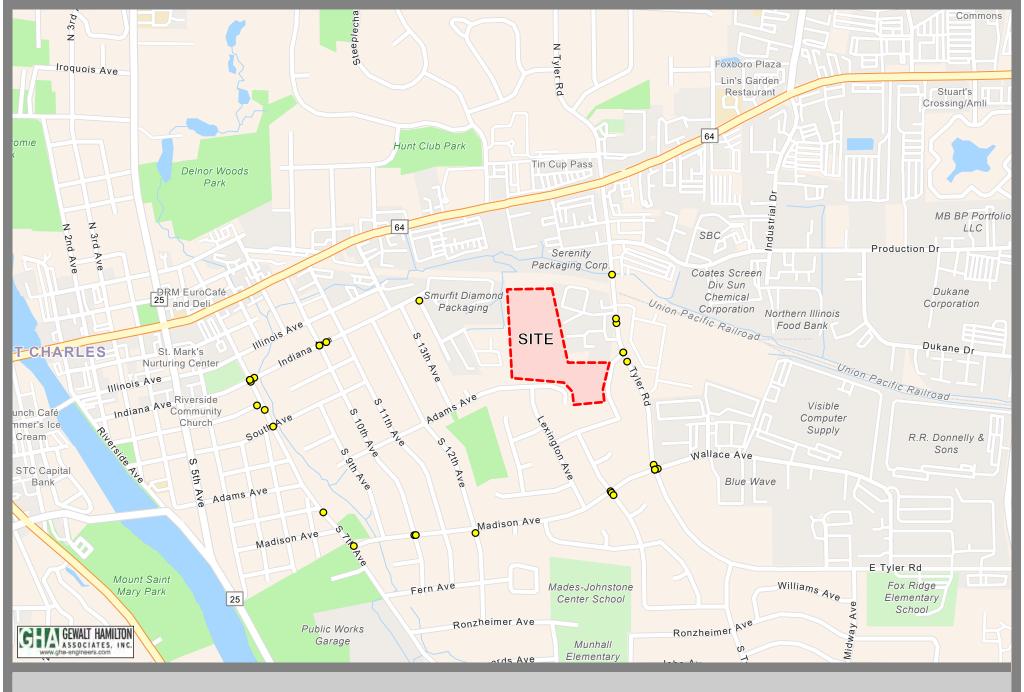
ID: 776046, Location: 41.914947, -88.30001

Leg	Indiana					Indiana	1				13th					
Direction	Eastbou	nd				Westbo	und				Northbo	und				
Time	T	R	U	App	Ped*	L	T	U	App	Pe d*	L	R	U	App	Pe d*	Int
2020-08-25 4:30PM	5	2	0	7	0	0	6	0	6	0	1	0	0	1	0	14
4:45PM	15	2	0	17	0	1	12	0	13	0	2	0	0	2	0	32
5:00PM	11	0	0	11	0	0	17	0	17	0	3	0	0	3	0	31
5:15PM	11	6	0	17	0	0	7	0	7	0	2	2	0	4	0	28
Total	42	10	0	52	0	1	42	0	43	0	8	2	0	10	0	105
% Approach	80.8%	19.2%	0%	-	-	2.3%	97.7%	0%	-	-	80.0%	20.0%	0%	-	-	-
% Total	40.0%	9.5%	0%	49.5%	-	1.0%	40.0%	0%	41.0%	-	7.6%	1.9%	0%	9.5%	-	-
PHF	0.700	0.417	-	0.765	-	0.250	0.618	-	0.632	-	0.667	0.250	-	0.625	-	0.820
Lights	40	10	0	50	-	1	42	0	43	-	8	2	0	10	-	103
% Lights	95.2%	100%	0%	96.2%	-	100%	100%	0%	100%	-	100%	100%	0%	100%	-	98.1%
Articulate d Trucks	1	0	0	1	-	0	0	0	0	-	0	0	0	0	-	1
% Articulated Trucks	2.4%	0%	0%	1.9 %	-	0%	0%	0%	0 %	-	0%	0%	0%	0 %	-	1.0%
Buses and Single-Unit Trucks	1	0	0	1	-	0	0	0	0	-	0	0	0	0	-	1
% Buses and Single-Unit Trucks	2.4%	0%	0%	1.9 %	-	0%	0%	0%	0 %	-	0%	0%	0%	0 %	-	1.0%
Pe de strians	-	-	-	-	0	-	-	-	-	0	-	-	-	-	0	
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-

^{*}Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Appendix D Crash Summary Map







IDOT Crash Data (2014-2018)

Proposed Residential Development St. Charles, IL

Appendix E CMAP Traffic Projections





Lynn M. Means, P.E., PTOE Senior Transportation Engineer Gewalt Hamilton Associates 625 Forest Edge Drive Vernon Hills, IL 60061 233 South Wacker Drive Suite 800 Chicago, Illinois 60606

312 454 0400 www.cmap.illinois.gov

June 19, 2020

Subject: Tyler Road - Madison Avenue - 7th Avenue - Main Street

IDOT

Dear Ms. Mean:

In response to a request made on your behalf and dated June 18, 2020, we have developed year 2050 average daily traffic (ADT) projections for the subject location.

ROAD SEGMENT	Current Volumes	Year 2050 ADT
Main St (IL 64) fr 7th Ave to Kirk Rd	29,500	36,700
Tyler Rd fr Main St to Madison Ave	7,350	8,400
Tyler Rd fr Madison Ave to Kirk Rd	4,700	7,300
Madison Ave from7th Ave to Tyler Rd	2,700	4,200
7th Ave from Main St to Madison Ave	3,400	3,600
7th Ave from Madison Ave to State St	3,850	4,100

Traffic projections are developed using existing ADT data provided in the request letter and the results from the March 2020 CMAP Travel Demand Analysis. The regional travel model uses CMAP 2050 socioeconomic projections and assumes the implementation of the ON TO 2050 Comprehensive Regional Plan for the Northeastern Illinois area. The provision of this data in support of your request does not constitute a CMAP endorsement of the proposed development or any subsequent developments.

If you have any questions, please call me at (312) 386-8806.

Sincerely,

Jose Rodriguez, PTP, AICP

Senior Planner, Research & Analysis

cc: Quigley (IDOT)

 $\verb|\2020_TrafficForecast| St. Charles \\ | ka-13-20 \\ | ka-13-20. \\ | docx$

Appendix F ITE Trip Generation Excerpts



Single-Family Detached Housing (210)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday

Setting/Location: General Urban/Suburban

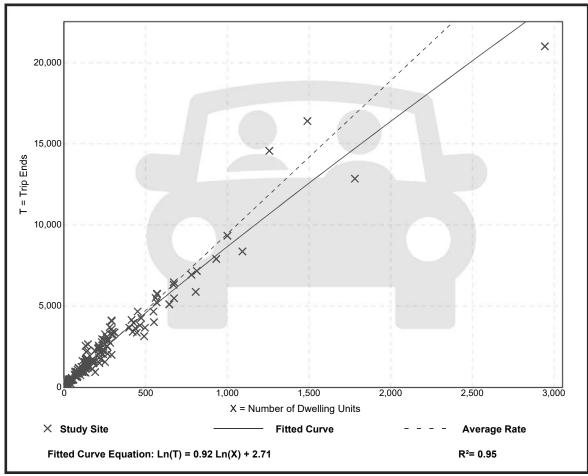
Number of Studies: 159 Avg. Num. of Dwelling Units: 264

Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
9.44	4.81 - 19.39	2.10

Data Plot and Equation



Trip Gen Manual, 10th Ed + Supplement • Institute of Transportation Engineers

Single-Family Detached Housing (210)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

Peak Hour of Adjacent Street Traffic, One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

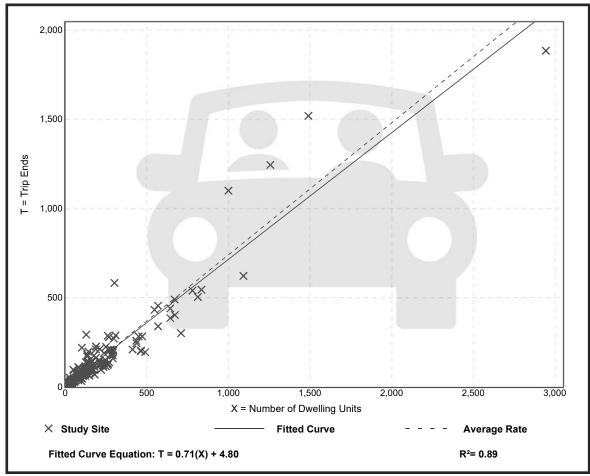
Number of Studies: 173 Avg. Num. of Dwelling Units: 219

Directional Distribution: 25% entering, 75% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.74	0.33 - 2.27	0.27

Data Plot and Equation



Trip Gen Manual, 10th Ed + Supplement • Institute of Transportation Engineers

Single-Family Detached Housing (210)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

Peak Hour of Adjacent Street Traffic, One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

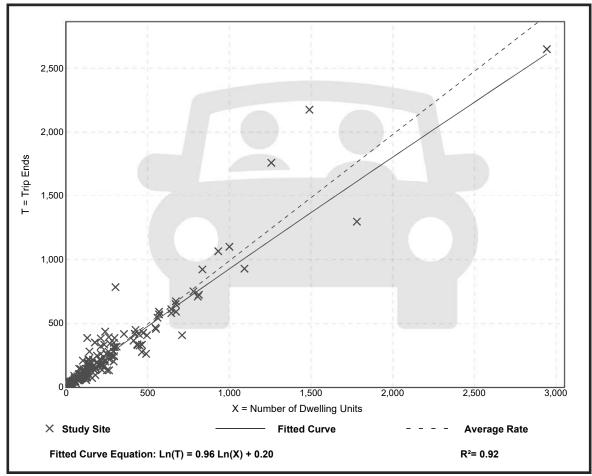
Number of Studies: 190 Avg. Num. of Dwelling Units: 242

Directional Distribution: 63% entering, 37% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.99	0.44 - 2.98	0.31

Data Plot and Equation



Trip Gen Manual, 10th Ed + Supplement • Institute of Transportation Engineers

Senior Adult Housing - Detached (251)

Vehicle Trip Ends vs: Dwelling Units On a: Weekday

Setting/Location: General Urban/Suburban

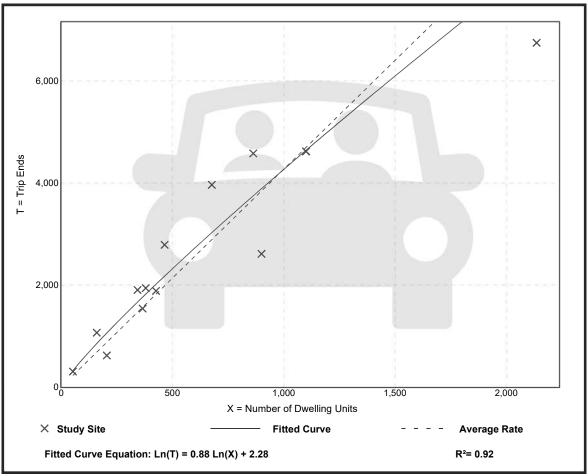
Number of Studies: 14 Avg. Num. of Dwelling Units: 655

Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
4.27	2.90 - 6.66	1.11

Data Plot and Equation



Trip Gen Manual, 10th Ed + Supplement • Institute of Transportation Engineers

Senior Adult Housing - Detached

(251)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

Peak Hour of Adjacent Street Traffic, One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

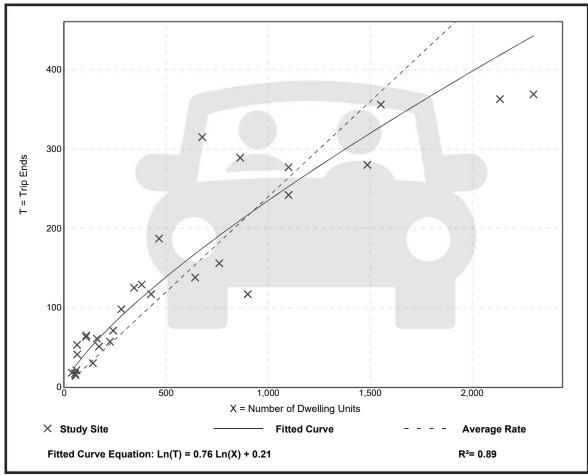
Number of Studies: 29 Avg. Num. of Dwelling Units: 583

Directional Distribution: 33% entering, 67% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.24	0.13 - 0.84	0.10

Data Plot and Equation



Trip Gen Manual, 10th Ed + Supplement ● Institute of Transportation Engineers

Senior Adult Housing - Detached (251)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

Peak Hour of Adjacent Street Traffic, One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

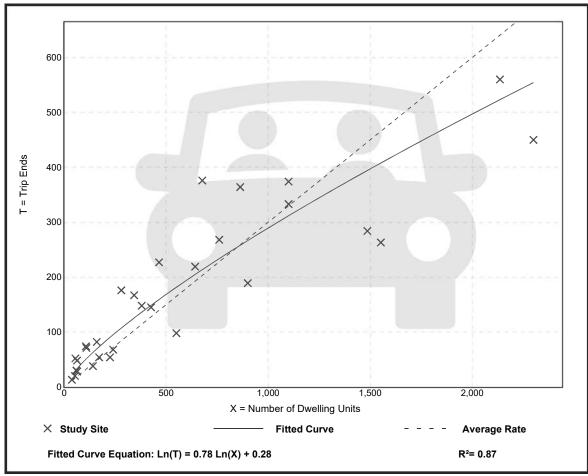
Number of Studies: 30 Avg. Num. of Dwelling Units: 582

Directional Distribution: 61% entering, 39% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.30	0.17 - 0.95	0.13

Data Plot and Equation

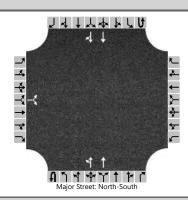


Trip Gen Manual, 10th Ed + Supplement • Institute of Transportation Engineers

Appendix GCapacity Analysis Worksheets

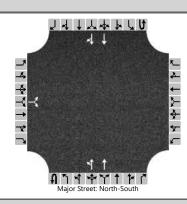


HCS7 Two-Way Stop-Control Report											
General Information Site Information											
Analyst	LMM	Intersection	Munhall Ave / Tyler Rd								
Agency/Co.	GHA	Jurisdiction	Local								
Date Performed	9/2/2020	East/West Street	Munhall Ave								
Analysis Year	2020	North/South Street	Tyler Rd								
Time Analyzed	Existing AM	Peak Hour Factor	0.84								
Intersection Orientation	North-South	Analysis Time Period (hrs)	0.25								
Project Description	5718.900										



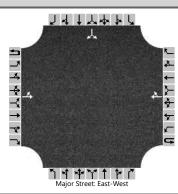
Vehicle Volumes and Adj	ustme	nts														
Approach		Eastb	ound			Westbound				North	bound			South	bound	
Movement	U	L	Т	R	U	L	Т	R	U	L	Т	R	U	L	Т	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		0	1	0		0	0	0	0	0	2	0	0	0	2	0
Configuration			LR							LT	Т				Т	TR
Volume (veh/h)		18		1						1	349				290	6
Percent Heavy Vehicles (%)		8		0						0						
Proportion Time Blocked																
Percent Grade (%)		(0													
Right Turn Channelized																
Median Type Storage				Undi	vided											
Critical and Follow-up Ho	eadwa	ys														
Base Critical Headway (sec)		7.5		6.9						4.1						
Critical Headway (sec)		6.96		6.90						4.10						
Base Follow-Up Headway (sec)		3.5		3.3						2.2						
Follow-Up Headway (sec)		3.58		3.30						2.20						
Delay, Queue Length, and	d Leve	l of S	ervice													
Flow Rate, v (veh/h)			23							1						
Capacity, c (veh/h)			456							1218						
v/c Ratio			0.05							0.00						
95% Queue Length, Q ₉₅ (veh)			0.2							0.0						
Control Delay (s/veh)			13.3							8.0						
Level of Service (LOS)			В							А						
Approach Delay (s/veh)		13.3								0.0						
Approach LOS		I	В													

HCS7 Two-Way Stop-Control Report											
General Information											
Analyst	LMM	Intersection	Munhall Ave / Tyler Rd								
Agency/Co.	GHA	Jurisdiction	Local								
Date Performed	9/2/2020	East/West Street	Munhall Ave								
Analysis Year	2020	North/South Street	Tyler Rd								
Time Analyzed	Existing PM	Peak Hour Factor	0.93								
Intersection Orientation	North-South	Analysis Time Period (hrs)	0.25								
Project Description	5718.900										



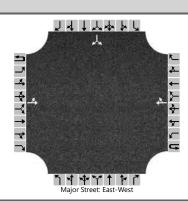
Approach		Eastb	ound		Westbound					North	bound			South	bound	
Movement	U	L	Т	R	U	L	Т	R	U	L	Т	R	U	L	Т	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		0	1	0		0	0	0	0	0	2	0	0	0	2	0
Configuration			LR							LT	Т				Т	TR
Volume (veh/h)		24		1						1	400				343	33
Percent Heavy Vehicles (%)		0		0						0						
Proportion Time Blocked																
Percent Grade (%)		(0													
Right Turn Channelized																
Median Type Storage				Undi	vided											
Critical and Follow-up H	eadwa	ys														
Base Critical Headway (sec)		7.5		6.9						4.1						
Critical Headway (sec)		6.80		6.90						4.10						
Base Follow-Up Headway (sec)		3.5		3.3						2.2						
Follow-Up Headway (sec)		3.50		3.30						2.20						
Delay, Queue Length, an	d Leve	l of S	ervice													
Flow Rate, v (veh/h)	Τ		27							1						
Capacity, c (veh/h)			442							1165						
v/c Ratio			0.06							0.00						
95% Queue Length, Q ₉₅ (veh)			0.2							0.0						
Control Delay (s/veh)			13.7							8.1						
Level of Service (LOS)			В							А						
Approach Delay (s/veh)		13.7					_	-		0	.0	-			•	
Approach LOS			В													

HCS7 Two-Way Stop-Control Report											
General Information Site Information											
Analyst	LMM	Intersection	Indiana/13th (N. Leg)								
Agency/Co.	GHA	Jurisdiction	Local								
Date Performed	9/2/2020	East/West Street	Indiana Ave								
Analysis Year	2020	North/South Street	13th Ave (N Leg)								
Time Analyzed	Existing AM	Peak Hour Factor	0.76								
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25								
Project Description	5718.900										



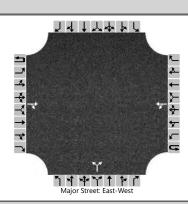
Vehicle Volumes and Ad	justme	nts															
Approach	T	Eastb	oound			Westbound				North	bound			South	bound		
Movement	U	L	Т	R	U	L	Т	R	U	L	Т	R	U	L	Т	R	
Priority	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12	
Number of Lanes	0	0	1	0	0	0	1	0		0	0	0		0	1	0	
Configuration		LT						TR							LR		
Volume (veh/h)		26	15				30	20						21		20	
Percent Heavy Vehicles (%)		5												6		6	
Proportion Time Blocked																	
Percent Grade (%)															0		
Right Turn Channelized																	
Median Type Storage				Undi	ivided												
Critical and Follow-up H	eadwa	ys															
Base Critical Headway (sec)	T	4.1												7.1		6.2	
Critical Headway (sec)		4.15												6.46		6.26	
Base Follow-Up Headway (sec)		2.2												3.5		3.3	
Follow-Up Headway (sec)		2.25											3.55 3.3				
Delay, Queue Length, an	d Leve	l of S	ervice	•													
Flow Rate, v (veh/h)		34													54		
Capacity, c (veh/h)		1517													903		
v/c Ratio		0.02													0.06		
95% Queue Length, Q ₉₅ (veh)		0.1													0.2		
Control Delay (s/veh)		7.4													9.2		
Level of Service (LOS)		А													А		
Approach Delay (s/veh)		4	.8										9.2				
Approach LOS														,	A		

HCS7 Two-Way Stop-Control Report											
General Information Site Information											
Analyst	LMM	Intersection	Indiana/13th (N. Leg)								
Agency/Co.	GHA	Jurisdiction	Local								
Date Performed	9/2/2020	East/West Street	Indiana Ave								
Analysis Year	2020	North/South Street	13th Ave (N Leg)								
Time Analyzed	Existing PM	Peak Hour Factor	0.87								
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25								
Project Description	5718.900										



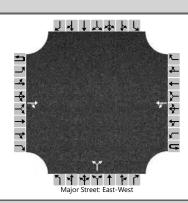
Vehicle Volumes and Adj	justme	nts														
Approach		Eastb	ound			Westbound				North	bound			South	bound	
Movement	U	L	Т	R	U	L	Т	R	U	L	Т	R	U	L	Т	R
Priority	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12
Number of Lanes	0	0	1	0	0	0	1	0		0	0	0		0	1	0
Configuration		LT						TR							LR	
Volume (veh/h)		15	20				38	25						45		31
Percent Heavy Vehicles (%)		0												3		0
Proportion Time Blocked																
Percent Grade (%)															0	
Right Turn Channelized																
Median Type Storage				Undi	vided											
Critical and Follow-up H	eadwa	ys														
Base Critical Headway (sec)		4.1												7.1		6.2
Critical Headway (sec)		4.10												6.43		6.20
Base Follow-Up Headway (sec)		2.2												3.5		3.3
Follow-Up Headway (sec)		2.20												3.53		3.30
Delay, Queue Length, an	d Leve	l of Se	ervice													
Flow Rate, v (veh/h)		17													87	
Capacity, c (veh/h)		1540													922	
v/c Ratio		0.01													0.09	
95% Queue Length, Q ₉₅ (veh)		0.0													0.3	
Control Delay (s/veh)		7.4													9.3	
Level of Service (LOS)		A													Α	
Approach Delay (s/veh)		3.2												9.3		
Approach LOS											А					

HCS7 Two-Way Stop-Control Report											
General Information		Site Information	e Information								
Analyst	LMM	Intersection	Indiana/13th (S Leg)								
Agency/Co.	GHA	Jurisdiction	Local								
Date Performed	9/2/2020	East/West Street	Indiana Ave								
Analysis Year	2020	North/South Street	13th Stree (S Leg)								
Time Analyzed	Existing AM	Peak Hour Factor	0.94								
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25								
Project Description	5718.900										



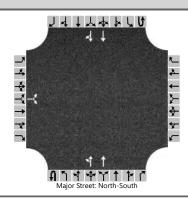
Vehicle Volumes and Ad	justme	nts														
Approach	T	Eastb	ound			Westl	oound			North	bound			South	bound	
Movement	U	L	Т	R	U	L	Т	R	U	L	Т	R	U	L	Т	R
Priority	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12
Number of Lanes	0	0	1	0	0	0	1	0		0	1	0		0	0	0
Configuration				TR		LT					LR					
Volume (veh/h)			26	10		1	31			19		1				
Percent Heavy Vehicles (%)						0				20		0				
Proportion Time Blocked																
Percent Grade (%))					
Right Turn Channelized																
Median Type Storage				Undi	ivided											
Critical and Follow-up H	eadwa	ys														
Base Critical Headway (sec)	T					4.1				7.1		6.2				
Critical Headway (sec)						4.10				6.60		6.20				
Base Follow-Up Headway (sec)						2.2				3.5		3.3				
Follow-Up Headway (sec)						2.20				3.68		3.30				
Delay, Queue Length, an	d Leve	l of S	ervice													
Flow Rate, v (veh/h)	Т					1					21					
Capacity, c (veh/h)						1585					900					
v/c Ratio						0.00					0.02					
95% Queue Length, Q ₉₅ (veh)						0.0					0.1					
Control Delay (s/veh)						7.3					9.1					
Level of Service (LOS)						A			A							
Approach Delay (s/veh)		0.2							9.1							
Approach LOS									A							

HCS7 Two-Way Stop-Control Report										
General Information Site Information										
Analyst	LMM	Intersection	Indiana/13th (S Leg)							
Agency/Co.	GHA	Jurisdiction	Local							
Date Performed	9/2/2020	East/West Street	Indiana Ave							
Analysis Year	2020	North/South Street	13th Stree (S Leg)							
Time Analyzed	Existing PM	Peak Hour Factor	0.82							
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25							
Project Description	5718.900									



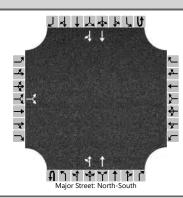
Vehicle Volumes and Ad	justme	nts														
Approach		Eastb	ound			Westl	oound			North	bound			South	bound	
Movement	U	L	Т	R	U	L	Т	R	U	L	T	R	U	L	Т	R
Priority	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12
Number of Lanes	0	0	1	0	0	0	1	0		0	1	0		0	0	0
Configuration				TR		LT					LR					
Volume (veh/h)			53	12		1	53			10		3				
Percent Heavy Vehicles (%)						0				0		0				
Proportion Time Blocked																
Percent Grade (%)										()					
Right Turn Channelized																
Median Type Storage				Undi	vided											
Critical and Follow-up H	eadwa	ys														
Base Critical Headway (sec)						4.1				7.1		6.2				
Critical Headway (sec)						4.10				6.40		6.20				
Base Follow-Up Headway (sec)						2.2				3.5		3.3				
Follow-Up Headway (sec)						2.20				3.50		3.30				
Delay, Queue Length, an	d Leve	l of S	ervice													
Flow Rate, v (veh/h)						1					16					
Capacity, c (veh/h)						1532					886					
v/c Ratio						0.00					0.02					
95% Queue Length, Q ₉₅ (veh)						0.0					0.1					
Control Delay (s/veh)						7.4					9.1					
Level of Service (LOS)						Α			A							
Approach Delay (s/veh)		0.1							9.1							
Approach LOS									A							

HCS7 Two-Way Stop-Control Report										
General Information Site Information										
Analyst	LMM	Intersection	Munhall Ave / Tyler Rd							
Agency/Co.	GHA	Jurisdiction	Local							
Date Performed	9/2/2020	East/West Street	Munhall Ave							
Analysis Year	2027	North/South Street	Tyler Rd							
Time Analyzed	NoBuild AM	Peak Hour Factor	0.84							
Intersection Orientation	North-South	Analysis Time Period (hrs)	0.25							
Project Description	5718.900									



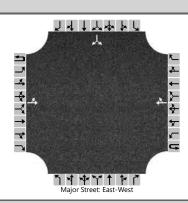
Approach		Eastb	ound			Westl	oound			North	bound			South	bound		
Movement	U	L	Т	R	U	L	Т	R	U	L	Т	R	U	L	Т	R	
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6	
Number of Lanes		0	1	0		0	0	0	0	0	2	0	0	0	2	0	
Configuration			LR							LT	Т				Т	TR	
Volume (veh/h)		18		1						1	366				304	6	
Percent Heavy Vehicles (%)		8		0						0							
Proportion Time Blocked																	
Percent Grade (%)			0														
Right Turn Channelized																	
Median Type Storage				Undi	vided												
Critical and Follow-up H	eadwa	ys															
Base Critical Headway (sec)		7.5		6.9						4.1							
Critical Headway (sec)		6.96		6.90						4.10							
Base Follow-Up Headway (sec)		3.5		3.3						2.2							
Follow-Up Headway (sec)		3.58		3.30						2.20							
Delay, Queue Length, an	d Leve	l of S	ervice														
Flow Rate, v (veh/h)			23							1							
Capacity, c (veh/h)			438							1201							
v/c Ratio			0.05							0.00							
95% Queue Length, Q ₉₅ (veh)			0.2							0.0							
Control Delay (s/veh)		13.7								8.0							
Level of Service (LOS)			В						A								
Approach Delay (s/veh)		13.7							0.0								
Approach LOS			В						Î								

HCS7 Two-Way Stop-Control Report										
General Information Site Information										
Analyst	LMM	Intersection	Munhall Ave / Tyler Rd							
Agency/Co.	GHA	Jurisdiction	Local							
Date Performed	9/2/2020	East/West Street	Munhall Ave							
Analysis Year	2027	North/South Street	Tyler Rd							
Time Analyzed	NoBuild PM	Peak Hour Factor	0.93							
Intersection Orientation	North-South	Analysis Time Period (hrs)	0.25							
Project Description	5718.900									



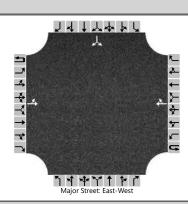
Vehicle Volumes and Ad	iustme	nts														
Approach			oound		I	Westl	bound		I	North	bound		I	South	bound	
Movement	U	L	Т	R	U	L	Т	R	U	L	Т	R	U	L	Т	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		0	1	0		0	0	0	0	0	2	0	0	0	2	0
Configuration			LR							LT	Т				Т	TR
Volume (veh/h)		24		1						1	418				360	33
Percent Heavy Vehicles (%)		0		0						0						
Proportion Time Blocked																
Percent Grade (%)			0													
Right Turn Channelized																
Median Type Storage				Undi	vided											
Critical and Follow-up H	eadwa	ys							•							
Base Critical Headway (sec)	\top	7.5		6.9						4.1			П			
Critical Headway (sec)		6.80		6.90						4.10						
Base Follow-Up Headway (sec)		3.5		3.3						2.2						
Follow-Up Headway (sec)		3.50		3.30						2.20						
Delay, Queue Length, an	d Leve	of S	ervice													
Flow Rate, v (veh/h)	\top		27							1						
Capacity, c (veh/h)			425							1147						
v/c Ratio			0.06							0.00						
95% Queue Length, Q ₉₅ (veh)			0.2							0.0						
Control Delay (s/veh)		14.0							8.1							
Level of Service (LOS)			В						A							
Approach Delay (s/veh)		14.0							0.0							
Approach LOS			В													

HCS7 Two-Way Stop-Control Report										
General Information Site Information										
Analyst	LMM	Intersection	Indiana/13th (N. Leg)							
Agency/Co.	GHA	Jurisdiction	Local							
Date Performed	9/2/2020	East/West Street	Indiana Ave							
Analysis Year	2027	North/South Street	13th Ave (N Leg)							
Time Analyzed	NoBuild AM	Peak Hour Factor	0.76							
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25							
Project Description	5718.900									



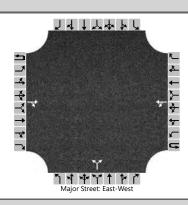
Vehicle Volumes and Adj	justme	nts															
Approach		Eastb	ound			Westl	oound			North	bound			Southbound			
Movement	U	L	Т	R	U	L	Т	R	U	L	Т	R	U	L	Т	R	
Priority	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12	
Number of Lanes	0	0	1	0	0	0	1	0		0	0	0		0	1	0	
Configuration		LT						TR							LR		
Volume (veh/h)		27	15				31	21						22		21	
Percent Heavy Vehicles (%)		5												6		6	
Proportion Time Blocked																	
Percent Grade (%)														-	0		
Right Turn Channelized																	
Median Type Storage				Undi	vided												
Critical and Follow-up H	eadwa	ys															
Base Critical Headway (sec)		4.1												7.1		6.2	
Critical Headway (sec)		4.15												6.46		6.26	
Base Follow-Up Headway (sec)		2.2												3.5		3.3	
Follow-Up Headway (sec)		2.25												3.55		3.35	
Delay, Queue Length, an	d Leve	l of Se	ervice														
Flow Rate, v (veh/h)		36													57		
Capacity, c (veh/h)		1514													898		
v/c Ratio		0.02													0.06		
95% Queue Length, Q ₉₅ (veh)		0.1													0.2		
Control Delay (s/veh)		7.4													9.3		
Level of Service (LOS)		А												Ì	Α		
Approach Delay (s/veh)		4.8				•							9.3				
Approach LOS														,	A		

HCS7 Two-Way Stop-Control Report										
General Information Site Information										
Analyst	LMM	Intersection	Indiana/13th (N. Leg)							
Agency/Co.	GHA	Jurisdiction	Local							
Date Performed	9/2/2020	East/West Street	Indiana Ave							
Analysis Year	2027	North/South Street	13th Ave (N Leg)							
Time Analyzed	NoBuild PM	Peak Hour Factor	0.87							
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25							
Project Description	5718.900									



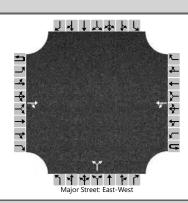
Vehicle Volumes and Adj	ustme	nts														
Approach		Eastb	ound			Westl	oound			North	bound			South	bound	
Movement	U	L	Т	R	U	L	Т	R	U	L	Т	R	U	L	Т	R
Priority	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12
Number of Lanes	0	0	1	0	0	0	1	0		0	0	0		0	1	0
Configuration		LT						TR							LR	
Volume (veh/h)		15	21				39	26						46		32
Percent Heavy Vehicles (%)		0												3		0
Proportion Time Blocked																
Percent Grade (%)														()	
Right Turn Channelized																
Median Type Storage				Undi	vided											
Critical and Follow-up H	eadwa	ys														
Base Critical Headway (sec)		4.1												7.1		6.2
Critical Headway (sec)		4.10												6.43		6.20
Base Follow-Up Headway (sec)		2.2												3.5		3.3
Follow-Up Headway (sec)		2.20												3.53		3.30
Delay, Queue Length, an	d Leve	l of Se	ervice													
Flow Rate, v (veh/h)		17													90	
Capacity, c (veh/h)		1537													920	
v/c Ratio		0.01													0.10	
95% Queue Length, Q ₉₅ (veh)		0.0													0.3	
Control Delay (s/veh)		7.4													9.3	
Level of Service (LOS)		А													Α	
Approach Delay (s/veh)		3.1											9.3			
Approach LOS													,	4		

HCS7 Two-Way Stop-Control Report										
General Information Site Information										
Analyst	LMM	Intersection	Indiana/13th (S Leg)							
Agency/Co.	GHA	Jurisdiction	Local							
Date Performed	9/2/2020	East/West Street	Indiana Ave							
Analysis Year	2027	North/South Street	13th Stree (S Leg)							
Time Analyzed	NoBuild AM	Peak Hour Factor	0.94							
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25							
Project Description	5718.900									



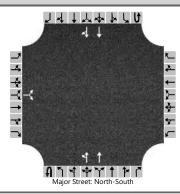
Approach		Eastb	ound			Westk	oound			North	bound			South	bound	
Movement	U	L	Т	R	U	L	Т	R	U	L	Т	R	U	L	Т	R
Priority	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12
Number of Lanes	0	0	1	0	0	0	1	0		0	1	0		0	0	0
Configuration				TR		LT					LR					
Volume (veh/h)			27	10		1	32			20		1				
Percent Heavy Vehicles (%)						0				20		0				
Proportion Time Blocked																
Percent Grade (%)										()					
Right Turn Channelized																
Median Type Storage				Undi	vided											
Critical and Follow-up H	eadwa	dways														
Base Critical Headway (sec)						4.1				7.1		6.2				
Critical Headway (sec)						4.10				6.60		6.20				
Base Follow-Up Headway (sec)						2.2				3.5		3.3				
Follow-Up Headway (sec)						2.20				3.68		3.30				
Delay, Queue Length, an	d Leve	l of Se	ervice													
Flow Rate, v (veh/h)						1					22					
Capacity, c (veh/h)						1584					897					
v/c Ratio						0.00					0.02					
95% Queue Length, Q ₉₅ (veh)						0.0					0.1					
Control Delay (s/veh)						7.3					9.1					
Level of Service (LOS)						Α					А					
Approach Delay (s/veh)		0.2						9.1								
Approach LOS									A							

HCS7 Two-Way Stop-Control Report										
General Information Site Information										
Analyst	LMM	Intersection	Indiana/13th (S Leg)							
Agency/Co.	GHA	Jurisdiction	Local							
Date Performed	9/2/2020	East/West Street	Indiana Ave							
Analysis Year	2027	North/South Street	13th Stree (S Leg)							
Time Analyzed	NoBuild PM	Peak Hour Factor	0.82							
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25							
Project Description 5718.900										



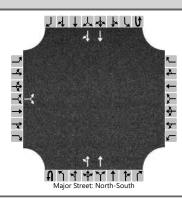
Vehicle Volumes and Adj	ustme	nts														
Approach		Eastb	ound			Westl	oound			North	bound			South	bound	
Movement	U	L	Т	R	U	L	Т	R	U	L	Т	R	U	L	Т	R
Priority	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12
Number of Lanes	0	0	1	0	0	0	1	0		0	1	0		0	0	0
Configuration				TR		LT					LR					
Volume (veh/h)			55	12		1	55			10		3				
Percent Heavy Vehicles (%)						0				0		0				
Proportion Time Blocked																
Percent Grade (%)										()					
Right Turn Channelized																
Median Type Storage				Undi	vided											
Critical and Follow-up H	eadwa	adways														
Base Critical Headway (sec)						4.1				7.1		6.2				
Critical Headway (sec)						4.10				6.40		6.20				
Base Follow-Up Headway (sec)						2.2				3.5		3.3				
Follow-Up Headway (sec)						2.20				3.50		3.30				
Delay, Queue Length, an	d Leve	l of S	ervice													
Flow Rate, v (veh/h)						1					16					
Capacity, c (veh/h)						1528					881					
v/c Ratio						0.00					0.02					
95% Queue Length, Q ₉₅ (veh)						0.0					0.1					
Control Delay (s/veh)						7.4					9.2					
Level of Service (LOS)						A			A							
Approach Delay (s/veh)					0.1			9.2								
Approach LOS								А								

HCS7 Two-Way Stop-Control Report									
General Information		Site Information							
Analyst	LMM	Intersection	Munhall Ave / Tyler Rd						
Agency/Co.	GHA	Jurisdiction	Local						
Date Performed	9/2/2020	East/West Street	Munhall Ave						
Analysis Year	2027	North/South Street	Tyler Rd						
Time Analyzed	Total (Gate) AM	Peak Hour Factor	0.84						
Intersection Orientation	North-South	Analysis Time Period (hrs)	0.25						
Project Description	5718.900								



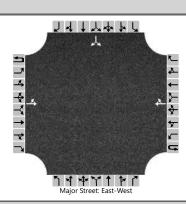
Vehicle Volumes and Ad	iustme	nts														
Approach			ound		l	Westl	oound		Π	North	bound		l	South	bound	
Movement	U	L	Т	R	U	L	Т	R	U	L	Т	R	U	L	Т	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		0	1	0		0	0	0	0	0	2	0	0	0	2	0
Configuration			LR							LT	Т				Т	TR
Volume (veh/h)		33		9						3	366				304	12
Percent Heavy Vehicles (%)		8		0						0						
Proportion Time Blocked																
Percent Grade (%)			0													
Right Turn Channelized																
Median Type Storage				Undi	vided											
Critical and Follow-up H	eadwa	ys							•							
Base Critical Headway (sec)	Т	7.5		6.9						4.1						
Critical Headway (sec)		6.96		6.90						4.10						
Base Follow-Up Headway (sec)		3.5		3.3						2.2						
Follow-Up Headway (sec)		3.58		3.30						2.20						
Delay, Queue Length, an	d Leve	of S	ervice													
Flow Rate, v (veh/h)	Т		50							4						
Capacity, c (veh/h)			470							1193						
v/c Ratio			0.11							0.00						
95% Queue Length, Q ₉₅ (veh)			0.4							0.0						
Control Delay (s/veh)			13.6							8.0						
Level of Service (LOS)			В							А						
Approach Delay (s/veh)		13.6							0.1							
Approach LOS		В														

HCS7 Two-Way Stop-Control Report										
General Information Site Information										
Analyst	LMM	Intersection	Munhall Ave / Tyler Rd							
Agency/Co.	GHA	Jurisdiction	Local							
Date Performed	9/2/2020	East/West Street	Munhall Ave							
Analysis Year	2027	North/South Street	Tyler Rd							
Time Analyzed	Total (Gate) PM	Peak Hour Factor	0.93							
Intersection Orientation	North-South	Analysis Time Period (hrs)	0.25							
Project Description 5718.900										



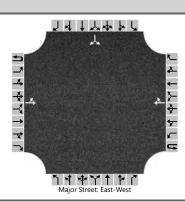
Vehicle Volumes and Ad	justme	nts														
Approach			oound			Westl	bound			North	bound			South	bound	
Movement	U	L	Т	R	U	L	Т	R	U	L	T	R	U	L	Т	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		0	1	0		0	0	0	0	0	2	0	0	0	2	0
Configuration			LR							LT	Т				Т	TR
Volume (veh/h)		34		5						9	418				360	50
Percent Heavy Vehicles (%)		0		0						0						
Proportion Time Blocked																
Percent Grade (%)			0													
Right Turn Channelized																
Median Type Storage				Undi	vided											
Critical and Follow-up H	eadwa	ys							•							
Base Critical Headway (sec)	Т	7.5		6.9						4.1						
Critical Headway (sec)		6.80		6.90						4.10						
Base Follow-Up Headway (sec)		3.5		3.3						2.2						
Follow-Up Headway (sec)		3.50		3.30						2.20						
Delay, Queue Length, an	d Leve	l of S	ervice													
Flow Rate, v (veh/h)	Т		42							10						
Capacity, c (veh/h)			424							1130						
v/c Ratio			0.10							0.01						
95% Queue Length, Q ₉₅ (veh)			0.3							0.0						
Control Delay (s/veh)			14.4							8.2						
Level of Service (LOS)			В							А						
Approach Delay (s/veh)		14.4								0	.2					
Approach LOS		В														

HCS7 Two-Way Stop-Control Report										
General Information Site Information										
Analyst	LMM	Intersection	Indiana/13th (N. Leg)							
Agency/Co.	GHA	Jurisdiction	Local							
Date Performed	9/2/2020	East/West Street	Indiana Ave							
Analysis Year	2027	North/South Street	13th Ave (N Leg)							
Time Analyzed	Total (Gate) AM	Peak Hour Factor	0.76							
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25							
Project Description 5718.900										



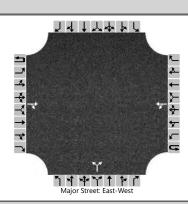
Vehicle Volumes and Adj	justme	nts														
Approach		Eastb	ound			Westl	oound			North	bound			South	bound	
Movement	U	L	Т	R	U	L	Т	R	U	L	Т	R	U	L	Т	R
Priority	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12
Number of Lanes	0	0	1	0	0	0	1	0		0	0	0		0	1	0
Configuration		LT						TR							LR	
Volume (veh/h)		27	15				31	21						22		21
Percent Heavy Vehicles (%)		5												6		6
Proportion Time Blocked																
Percent Grade (%)															0	
Right Turn Channelized																
Median Type Storage				Undi	vided											
Critical and Follow-up H	eadwa	ys														
Base Critical Headway (sec)		4.1												7.1		6.2
Critical Headway (sec)		4.15												6.46		6.26
Base Follow-Up Headway (sec)		2.2												3.5		3.3
Follow-Up Headway (sec)		2.25												3.55		3.35
Delay, Queue Length, an	d Leve	l of S	ervice													
Flow Rate, v (veh/h)		36													57	
Capacity, c (veh/h)		1514													898	
v/c Ratio		0.02													0.06	
95% Queue Length, Q ₉₅ (veh)		0.1													0.2	
Control Delay (s/veh)		7.4													9.3	
Level of Service (LOS)		А												Α		
Approach Delay (s/veh)		4.8												9.3		
Approach LOS													A			

HCS7 Two-Way Stop-Control Report										
General Information Site Information										
Analyst	LMM	Intersection	Indiana/13th (N. Leg)							
Agency/Co.	GHA	Jurisdiction	Local							
Date Performed	9/2/2020	East/West Street	Indiana Ave							
Analysis Year	2027	North/South Street	13th Ave (N Leg)							
Time Analyzed	Total (Gate) PM	Peak Hour Factor	0.87							
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25							
Project Description	5718.900									



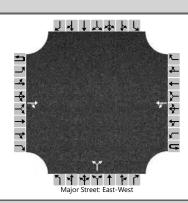
Approach		Eastb	ound			Westl	oound			North	bound			South	bound	
Movement	U	L	Т	R	U	L	Т	R	U	L	Т	R	U	L	Т	R
Priority	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12
Number of Lanes	0	0	1	0	0	0	1	0		0	0	0		0	1	0
Configuration		LT						TR							LR	
Volume (veh/h)		15	21				39	26						46		32
Percent Heavy Vehicles (%)		0												3		0
Proportion Time Blocked																
Percent Grade (%)							•						0			
Right Turn Channelized																
Median Type Storage		Undivided														
Critical and Follow-up H	eadwa	ys														
Base Critical Headway (sec)		4.1												7.1		6.2
Critical Headway (sec)		4.10												6.43		6.20
Base Follow-Up Headway (sec)		2.2												3.5		3.3
Follow-Up Headway (sec)		2.20												3.53		3.30
Delay, Queue Length, an	d Leve	l of Se	ervice	•												
Flow Rate, v (veh/h)		17													90	
Capacity, c (veh/h)		1537													920	
v/c Ratio		0.01													0.10	
95% Queue Length, Q ₉₅ (veh)		0.0													0.3	
Control Delay (s/veh)		7.4													9.3	
Level of Service (LOS)		А												Α		
Approach Delay (s/veh)		3.1											9.3			
Approach LOS													A			

HCS7 Two-Way Stop-Control Report										
General Information Site Information										
Analyst	LMM	Intersection	Indiana/13th (S Leg)							
Agency/Co.	GHA	Jurisdiction	Local							
Date Performed	9/2/2020	East/West Street	Indiana Ave							
Analysis Year	2027	North/South Street	13th Stree (S Leg)							
Time Analyzed	Total (Gate) AM	Peak Hour Factor	0.94							
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25							
Project Description 5718.900										



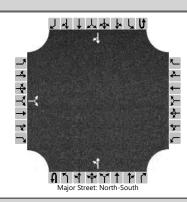
Vehicle Volumes and Adj	justme	nts														
Approach		Eastk	ound			Westl	oound			North	bound			South	bound	
Movement	U	L	Т	R	U	L	Т	R	U	L	Т	R	U	L	Т	R
Priority	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12
Number of Lanes	0	0	1	0	0	0	1	0		0	1	0		0	0	0
Configuration				TR		LT					LR					
Volume (veh/h)			27	10		1	32			20		1				
Percent Heavy Vehicles (%)						0				20		0				
Proportion Time Blocked																
Percent Grade (%)										()					
Right Turn Channelized																
Median Type Storage				Undi	vided											
Critical and Follow-up H	eadwa	adways														
Base Critical Headway (sec)						4.1				7.1		6.2				
Critical Headway (sec)						4.10				6.60		6.20				
Base Follow-Up Headway (sec)						2.2				3.5		3.3				
Follow-Up Headway (sec)						2.20				3.68		3.30				
Delay, Queue Length, an	d Leve	l of S	ervice													
Flow Rate, v (veh/h)						1					22					
Capacity, c (veh/h)						1584					897					
v/c Ratio						0.00					0.02					
95% Queue Length, Q ₉₅ (veh)						0.0					0.1					
Control Delay (s/veh)						7.3					9.1					
Level of Service (LOS)						A			А							
Approach Delay (s/veh)					0.2			9.1								
Approach LOS						A										

HCS7 Two-Way Stop-Control Report										
General Information		Site Information								
Analyst	LMM	Intersection	Indiana/13th (S Leg)							
Agency/Co.	GHA	Jurisdiction	Local							
Date Performed	9/2/2020	East/West Street	Indiana Ave							
Analysis Year	2027	North/South Street	13th Stree (S Leg)							
Time Analyzed	Total (Gate) PM	Peak Hour Factor	0.82							
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25							
Project Description 5718.900										



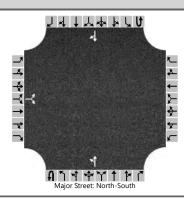
Vehicle Volumes and Adj	ustme	nts														
Approach		Eastb	ound			Westl	oound			North	bound			South	bound	
Movement	U	L	Т	R	U	L	Т	R	U	L	Т	R	U	L	Т	R
Priority	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12
Number of Lanes	0	0	1	0	0	0	1	0		0	1	0		0	0	0
Configuration				TR		LT					LR					
Volume (veh/h)			55	12		1	55			10		3				
Percent Heavy Vehicles (%)						0				0		0				
Proportion Time Blocked																
Percent Grade (%)										()					
Right Turn Channelized																
Median Type Storage				Undi	vided											
Critical and Follow-up Ho	adways															
Base Critical Headway (sec)						4.1				7.1		6.2				
Critical Headway (sec)						4.10				6.40		6.20				
Base Follow-Up Headway (sec)						2.2				3.5		3.3				
Follow-Up Headway (sec)						2.20				3.50		3.30				
Delay, Queue Length, and	d Leve	l of S	ervice													
Flow Rate, v (veh/h)						1					16					
Capacity, c (veh/h)						1528					881					
v/c Ratio						0.00					0.02					
95% Queue Length, Q ₉₅ (veh)						0.0					0.1					
Control Delay (s/veh)						7.4					9.2					
Level of Service (LOS)						A			A							
Approach Delay (s/veh)					0.1			9.2								
Approach LOS								А								

HCS7 Two-Way Stop-Control Report										
General Information		Site Information								
Analyst	LMM	Intersection	Munhall Ave/Site							
Agency/Co.	GHA	Jurisdiction	Local							
Date Performed	9/2/2020	East/West Street	Site							
Analysis Year	2027	North/South Street	Munhall Ave							
Time Analyzed	Total (Gated) AM	Peak Hour Factor	0.90							
Intersection Orientation	North-South	Analysis Time Period (hrs)	0.25							
Project Description	5718.900									



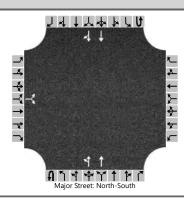
Vehicle Volumes and Adj	ustme	nts														
Approach		Eastk	ound			Westl	oound			North	bound			South	bound	
Movement	U	L	Т	R	U	L	Т	R	U	L	Т	R	U	L	Т	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		0	1	0		0	0	0	0	0	1	0	0	0	1	0
Configuration			LR							LT						TR
Volume (veh/h)		23		7						2	19				7	8
Percent Heavy Vehicles (%)		2		2						2						
Proportion Time Blocked																
Percent Grade (%)			0													
Right Turn Channelized																
Median Type Storage				Undi	vided											
Critical and Follow-up He	eadwa	ndways														
Base Critical Headway (sec)		7.1		6.2						4.1						
Critical Headway (sec)		6.42		6.22						4.12						
Base Follow-Up Headway (sec)		3.5		3.3						2.2						
Follow-Up Headway (sec)		3.52		3.32						2.22						
Delay, Queue Length, and	Leve	l of S	ervice													
Flow Rate, v (veh/h)			33							2						
Capacity, c (veh/h)			994							1601						
v/c Ratio			0.03							0.00						
95% Queue Length, Q ₉₅ (veh)			0.1							0.0						
Control Delay (s/veh)			8.7							7.3						
Level of Service (LOS)		A							А							
Approach Delay (s/veh)		8.7						0.7								
Approach LOS			Ą													

HCS7 Two-Way Stop-Control Report										
General Information		Site Information								
Analyst	LMM	Intersection	Munhall Ave/Site							
Agency/Co.	GHA	Jurisdiction	Local							
Date Performed	9/2/2020	East/West Street	Site							
Analysis Year	2027	North/South Street	Munhall Ave							
Time Analyzed	Total (Gated) PM	Peak Hour Factor	0.90							
Intersection Orientation	North-South	Analysis Time Period (hrs)	0.25							
Project Description 5718.900										



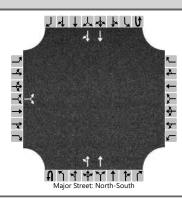
Approach		Eastb	ound			Westl	oound			North	bound			South	bound	
Movement	U	L	Т	R	U	L	Т	R	U	L	Т	R	U	L	Т	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		0	1	0		0	0	0	0	0	1	0	0	0	1	0
Configuration			LR							LT						TR
Volume (veh/h)		14		5						8	25				34	25
Percent Heavy Vehicles (%)		2		2						2						
Proportion Time Blocked																
Percent Grade (%)			0				•									
Right Turn Channelized																
Median Type Storage				Undi	vided											
Critical and Follow-up H	eadwa	ys														
Base Critical Headway (sec)		7.1		6.2						4.1						
Critical Headway (sec)		6.42		6.22						4.12						
Base Follow-Up Headway (sec)		3.5		3.3						2.2						
Follow-Up Headway (sec)		3.52		3.32						2.22						
Delay, Queue Length, an	d Leve	l of S	ervice													
Flow Rate, v (veh/h)			21							9						
Capacity, c (veh/h)			925							1536						
v/c Ratio			0.02							0.01						
95% Queue Length, Q ₉₅ (veh)			0.1							0.0						
Control Delay (s/veh)			9.0							7.4						
Level of Service (LOS)		A														
Approach Delay (s/veh)		9.0							1.8							
Approach LOS		А														

HCS7 Two-Way Stop-Control Report										
General Information		Site Information								
Analyst	LMM	Intersection	Munhall Ave / Tyler Rd							
Agency/Co.	GHA	Jurisdiction	Local							
Date Performed	9/2/2020	East/West Street	Munhall Ave							
Analysis Year	2027	North/South Street	Tyler Rd							
Time Analyzed	Total (Open) AM	Peak Hour Factor	0.84							
Intersection Orientation	North-South	Analysis Time Period (hrs)	0.25							
Project Description 5718.900										



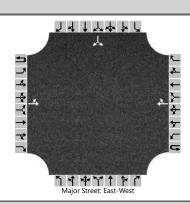
Approach		Eastb	ound			Westl	oound			North	bound			South	bound	
Movement	U	L	Т	R	U	L	Т	R	U	L	Т	R	U	L	Т	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		0	1	0		0	0	0	0	0	2	0	0	0	2	0
Configuration			LR							LT	Т				Т	TR
Volume (veh/h)		30		9						3	366				304	10
Percent Heavy Vehicles (%)		8		0						0						
Proportion Time Blocked																
Percent Grade (%)			0				•									
Right Turn Channelized																
Median Type Storage				Undi	vided											
Critical and Follow-up H	eadwa	ys														
Base Critical Headway (sec)		7.5		6.9						4.1						
Critical Headway (sec)		6.96		6.90						4.10						
Base Follow-Up Headway (sec)		3.5		3.3						2.2						
Follow-Up Headway (sec)		3.58		3.30						2.20						
Delay, Queue Length, an	d Leve	l of S	ervice													
Flow Rate, v (veh/h)			46							4						
Capacity, c (veh/h)			475							1196						
v/c Ratio			0.10							0.00						
95% Queue Length, Q ₉₅ (veh)			0.3							0.0						
Control Delay (s/veh)			13.4							8.0						
Level of Service (LOS)			В						A							
Approach Delay (s/veh)		13.4						0.1								
Approach LOS		В														

HCS7 Two-Way Stop-Control Report										
General Information		Site Information								
Analyst	LMM	Intersection	Munhall Ave / Tyler Rd							
Agency/Co.	GHA	Jurisdiction	Local							
Date Performed	9/2/2020	East/West Street	Munhall Ave							
Analysis Year	2027	North/South Street	Tyler Rd							
Time Analyzed	Total (Open) PM	Peak Hour Factor	0.93							
Intersection Orientation	North-South	Analysis Time Period (hrs)	0.25							
Project Description 5718.900										



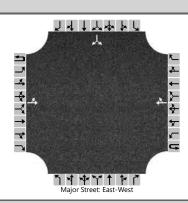
Approach		Eastb	ound			Westl	oound			North	bound			South	bound	
Movement	U	L	Т	R	U	L	Т	R	U	L	Т	R	U	L	Т	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		0	1	0		0	0	0	0	0	2	0	0	0	2	0
Configuration			LR							LT	Т				Т	TR
Volume (veh/h)		31		5						9	418				360	47
Percent Heavy Vehicles (%)		0		0						0						
Proportion Time Blocked																
Percent Grade (%)			0													
Right Turn Channelized																
Median Type Storage				Undi	vided											
Critical and Follow-up H	eadwa	ys														
Base Critical Headway (sec)		7.5		6.9						4.1						
Critical Headway (sec)		6.80		6.90						4.10						
Base Follow-Up Headway (sec)		3.5		3.3						2.2						
Follow-Up Headway (sec)		3.50		3.30						2.20						
Delay, Queue Length, an	d Leve	l of S	ervice	•												
Flow Rate, v (veh/h)			39							10						
Capacity, c (veh/h)			428							1133						
v/c Ratio			0.09							0.01						
95% Queue Length, Q ₉₅ (veh)			0.3							0.0						
Control Delay (s/veh)			14.3							8.2						
Level of Service (LOS)			В							А						
Approach Delay (s/veh)		14.3					0.2					-				
Approach LOS		В														

HCS7 Two-Way Stop-Control Report										
General Information		Site Information								
Analyst	LMM	Intersection	Indiana/13th (N. Leg)							
Agency/Co.	GHA	Jurisdiction	Local							
Date Performed	9/2/2020	East/West Street	Indiana Ave							
Analysis Year	2027	North/South Street	13th Ave (N Leg)							
Time Analyzed	Total (Open) AM	Peak Hour Factor	0.76							
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25							
Project Description	5718.900									



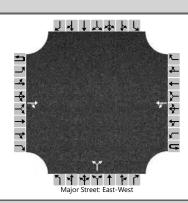
Vehicle Volumes and Adju	stme	nts														
Approach		Eastb	ound			Westl	oound			North	bound			South	bound	
Movement	U	L	Т	R	U	L	Т	R	U	L	Т	R	U	L	Т	R
Priority	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12
Number of Lanes	0	0	1	0	0	0	1	0		0	0	0		0	1	0
Configuration		LT						TR							LR	
Volume (veh/h)		27	16				33	23						24		21
Percent Heavy Vehicles (%)		5												6		6
Proportion Time Blocked																
Percent Grade (%)														()	
Right Turn Channelized																
Median Type Storage	Undivided															
Critical and Follow-up Hea	adwa	ys														
Base Critical Headway (sec)		4.1												7.1		6.2
Critical Headway (sec)		4.15												6.46		6.26
Base Follow-Up Headway (sec)		2.2												3.5		3.3
Follow-Up Headway (sec)		2.25											3.55 3.3			
Delay, Queue Length, and	Leve	of Se	ervice													
Flow Rate, v (veh/h)		36													59	
Capacity, c (veh/h)		1507													889	
v/c Ratio		0.02													0.07	
95% Queue Length, Q ₉₅ (veh)		0.1													0.2	
Control Delay (s/veh)		7.4													9.3	
Level of Service (LOS)	A														А	
Approach Delay (s/veh)	4.7										9.3					
Approach LOS											A					

HCS7 Two-Way Stop-Control Report										
General Information Site Information										
Analyst	LMM	Intersection	Indiana/13th (N. Leg)							
Agency/Co.	GHA	Jurisdiction	Local							
Date Performed	9/2/2020	East/West Street	Indiana Ave							
Analysis Year	2027	North/South Street	13th Ave (N Leg)							
Time Analyzed	Total (Open) PM	Peak Hour Factor	0.87							
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25							
Project Description	5718.900									



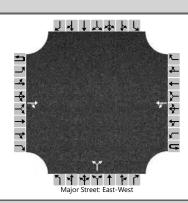
Vehicle Volumes and Adj	justme	nts														
Approach		Eastb	ound			Westl	oound			North	bound			South	bound	
Movement	U	L	Т	R	U	L	Т	R	U	L	Т	R	U	L	Т	R
Priority	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12
Number of Lanes	0	0	1	0	0	0	1	0		0	0	0		0	1	0
Configuration		LT						TR							LR	
Volume (veh/h)		15	23				41	28						47		32
Percent Heavy Vehicles (%)		0												3		0
Proportion Time Blocked																
Percent Grade (%)													-	0		
Right Turn Channelized																
Median Type Storage		Undivided														
Critical and Follow-up H	eadwa	ys														
Base Critical Headway (sec)		4.1												7.1		6.2
Critical Headway (sec)		4.10												6.43		6.20
Base Follow-Up Headway (sec)		2.2												3.5		3.3
Follow-Up Headway (sec)		2.20												3.53		3.30
Delay, Queue Length, an	d Leve	l of Se	ervice													
Flow Rate, v (veh/h)		17													91	
Capacity, c (veh/h)		1532													913	
v/c Ratio		0.01													0.10	
95% Queue Length, Q ₉₅ (veh)		0.0													0.3	
Control Delay (s/veh)		7.4													9.4	
Level of Service (LOS)		A													Α	
Approach Delay (s/veh)		3.0										9.4				
Approach LOS														,	A	

HCS7 Two-Way Stop-Control Report										
General Information Site Information										
Analyst	LMM	Intersection	Indiana/13th (S Leg)							
Agency/Co.	GHA	Jurisdiction	Local							
Date Performed	9/2/2020	East/West Street	Indiana Ave							
Analysis Year	2027	North/South Street	13th Stree (S Leg)							
Time Analyzed	Total (Open) AM	Peak Hour Factor	0.94							
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25							
Project Description	5718.900									



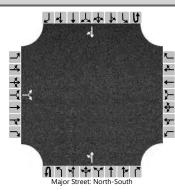
Vehicle Volumes and Ad	justme	nts														
Approach	T	Eastb	ound			Westl	oound			North	bound			South	bound	
Movement	U	L	Т	R	U	L	Т	R	U	L	Т	R	U	L	Т	R
Priority	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12
Number of Lanes	0	0	1	0	0	0	1	0		0	1	0		0	0	0
Configuration				TR		LT					LR					
Volume (veh/h)			30	10		1	36			20		1				
Percent Heavy Vehicles (%)						0				20		0				
Proportion Time Blocked																
Percent Grade (%))					
Right Turn Channelized																
Median Type Storage				Undi	ivided											
Critical and Follow-up H	eadwa	ys														
Base Critical Headway (sec)	T					4.1				7.1		6.2				
Critical Headway (sec)						4.10				6.60		6.20				
Base Follow-Up Headway (sec)						2.2				3.5		3.3				
Follow-Up Headway (sec)						2.20				3.68		3.30				
Delay, Queue Length, an	d Leve	l of S	ervice													
Flow Rate, v (veh/h)	T					1					22					
Capacity, c (veh/h)						1579					888					
v/c Ratio						0.00					0.03					
95% Queue Length, Q ₉₅ (veh)						0.0					0.1					
Control Delay (s/veh)						7.3					9.2					
Level of Service (LOS)						A			A							
Approach Delay (s/veh)		0.2				9.2						-				
Approach LOS						A										

HCS7 Two-Way Stop-Control Report										
General Information Site Information										
Analyst	LMM	Intersection	Indiana/13th (S Leg)							
Agency/Co.	GHA	Jurisdiction	Local							
Date Performed	9/2/2020	East/West Street	Indiana Ave							
Analysis Year	2027	North/South Street	13th Stree (S Leg)							
Time Analyzed	Total (Open) PM	Peak Hour Factor	0.82							
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25							
Project Description	5718.900									



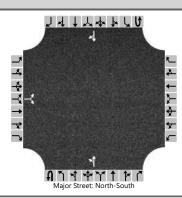
Vehicle Volumes and Ad	justme	nts														
Approach		Eastk	ound			Westl	oound			North	bound			South	bound	
Movement	U	L	Т	R	U	L	Т	R	U	L	Т	R	U	L	Т	R
Priority	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12
Number of Lanes	0	0	1	0	0	0	1	0		0	1	0		0	0	0
Configuration				TR		LT					LR					
Volume (veh/h)			60	12		1	59			10		3				
Percent Heavy Vehicles (%)						0				0		0				
Proportion Time Blocked																
Percent Grade (%)										()					
Right Turn Channelized																
Median Type Storage				Undi	vided											
Critical and Follow-up H	eadwa	ys														
Base Critical Headway (sec)						4.1				7.1		6.2				
Critical Headway (sec)						4.10				6.40		6.20				
Base Follow-Up Headway (sec)						2.2				3.5		3.3				
Follow-Up Headway (sec)						2.20				3.50		3.30				
Delay, Queue Length, an	d Leve	l of S	ervice													
Flow Rate, v (veh/h)	Τ					1					16					
Capacity, c (veh/h)						1521					870					
v/c Ratio						0.00					0.02					
95% Queue Length, Q ₉₅ (veh)						0.0					0.1					
Control Delay (s/veh)						7.4					9.2					
Level of Service (LOS)						Α					А					
Approach Delay (s/veh)		0.1					9.2					-		-		
Approach LOS								A								

HCS7 Two-Way Stop-Control Report										
General Information Site Information										
Analyst	LMM	Intersection	Munhall Ave/Site							
Agency/Co.	GHA	Jurisdiction	Local							
Date Performed	9/2/2020	East/West Street	Site							
Analysis Year	2027	North/South Street	Munhall Ave							
Time Analyzed	Total (Open) AM	Peak Hour Factor	0.90							
Intersection Orientation	North-South	Analysis Time Period (hrs)	0.25							
Project Description	5718.900									



Approach		Eastb	oound			Westl	oound			North	bound			South	bound	
Movement	U	L	Т	R	U	L	Т	R	U	L	Т	R	U	L	Т	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		0	1	0		0	0	0	0	0	1	0	0	0	1	0
Configuration			LR							LT						TR
Volume (veh/h)		20		6						1	19				7	6
Percent Heavy Vehicles (%)		2		2						2						
Proportion Time Blocked																
Percent Grade (%)			0													
Right Turn Channelized																
Median Type Storage	Undivided															
Critical and Follow-up H	eadwa	ys														
Base Critical Headway (sec)		7.1		6.2						4.1						
Critical Headway (sec)		6.42		6.22						4.12						
Base Follow-Up Headway (sec)		3.5		3.3						2.2						
Follow-Up Headway (sec)		3.52		3.32						2.22						
Delay, Queue Length, an	d Leve	l of S	ervice													
Flow Rate, v (veh/h)	T		29							1						
Capacity, c (veh/h)			998							1604						
v/c Ratio			0.03							0.00						
95% Queue Length, Q ₉₅ (veh)			0.1							0.0						
Control Delay (s/veh)			8.7							7.2						
Level of Service (LOS)		А								А						
Approach Delay (s/veh)		8.7					•	0.4								
Approach LOS		А														

HCS7 Two-Way Stop-Control Report										
General Information Site Information										
Analyst	LMM	Intersection	Munhall Ave/Site							
Agency/Co.	GHA	Jurisdiction	Local							
Date Performed	9/2/2020	East/West Street	Site							
Analysis Year	2027	North/South Street	Munhall Ave							
Time Analyzed	Total (Open) PM	Peak Hour Factor	0.90							
Intersection Orientation	North-South	Analysis Time Period (hrs)	0.25							
Project Description	5718.900									



Vehicle Volumes and Ad	iustme	nts														
Approach		Eastbound Westbound							North	bound		I	South	bound		
Movement	U	L	Т	R	U	L	Т	R	U	L	Т	R	U	L	Т	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		0	1	0		0	0	0	0	0	1	0	0	0	1	0
Configuration			LR							LT						TR
Volume (veh/h)		11		4						6	25				34	22
Percent Heavy Vehicles (%)		2		2						2						
Proportion Time Blocked																
Percent Grade (%)			0													
Right Turn Channelized																
Median Type Storage	Undivided															
Critical and Follow-up H	eadwa	ys							•							
Base Critical Headway (sec)	Т	7.1		6.2						4.1						
Critical Headway (sec)		6.42		6.22						4.12						
Base Follow-Up Headway (sec)		3.5		3.3						2.2						
Follow-Up Headway (sec)		3.52		3.32						2.22						
Delay, Queue Length, an	d Leve	of S	ervice													
Flow Rate, v (veh/h)	Т	Π	17							7						
Capacity, c (veh/h)			933							1541						
v/c Ratio			0.02							0.00						
95% Queue Length, Q ₉₅ (veh)			0.1							0.0						
Control Delay (s/veh)			8.9							7.3						
Level of Service (LOS)			Α							А						
Approach Delay (s/veh)		8.9						1.4								
Approach LOS		A														

	AGENDA	A ITE	EM EXECUTIVE SUMMARY	Agenda Item Number: 4e						
	Title:	Red	Recommendation regarding 2021 Inclusionary Housi							
CITY OF ST. CHARLES ILLINOIS • 1834	Presenter:	Elle	n Johnson							
Meeting: Planning	& Developm	ent C	ommittee Date: Octo	ber 12,	2020					
Proposed Cost: \$		·	Not Budgeted:							

Executive Summary (if not budgeted please explain):

Background

The Inclusionary Housing Ordinance (IHO), Title 19 of the Municipal Code, requires developers of new residential developments to build a proportionate share of affordable housing units on site, or pay a fee in-lieu of providing affordable units. Fee in-lieu payments made by developers are placed into the City's Housing Trust Fund to be used to provide and preserve affordable housing opportunities in St. Charles.

The IHO fee-in-lieu may be set on an annual basis at the discretion of the City Council. Council typically sets the fee at the beginning of each calendar year. The current fee for calendar year 2020 is \$39,665.75 per required affordable unit (calculated as the cost of a 25% downpayment for an affordable house priced at \$158,663). The fee is the same for all residential developments, regardless of unit type.

Discussion regarding IHO fee-in-lieu for 2021

Over the past few years, residential growth in the City has been primarily comprised of multi-family residential projects, and permanent affordable multi-family units have been created (including the completed Anthony Place project and proposed second phase building). The City is currently reviewing single-family and townhome developments that may begin construction next year, and in light of these projects, the IHO Fee for 2021 was discussed at the recent City Council Retreat.

Developers have noted that inclusion of affordable units within single family or townhome projects is challenging and that the IHO fee-in-lieu may be acting a disincentive for moderately-priced projects.

Staff is proposing that the Committee consider a tiered fee structure based on unit type, with the highest fee for multi-family units, a lesser fee for townhome units, and the lowest fee for single-family units. This tiered structure takes into account the relative ease of incorporating affordable units within multi-family developments and the greater likelihood of being able to take advantage of the density bonus allowed under the code for developments that incorporate affordable units, compared to single-family developments. Also, the cost of construction per multi-family unit vs. single-family unit is much lower, with the cost of a townhome unit falling between multi-family and single-family.

Proposal for 2021 IHO Fee:

The formula for calculating the fee could be modified as follows, while continuing to use the current affordable house price of \$158,663 as a baseline for the calculation:

- Multi-Family Developments: \$39,665.75 (cost of a 25% downpayment) same as existing
- Townhome Developments: \$27,766.03 (cost of a 17.5% downpayment)
- Single-Family Developments: \$15,866.30 (cost of a 10% downpayment)

Alternatively, a new formula could be utilized, or the fee could be chosen not based on any formula. The attached Memo lists the total fee in-lieu contributions resulting from the tiered fees and other fee options.

Attachments (please list):

IHO Fee Memo

Recommendation/Suggested Action (briefly explain):

Provide a recommendation on the 2021 Inclusionary Housing Fee.

Community & Economic Development Planning Division

Phone: (630) 377-4443 Fax: (630) 377-4062



Staff Memo

TO: Chairman Rita Payleitner

And the Members of the Planning & Development Committee

FROM: Ellen Johnson, Planner

RE: 2021 Inclusionary Housing Fee In-lieu

DATE: October 2, 2020

Staff is seeking direction from the Committee on setting the Inclusionary Housing Fee In-lieu for calendar year 2021, which would take effect January 1, 2021.

Per Section 19.02.060 of the City Code, the Inclusionary Housing fee in-lieu may be set on an annual basis at the discretion of the City Council:

The amount of the per-unit fee in-lieu of Affordable Units shall be determined annually by the City Council. If no fee has been determined by the City Council for the current year, the fee most recently determined by the City Council shall apply.

I. BACKGROUND

City Code Title 19 "Inclusionary Housing" requires developers of new residential developments to build a proportionate share of affordable housing units on site, or to pay a fee in-lieu of providing affordable units.

Fee in-lieu payments made by developers are placed into the City's Housing Trust Fund. The purpose of the Housing Trust Fund is to provide and preserve affordable housing opportunities in St. Charles. Currently, the City offers the Home Rehab & Accessibility Loan Program and First-Time Homebuyer Loan Program which are funded by the Housing Trust Fund.

The City has also earmarked a significant portion of the fund to the Kane County Affordable Housing Fund to be made available to developers of affordable housing. Two purchase/rehab/resale projects have been approved with HTF dollars: 1432 Dean St., which was recently completed and sold to an income-eligible buyer, and 704 Adams Ave., which is currently being rehabbed. In September, the Housing Commission also approved partial funding for construction of a new affordable single-family home on the City-owned Moore Ave. lot, which P&D Committee decided should be offered as a buildable lot for an affordable single-family home.

II. CURRENT IHO FEE

A summary of past IHO fees can be found attached. Currently the fee per required affordable unit is \$39,665.75, which was set in February 2020 and is the same as the fee for 2019. The fee is calculated as the cost of a 25% downpayment for an affordable house priced at \$158,663. The affordable house price used was determined by the Illinois Housing Development Authority in its 2018 Report on Statewide Local Government Affordability.

IHDA is expected to release its next Report on Statewide Local Government Affordability in 2023. Unless the Council sees a need to change the fee and/or method for calculating the fee, the existing fee could remain in place until the updated affordable home price is determined by IHDA in 2023.

III. 2021 IHO FEE

The 2021 IHO Fee was discussed at the recent City Council Retreat. Prior to the retreat, the Housing Commission provided informal feedback to the Commission's City Council Liaison that their preference is to keep the fee for 2021 as-is.

Options for the 2021 fee include:

- 1. Keep the current fee of \$39,665.75 per required affordable unit.
- 2. Establish a new fee, based on a formula or not.
- 3. Establish a tiered fee structure based on unit type, with the current fee for multi-family units, a lesser fee for townhome units, and the lowest fee for single-family units. This tiered structure takes into account the relative ease of incorporating affordable units within multi-family developments and the greater likelihood of being able to take advantage of the density bonus allowed under the code for developments that incorporate affordable units, compared to single-family developments. Also, the cost of construction per multi-family unit vs. single-family unit is much lower, with the cost of a townhome unit falling between multi-family and single-family.

If this course of action is desired, the existing formula for calculating the fee could be modified as follows:

- Multi-Family Developments: \$39,665.75 (cost of a 25% downpayment for an affordable house priced at \$158,663) *same as existing*
- Townhome Developments: \$27,766.03 (cost of a 17.5% downpayment for an affordable house priced at \$158,663)
- Single-Family Developments: \$15,866.30 (cost of a 10% downpayment for an affordable house priced at \$158,663)

The table on the following page lists the three fee tiers and the resulting fee in-lieu contributions for various sized developments.

		1-unit development	50-unit development	100-unit development	250-unit development	500-unit development
Affordable Units Required		0.05 unit	5 units	10 units	25 units	50 units
Multi-Family Fee (current)	\$39,665.75	\$1,983	\$198,329	\$396,658	\$991,644	\$1,983,288
Townhouse Fee	\$27,766.03	\$1,388	\$138,830	\$277,660	\$694,151	\$1,388,302
Single- Family Fee	\$15,866.30	\$793	\$79,332	\$158,663	\$396,658	\$793,315

The IHO Fee does not need to be set based on a formula. The current formula has been used since 2016. However, a new formula for calculating the fee(s) could be utilized, or the fee could be chosen not based on any formula. The table below lists possible fees not based on a formula, from \$50,000 down to \$5,000 per required affordable unit.

Other Poss	Other Possible Fees:								
\$50,000	\$ 2,500	\$250,000	\$ 500,000	\$1,250,000	\$2,500,000				
\$40,000	\$ 2,000	\$200,000	\$ 400,000	\$1,000,000	\$2,000,000				
\$30,000	\$ 1,500	\$150,000	\$ 300,000	\$750,000	\$1,500,000				
\$20,000	\$ 1,000	\$100,000	\$ 200,000	\$500,000	\$1,000,000				
\$10,000	\$ 500	\$50,000	\$ 100,000	\$250,000	\$500,000				
\$5,000	\$ 250	\$25,000	\$ 50,000	\$125,000	\$250,000				

IV. ATTACHMENTS

• Summary of IHO Fee In-lieu History

Inclusionary Housing Fee In-lieu History

The table below lists the past and current fee in-lieu amounts since the Inclusionary Housing Ordinance was adopted in 2008:

Year Fee Established	Fee Amount
2008	\$140,000 / required affordable unit
2010	\$104,500 / required affordable unit
2016	\$72,819.50 / required affordable unit
2017	\$72,819.50/ required affordable single-family, townhome, or duplex unit
2017	\$5,000 / required affordable multi-family unit
2018	\$36,409.75
2019	\$39,665.75 / required affordable unit
2020	\$39,665.75 / required affordable unit

The fee in-lieu that was set upon adoption of the IHO in 2008 was **\$140,000** per required affordable unit. The following calculation was used to determine the fee:

St. Charles' Median Home Sale Price (from Assessor's data) – 2/3 of the IHDA Affordable Price for a 4-person Household

In 2010, the fee in-lieu was lowered to \$104,500 after IHDA released an updated affordability chart with a lower affordable price for a 4-person household. The same calculation as used in 2008 was used to determine the new fee.

In 2013, the IHO was suspended after a determination by staff that over 25% of the City's housing stock was affordable, per the provisions of the IHO at that time.

In March of 2016, the IHO was reinstated in an amended form. A new fee in-lieu was set due to concern expressed by City Council members that the fee of \$104,500 was too high. The Housing Commission recommended the fee in-lieu be calculated as the cost of providing a 25% downpayment for two affordable units priced at \$145,639, which is the affordable home price for St. Charles determined by IHDA in 2013. This resulted in a fee of \$72,819.50.

In February 2017, City Council established a different fee for multi-family housing developments due to concerns that the fee of \$72,819.50 was too high for multi-family projects. This resulted in a dual fee structure: \$5,000 for multi-family developments and \$72,819.50 for single-family/duplex/townhome developments.

In March of 2018, upon discussion with the Housing Commission at a joint meeting, City Council returned to a single fee for all types of residential units and set the fee at \$36,409.75, calculated as the cost of a 25% downpayment for one affordable unit priced at \$145,639.

In January 2019, based upon the Housing Commission's recommendation City Council kept the previous methodology for establishing the fee, but recalculated it based on the new affordable home price for St. Charles determined by the Illinois Housing Development Authority in its 2018 Report on Statewide Local Government Affordability. The fee was increased to \$39,665.75, calculated as the cost of a 25% downpayment for one affordable unit priced at \$158,663.

In February 2020, City Council kept the previous year's fee of \$39,665.75, as recommended by the Housing Commission. IHDA is expected to release its next Report on Statewide Local Government Affordability in 2023. Unless the Council sees a need to change the fee and/or method for calculating the fee, the existing fee could remain in place until the updated affordable home price is determined by IHDA in 2023.

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	ST. CHARLES

AGENDA ITEM EXECUTIVE SUMMARY		Agenda Item number: *4f
Title:	Historic Preservation Commission recommendation to approve Historic Landmark Designation for 210 Cedar Ave. "Walker-Morse"	
Presenter:	Rachel Hitzemann	

Meeting: Planning & Development Committee Date: October 12, 2020

Proposed Cost: N/A Budgeted Amount: N/A Not Budgeted:

Executive Summary (if not budgeted please explain):

Lance Ramella has nominated his property at 210 Cedar Ave. for Landmark status.

In accordance with the Zoning Ordinance, the Historic Preservation Commission held a public hearing on the landmark nomination on 10/7/2020. The Commission recommended approval of the nomination by a vote of 5-0, based on the criteria listed in the attached resolution.

The Historic Commission discussed at length whether or not the building should be landmarked. They noted that many non-contributing elements were added to the home, making the original structure almost unrecognizable. However, the Commission decided that based on history of those associated with the building, and the contribution of the building to the neighborhood as a whole, it was worthy of a landmark status.

The house was built in the National style in 1848. Jane B. Walker was the original owner of the home, but the Morse family owned the property from 1855-1975.

If the Landmark designation is approved by City Council, a Certificate of Appropriateness from the Historic Preservation Commission will be required prior to issuance of a permit for construction, alteration, repair, demolition, relocation, or other material change that affects the exterior architectural appearance of the structure.

Attachments (please list):

Historic Commission Resolution, Landmark Nomination

Recommendation/Suggested Action (briefly explain):

Historic Preservation Commission recommendation to approve a Historic Landmark Designation for 210 Cedar Ave., "Walker-Morse".

City of St. Charles, Illinois

Historic Preservation Commission Resolution No. 9-2020

A Resolution Recommending Approval for Landmark Designation (210 Cedar Ave. – Walker-Morse)

WHEREAS, per Section 17.32.060 of the St. Charles Zoning Ordinance, it is the responsibility of the St. Charles Historic Preservation Commission to evaluate applications for Landmark Designation and to make recommendations to the City Council regarding them; and

WHEREAS, the Historic Preservation Commission has reviewed the application for designation of 210 Cedar Ave. and hereby finds that the Landmark nomination meets one or more of the criteria for Landmark Designation listed in Section 17.32.060.C of the St. Charles Zoning Ordinance based on the historical and architectural significance as described in the following findings:

- The property has character, interest or value which is part of the development, heritage or cultural character of the community.
 -AND-
- 2. The property is identified with a person who significantly contributed to the development of the community.

Constructed in 1848 in the National style, the building was home to the Morse family for 120 years. Levi Morse, along with two other people, are responsible for rebuilding the structures along East Main St. after a fire destroyed most of them. The Morse building, located across from City Hall, is named after Levi Morse.

4. Property has a unique location or physical characteristics that make it a familiar visual feature.

Located in Century Corners, the property is close to Downtown and on a heavily trafficked area. While not architecturally significant, the house sits towards the top on the hill on Cedar Ave. and contributes to the aesthetic of the neighborhood and blends in with the surrounding structures.

5. The property is suitable for preservation or restoration.

There have been many non-contributing elements added to the original structure including the front gable, south side bay window, the two-story bay window on the west and the half round window on the gable. However, due to the buildings history and relation to the neighborhood, the structure is suitable for preservation and restoration.

NOW THEREFORE, be it resolved by the St. Charles Historic Preservation Commission to recommend to the City Council that the property known as 316 Cedar St. as legally described in Exhibit "A", be designated as a Landmark, and that it be referred to as the "Walker-Morse", with a construction date of circa 1848.

Roll Call Vote:

Ayes: Norris, Kessler, Smunt, Pretz, Mann

Nays: None Absent: Malay Abstain: None

Motion Carried.

PASSED, this 7th day of October, 2020.

Chairman

Exhibit "A" Legal Description

LOT 6 (EXCEPT THE NORTH 20FT), IN BLOCK 21 OF THE ORIGINAL TOWN OF ST. CHARLES, ON THE EAST SIDE OF THE FOX RIVER, IN THE CITY OF ST. CHARLES, KANE COUNTY, ILLINOIS

PIN #09-27-384-011

CITY OF ST. CHARLES

TWO EAST MAIN STREET ST. CHARLES, ILLINOIS 60174-1984



COMMUNITY DEVELOPMENT/PLANNING DIVISION

PHONE: (630) 377-4443 FAX: (630) 377-4062

HISTORIC LANDMARK NOMINATION

Instructions:

To nominate a property for Historic Landmark Designation, complete this application and submit all required documentation to the Planning Division. Based on a review of the application by City staff and the Historic Preservation Commission, additional detailed information to support this application may be required.

The information you provide must be complete and accurate. If you have a question please call the Planning Division and we will be happy to assist you.



1. Property	Parcel Number(s):		
Information:	09 27 384 011		
	Property Name (Historic or common name of the property):	. 84	
	WAKER-MORSE House DR	SIRELD AME OWNER)	
2. Applicant:	Name LANCE RAMELLA	Phone 630 544 7826	
	Address 210 CEDAR AVENUE	Fax	
	ST. CHARLES, IL 60174	Email LRAMELLA 30 @ GMA: L	
3. Record Owner:	Name RicHARD (MARY RAMELLA	Phone Com	
	Address F-Amily TRUST	Fax	
		Email	
or other recorded docu	n of Property: The legal description should be obtained from ment (attach sheets if necessary).		
LOT 6 (EX	CEPT THE NORTH 20 FEET) IN B	Kock 21 OF PHE	
ORIGINAL"	TOWN OF ST. CHARLES, ON	THE EAST SIDE	
OF THE FO	ox Riven, in THE City OF.	ST. CHARLES, KAN	
l .	KLINOIS. (SEE ATTACHES)		

I. Classification of Property (Check all that apply):

a) Ownership: X private public-local	b) <u>Category:</u> building district	c) Integrity:
public-state d) Function or Use:	site	unaltered
Historic/Current	Historic/	Current Historic/Current
/_agriculture/_x commercial/_educational/_government/_entertainment	/	ndustrial / religious military / scientific museum / transportation private residence / other(specify park
e) Architecture:		
Early RepublicFederalEarly Classical Revival		Late Victorian2 nd Gothic RevivalItanlianateSecond Empire
Mid-19 th Century Greek Revival Gothic Revival Italian Villa National - Not Co	entributing	Second Empire Queen Ann Stick/Eastlake Shingle Style - SEE 1994 Survey Romanesque Renaissance Folk Victorian
Late 19 th /20 th Century Re Beaux Arts Colonial Revival Classical Revival		Late 19 th and Early 20 th Century (American Movements)Princess AnnHomestead
Tudor RevivalLate Gothic RevivalDutch Colonial RevivaEnglish CottageItalian RenaissanceFrench RenaissanceSpanish/Mission	al	(Amer. Arts & Crafts Movement)CraftsmanBungalowFoursquarePrairie School
Regional Origin _Vernacular (describe)		Modern MovementModern
Other (describe)		Art Deco International Style Ranch
	······································	

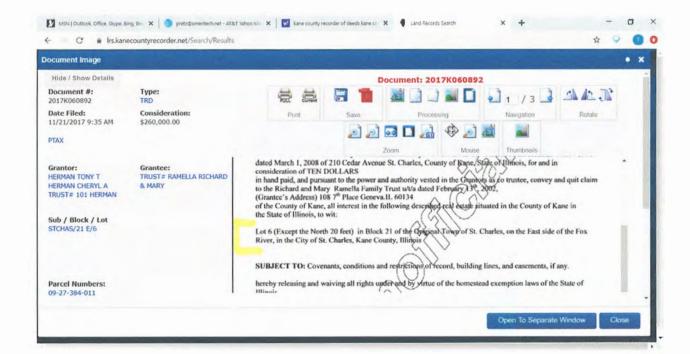
II. Building Materials:

Please mark the appropriate boxes listing the materials that exist on the building.

	Foundation	Walls	Roof	Others
Wood				Wirdows
Weatherboard,				
Clapboard				
Shingle		χ		
Log				
Plywood				
Shake	, (,\	Χ	
Stone	X (opino	(1)		
Granite	, (
Sandstone		,		
Limestone				
Marble				
Slate				1
Brick				CHIMANY
Metal				
Iron		**************************************		
Copper				
Bronze				
Tin				
Steel				
Lead				
Nickel				
Cast Iron				
Stucco				
Terra Cotta				
Asphalt				
Asbestos				
Concrete	X Coulel			
Adobe	7 000			
Ceramic Tile				
Glass				
Cloth/Canvas				
Synthetics				
Fiberglass				
Vinyl				
Aluminum				
Rubber				
Plastic				
Drivit/EIFS				
Other				

Ple	ease indicate source of documentation, if available.
a)	Original Owner: TANE B. WAKKER
b)	Architect/ Builder:
	Significant Person(s): LEUI MORSE (SEE ATTACHED)
d)	Significant Dates (i.e., construction dates): 1848 (SEM TTACHE) OWNERSH
	AND MAP)
e)	Please indicate which of the following criteria apply to the property:(check all that apply.)
	Y Property has character, interest, or value which is part of the development, heritage, or cultural character of the community, county, or nation.
	Property is the site of a significant local, county, state, or national event.
	Property is identified with a person who significantly contributed to the development of the community, eounty, state, or nation. LEVI MORSE (SEE ATTACHED) MORSE FAMILY OWNERSHIP 120 YEARS. Structure embodies distinguishing characteristics of an architectural style valuable for the study of a period, type, method of construction, or use of indigenous materials.
	Property is identified with the work of a master builder, designer, architect, or landscape architect whose work has influenced the development of the area, the county, the state, or the nation.
	Structure embodies elements of design, detailing, materials, or craftsmanship that are of architectural significance.
	Structure embodies design elements that make it-structurally or architecturally innovative.
	Property has a unique location or physical characteristics that make it a minimar visual reature. CENTURY CORNERS / DOWNTOWN City Structure is a particularly fine or unique example of a utilitarian structure with a high level of historical or architectural significance.
	X Property is suitable for preservation or restoration.
	Property is included on the Illinois and/or National Register of Historic Places.
	Property has yielded, or is likely to yield information important to prehistory, history, or other areas of archaeological significance.

III. Significance of Property:



FOLK HOUSES

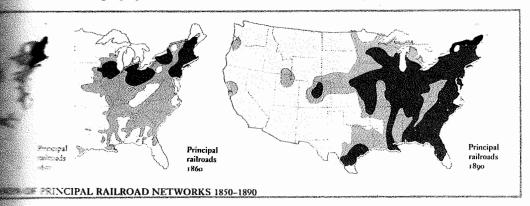
National

after ca. 1850-1890

the continent in the decades from 1850 to 1890. Modest dwellings built far from transport were no longer restricted to local materials. Instead, bulky items used for moved rapidly and cheaply over long distances. As a result, large lumberyards became standard fixtures in the thousands of new towns which sprouted as trade along the railroad routes. Soon folk houses built with logs, sod, or heavy hewn were being abandoned for wooden dwellings constructed with light balloon or framing covered by wood sheathing. The railroads thus changed the traditional materials and construction techniques of folk dwellings over much of the nation.

The railroad-inspired era of national folk housing did not completely erase the eardictions, however, for many of the previous folk shapes persisted even though now different techniques. These, along with some new shape innovations, make up matrice families of house shapes that dominated American folk building through half of the 20th century. Only recently have these generally been abandoned for the forms of folk dwellings (see pages 496-99).

the expansion of the railroads, gable-front houses remained common in the stern region formerly dominated by the New England folk tradition, as did similated plans with an added extension known as gable-front-and-wing houses. In the remaining eastern half of the country, hall-and-parlor and I-house shapes, seemeded from the Tidewater South tradition by way of the Midland log adaptational the dominant folk dwellings. All of these later folk forms, however, tend much less geographic restriction than did their pre-railroad predecessors, for as



transportation and communication improved, each shape became distributed beyond its area of traditional dominance. Light framing techniques also led to new folk forms which grew in popularity through the early decades of this century. These were generally massed-plan houses that were now relatively simple to construct because light wooden roof framing could easily be adapted to span two-room depths. Such houses, when of rectangular shape, normally had side-gabled roofs and are called massed-plan, side-gabled folk houses. More nearly square plans typically had pyramidal (equilateral hipped) roofs.

GABLE-FRONT FAMILY

The Greek Revival movement, which dominated American styled houses during the period from 1830 to 1850, commonly used the front-gabled shape to echo the pedimented facade of typical Greek temples. This form was particularly common in New England and the adjacent northeast region where simple gable-front folk houses also became popular during the pre-railroad era. This shape persisted with the expansion of the eastern railroad network in the 1850s and became a dominant folk form until well into the 20th century. Gable-front houses were particularly suited for narrow urban lots in the rapidly expanding cities of the northeast. There, many late 19th- and early 20th-century neighborhoods are dominated by both styled and simple folk examples built in this form. Most are narrow, two-story houses with relatively steep roof pitches. A related one-story urban form first became common in expanding southern cities in the late 19th century. This is the shotgun house, narrow gable-front dwellings one room wide that dominated many modest southern neighborhoods built from about 1880 to 1930. Some are elaborately styled but most are simple folk houses. The origin of these southern shotgun houses has been much debated. Some scholars note that similar forms are common in the West Indies and trace them from Africa to early Haitian influences in New Orleans, whence they became popular with Black freedmen migrating to southern urban centers following the Civil War. A less complex theory is that they are simply the familiar oneroom-deep, hall-and-parlor plan of the rural South turned sideways to accommodate narrow urban lots.

An additional wave of interest in the gable-front shape grew from styled houses of the early 20th-century Craftsman movement, which were typically built in this form. Many modest folk houses without stylistic detailing were inspired by such Craftsman houses in the decades from 1910 to 1930. These are usually one-story, double-width forms with low-pitched roofs; they are most common in rural areas and occur throughout the country.





RONT FAMILY

New York; late 19th century. Typical urban two-story examindlework porch detailing and patterned shingles in the gable id from the contemporary Queen Anne style.

5. New York; ca. 1907. An urban one-and-one-half-story exmodest Queen Anne detailing. The door and windows are later

and, Ohio; late 19th century. Urban example executed in ma-

tet County, North Carolina; 1864. Thomas House. Early examg Greek Revival influence in the pedimented gable and double th was common in the coastal Carolinas. Metal doors and storm 76 later additions.

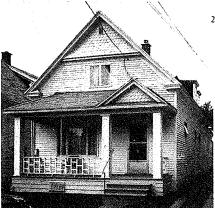
Mississippi; ca. 1905. Typical shotgun house of the urban retample has integral porch and modest Queen Anne detailing.

E. Kentucky; ca. 1910. Shotgun with Greek Revival-like

n County, Indiana; ea. 1935. Late example inspired by the Cape of the Colonial Revival movement.

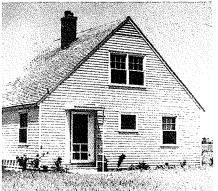
assion, Louisiana; ca. 1938. Typical example inspired by simi-© Craftsman houses.

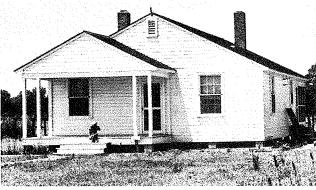


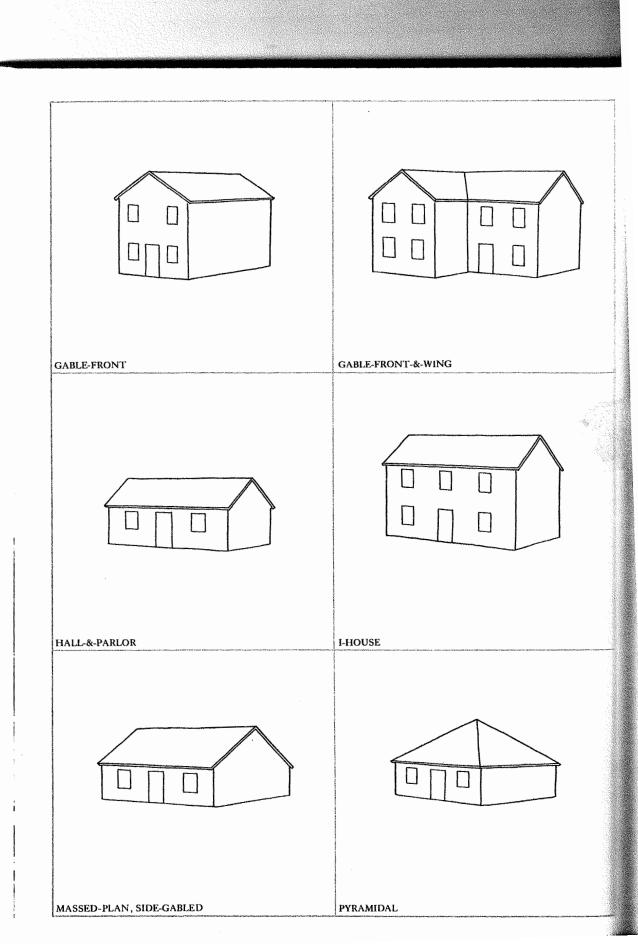














ARCHITECTURAL SURVEY

CENTRAL HISTORIC DISTRICT ST. CHARLES, ILLINOIS

ST. CHARLES HISTORIC PRESERVATION COMMISSION

Primary Structure

ADDRESS 210 Cedar Ave

1994 Photo Roll: 5
Negative: 6

ARCHITECTURAL SIGNIFICANCE

Significant

- ♦ Contributing
- □ Non-Contributing
- ☐ Potential for Individual National Register Designation

Photo: Aug. 2003

BUILDING CONDITION

- Excellent
- □ Good
- □ Fair
- □ Poor

ARCHITECTURAL INFORMATION

Architectural Style/Type:	National style with a non- contributing gable	Exterior Walls (Current):	
Architectural Features:		Exterior Walls (Original):	
Date of Construction: Source:	1848	Foundation: Roof Type/Material:	
Overall Plan Configuration:		Window Material/Type:	

ARCHITECTURAL FEATURES: Front gable roof, Bay window and porch appear to be added recently. Round top window at second level added recently. The front gable, south side bay window, the two-story bay window on the west and the half round window on the gable are all non-contributing elements to the original architecture.

ALTERATIONS: Major alterations to first and second floors.

210 Cedar Avenue Description

Built in 1848 on Lot 6, Block 21 of the Original Town of St. Charles, Kane County, Illinois (see attached), by Jane Walker (see attached), this beautifully restored National Gable style house (see attached) resides prominently in an area known as Century Corners within walking distance of Baker Memorial Church, Pottawatomie Park, the Fox River, and the heart of the city downtown. Shingle cladding was added later which might have caused confusion on a 1994 area architectural review by Mike Dixon with a gabled roof. It appears the bay window, porch and top round window are not original and are not contributing to the style. The building is now known as 210 Cedar an event site. The Morse family owned the property for 120 years from 1855 to 1975. Levi Morse (see attached) is the known resident having been born is 1850 and his name on the Morse building located on the south side of Main across the street from the municipal center. Levi, along with two others rebuilt that section of town in 1895 after the great fire which destroyed the then existing buildings.

210 Cedar Avenue

09-27-384-011

STCHAS/21/6

All support documents are housed with the Kane County Recorder of Deeds.

1848	WD	Ira/Sarah Minard to Alexander B. Moore
1848	WD	Alexander B./Helen P. Moore to Jane B. Walker
1848	MTG	Alexander B. Moore to Jane B. Walker
		House/structure is constructed in 1848
1851	REL	Alexander B. Moore to Jane B. Walker
1851	WD	Abraham/Jane B. Howe to Sidney S. Grainis
1855	WD	Sidney S./Sarah S. Grainis to Wm Morse
		1860 Library of Congress City of St. Charles shows structure is present
1861	WD	Wm/Mariah Morse to John Mead
1867	WD	John/Mary A. Mead to Maria Morse
1885		Estate of Estella Morse
1886	Adm	Levi Morse as administrator to Willie Morse
1888	QCD	Mary (Mead) Gulig, widow of Jno Mead & D'csd Andrew Gulig to
		Willie Morse
1893	QCD	Levi/Bessie B. Morse to Willie Morse
1899	QCD	Levi/Bessie B. Morse to Willie Morse

- 1925 WD Wm & wife Morse to Norma (Morse) Cook
- 1975 ExD Probate of Norma Estella (Morse) McConky estate to Lois Scaleif
- 1975 JTWD Lois Scaleif to Thomas & wife Ference
- 1983 JTWD Thomas & wife Ference to Walter & wife Volbrecht
- 1998 TD First National Bank to Cheryl Herman
- 2008 TD Cheryl Herman to Cheryl Herman
- 2017 TD Cheryl Herman to Richard/Mary Ramella Trust



1860 ary map

IV. Attachments

- 1. <u>Descriptive Statement:</u> Attach a narrative statement describing the property and its historical architectural significance as indicated in Sections I, II, and III above. Describe structural changes, additions, and decorative modifications or material changes and dates of such work if known. State the reasons it should be designated as a Historic property.
- 2. <u>Plat of Survey:</u> Attach a plat of survey showing the boundaries and location of the property. This may be obtained from the County Recorder (630-232-5935) at the Government Center. You may also have one from your house closing.
- 3. Photographs: Attach photographs showing the important structures or features of the property and a photograph as viewed from the public way. Black and white or color prints. A minimum of one photograph of the structure as viewed from the public way is required.

I (we) certify that this application and the documents submitted with it are true and correct to the best of my (our) knowledge and belief.

Record Owner Date

Applicant or Authorized Agent Date

Levi Morse

Born in St. Charles on July 4th ,1850 he died in 1935. Best known for the Osgood, Hunt, and Morse building (see attached) and the meat market he ran with his brother William on that site. His loss in 1935 is contained in the newspaper (see attached).

9 C

Search this site

St. Charles Public Library
One South Sixth Ave., St. Charles, IL 60174 # 630.584.0076

St. Charles Historic Buildings

SCPL Historic
Buildings Page

Welcome

Buildings with Historic Landmark Documentation

Library Resources

Links

Local Buildings -Alphabetically

Local Buildings By Address

Map

Comments

Email or Call 630-584-0076 x1 Welcome > Local Buildings - Alphabetically >

Osgood, Hunt and Morse Block

(View on map)

Most of the buildings in what is known as the Osgood, Hunt, and Morse Block were constructed around 1896 after earlier structures were destroyed. In April 1895, embers from a fire in the



Image credit: St. Charles Public Library

Fredenhagen Mill across the street (the present location of the <u>Municipal Building</u>) spread to the business buildings in this block and all but two of them were demolished. One of the remaining walls collapsed during a severe storm one month later. Falling debris killed four people and destroyed Anderson Dressmaking and Church's Milliner Shop. W.Frank Osgood, Bela Hunt, and Levi Morse rebuilt the entire block.

Another dramatic event took place in this block in 1915. A Chicago Great Western Railway train jumped the tracks on the siding rail that ran near where the Municipal Center stands today and struck the 12 E. Main Street building. The accident caused little damage to the building and no injuries were reported.

Many businesses and professional offices have occupied the Osgood, Hunt, and Morse Block since 1896 when Levi and William Morse opened a meat market in the Morse Building. During the early 1910s, Louis Kreeger ran a dry goods store and Joseph A. Saperston owned a grocery and fruit store in the block, Arvid O. Johnson established a tailor shop in the 1920s at the 9 E. Main Street address. The National Tea Company was located next door at 10 E. Main Street. The John Stewart and Company Bank occupied 5 E. Main Street from 1902-1930. The State Bank of St. Charles acquired the Stewart Bank building in 1930. In 1965 the State Bank of St. Charles moved to the modern structure it built at 1 E. Main Street. The Harris Bank took over the State Bank's operation in 1988. Sandholm's Meat Market flourished through the 1920s, and Glos and Hunt Hardware stores occupied the block at the turn of the 19th century. Burger Drugs established its pharmacy business in 1938 at 11 E. Main Street and later moved to 9 E. Main Street when the National Tea Store vacated the property in 1951. Burger Drugs closed in 2008 after nearly 70 years in business. Smitty's On the Corner sandwich shop opened at 15 E. Main Street in February, 2000.

Over the years, doctors Van Patten, I.G. Langum, Elvero Pierson, Ralph Edwards and C.L. Smith operated medical and dental offices above the retail businesses. Lawyers Charles L. Hunt, Harry Hempstead, and D.A. Green also occupied the second floor offices.

The BMO Harris Bank currently owns most of the buildings in the Osgood, Hunt, and Morse Block.

For additional photographs see:

- Cairo, Alice. Then and Now. 197? (R977.323
 THE) View online
- Pearson, Ruth. Reflections of St. Charles. St.
 Charles: St. Charles Historical Society, 1976. (977.32 PEA, p. 37)
 View online
- Spirou, Costas. St. Charles: Culture and Leisure in an All-American Town. Charleston, SC: Arcadia, 2005. (977.323 COS, pp. 24-25, 31, 33, 40)
- Celebrating History: A of St. Charles, Illinois
 Pictorial Essay. St. Charles: St. Charles Historical
 Society, 1990. (977.323 CEL, p. 15).

Sources

BIG REVENUE JUSTIFIES

18.Wks.Income;\$63,000 Proposed Bidg. only \$54,500.

ASK tithns you south out to the dirt scout count held Tuesday evening. Reso which an appeinted commit formed were rend and ap

coved.

Girl Scouts are to be saked to take swern to the Community House the sy before Memorial Day to be made to swerter for decerating graves of pterans. Also the Girl Scouts are

ELECTION NOTICE

OBITUARY

Mrs. Loone V. Spriet.
Leens Spriet, widow
so Spriet, Sr., died at the he
r son Charles, May 8th, a

G years.

An active weams all her itfe, it ealth overteek her a few weeks age, and to her ooly denghter, Men. Manha Vandenheek of Jersey Chr., M., who came on wist to her mother a mouth age, she couffided that he was not to get well. Her worth he was not to get well. Her worth we prophetic. Reprint we her it Belgium, at came to America before her man-

ber family and friends.

Mes. Wes. R. Gereissan.
Another former resident of St.
Chartes has goos is her long home.
Eztelle Gooding Goodman, a reedsident of Palisaderson mars Riverside,
California, on Sundey, May S, 1985.
Mrs. Geodman was born at Preeport, IR. Pedvary 9, 1844. She
moved with her family to St. Charles
in 1875, where she and Wes. R. Goodman were school mates and where
they were married on Just 19, 1890.
Mr. Goodman attended the East Side
school.

s \$3.1,28.5.0.7, for 1933 \$33.7, for 1933 \$34,373.1, for 1934 \$34,170.6.1. Revenue 1934, \$39.3,170.6.1. Revenue 1934, \$39.3,170.6. Revenue 1934,

Bus. & Professional

for Public Is May 23
Owing to indement weather the Business and Professional Woman's club party which was to have been held at Wing-Hanner camp. Thursday ovening, May 23rd, has been changed to the Community House. A nice party is being arranged for public pathronge, the tickets which sall for 25 central and a second of the public pathronge, the tickets which sall for 25 central and your friends to go. This club has been most liberal in their donations to the Community House giving that worthy place 3th from their treasury at their last regular meeting. The last regular meeting of the year for this club is their dimerimenting next Tuesday ovening in their club recon. for Public Is May 23

The Kinkelen Girls of the Bethle-le hers Lutherm church will sheet on ill Thorpator, May She'l at a gam, Man Margarett Bary and Man Phone

ELECTION IN

Mayor's Appointments; D. A. Green Named City Atty.

A number of candidates are in the field for second ward alderman to fill the vacancy caused by the death of John B. Nichola, re-elected.

Parks and Playgrounds: Spi Murphy. Fire: Larson, Murphy Judiciary Purfield, Larson. Health: Crandall, Purfield. Buildings: Murphy, Spriet, Lar Special standing committee: 1

Special standing committee: Larn, Speiet, Muruhy.

Board of Local Improvements:
armon, Crasiall, Spriet,
Other appointments by the rayor
of confirmed by the city council
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Elliott Writes of Old Time Friends, & New, Who Are Laid to Rest

By U. S. Elliet.

An extremely heavy toll by death has lately removed several old times. St. Charles citizens to their eternal rest. Some of the departed always have made their home bere in St. Charles, while others, years ago removed to homes in other parts which they adopted. The death toll being cextremely heavy at this time, the

in St. Charles, July 4th, 1850, and until the close, or there about of the 19th century, had always made St. Charles has home. Levi Morse, 50 years ago, was a wall known cittien of St. Charles. He was then an emport paper maker and machine terminates and taker in the Brawnall and Miller paper insilia. He was for many years a deacon and taker in the Congregational church. He was also a member of the Good Templar Society of which the Chronicle lately re-published 57 years ago never items stating: "Levi Morse and Alex ('ick') Sili ware delegates to a Good Templar Society." He was also a member of the Good to the Good Woods, which I, as teamster and tructions, attended. In Statistics when the departed ones while they were here take not so the departed ones while they were here to take not so the statistic ones while they were here to take not so the statistic ones while they were here to take not set to the tentum ones the set of the tentum ones.

CITY CALLS St. Charles Ball Fans , and Fonseca in Perso

Baseball fame of St. Charles will have an opperingly on Thursday, June 6th, to hear Lew Ponsen give his lecture on baseball and see the film "Play Ball" which has been peared under hig supervision.

The film shows closenaps and slow motion ablots of "Balls" Ruth, Lou Gehrig, Mickey Cochrane, "School Bey" Rows. Jimmy Praxz, Ragner Hormsby, and many others. Cordsuing with a descriptive play by play around the diamend, Mickey Cechrane demonstrates how signals are

with the plate, write meaning the property of the property of

Home Rule Bill for Municipalities to

Cut Out Red Tape
The Kane County Council of Secial
Agencies which met at Elgin Monday
werning passed the following resolutions which were sent this paper with

PUB. SCHOOLS' MUSIC FEST BAND AND GLEE

Director E. Harris Has Program of Vocal & Band Music.

May 17th at b p.m., who are robser-ing program:

March, "Gelden Gatz" ... Johnson
"Evening Idyl", "Reverie", Weber
Grade and Junior High Band
Clarinet Quartet ... (a) "Aloba Ce"
(b) "Santa Lucia"
March, "Ambessador" ... Davis

"All Matton de Manilla"

"El Matton de Manilla"

"El Matton de Manilla"

"Lusien High Grie Glee Chab

"Massar's in the Code, Gold ... Fester

"Massar's in the Code, Gold ... Fester

"Who Did"

"Who Did"

"Mine Thing To The Thing The Thing Thing The Thing Th

Kane Co. Council of Social Agencies to Carry on Relief Work

In March St. Charles had 300 un-

employed, registered persons on re-lief. Kane county has a total of 4,-300 families. Awaiting relief appropriations from the government, the work will be

The Hospital Report Is News of Patients Making Recoveries

St. Charles city hospital news in-udes the birth of a daughter to

The removas on a man-Art Saelam.

The recovery of George Holden
The entrance for medical attention
of Sam Todorofsky.

The retention of Airin Wilhison,
a 21-year-old young man employed
on the Einer Carison farm west of

take one's part. In the world's work at field or desk or mart, When this old joy of living we re-

All of our immediate neighbors belonged to this church They were a high-chase prosperous people, but "wee was sae" if I started to whistle on the Sabbath, or called the day "Sunday," in their hearing

The Brain Trust several months of a man who is one repealed the law of supply and dramad it ovidently has repealed the what makes it so hard of hern your first on the ground the state of the state of

LIL' THEATRES TABLE CLATTER THE BAD MAN" PLAY SCORES

Mexican Bandit and Wheel-Chair Uncle Make Big Hit.

"The Bad Man" given at the High School and term Treeday evening by the Little Theatre, was puchase one of the meet pretentious pres-entation they have yet made. It was a fell eveningly play and the

Did You Escape from Chain Gang? Maybe You Cashed in on It

Chain letters, dime and collar type became an epidemic in St. Charles almost overnight. Pairy tales of persens who have received immense sums from such let-

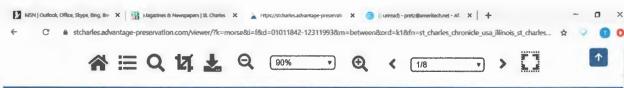
The St. Charles Unit of the Hom Bureau will sponsor a card party an-dates on Saturday evening, May 180 at the Little Woods School



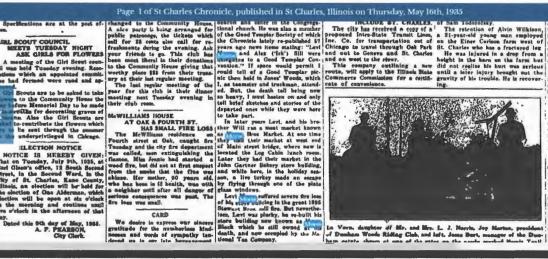
Page 1 of St Charles Chronicle, published in St Charles, Illinois on Thursday, May 16th, 1935

in St. Charles Monday evening to the maprovement which is reconstruction to the contraction as a home marker and has leved abe grown.

Lenter J. Storria, provident of the contraction o



Page 1 of St Charles Chronicle, published in St Charles, Illinois on Thursday, May 16th, 1935



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A reminiscent estation in Elgin, of my terrapeo room. They were Hahn They awal who we coming ramp to go to the al, by orders fr Prity sees in en like a bay in thyong and eage alipped out of the lag him Hew coul well! Yes he see

Elliott Writes of Old Time Friends, & New, Who Are Laid to Rest

By U. S. Elliott.

An extremely heavy toll by death has lately removed several old time St. Charles citizens to their eternal rest. Some of the departed always have made their home here in St. Charles, while others, years ago removed to homes in other parts which they adopted. The death toll being so extremely heavy at this time, the writer must be brief and not tell too long stories regarding the life time memories of each individual.

Levi Morse, the most aged old timer to pass on, died lately in his adopted western home. He was born here in St. Charles, July 4th, 1850, and until the close, or there about of the 19th century, had always made St. Charles his home. Levi Morse. 50 years ago, was a well known citizen of St. Charles. He was then an expert paper maker and machine tender in the Brownell and Miller paper He was for many years deacon and usher in the Congregational church. He was also a member of the Good Templar Society of which the Chronicle lately re-published 57

years ago news items stating: "Levi Morse and Alex ('Ick') Sill were delegates to a Good Templar Convention." If space would permit I could tell of a Good Templar picnic then held in Jones' Woods, which I, as teamster and truckman, attended. But, the death toll being now so heavy, I must hasten on and only tell brief sketches and stories of the departed ones while they were here to take part.

In later years Levi, and his brother Will ran a meat market known as Morse Bros. Market. At one time they had their market at west end of Main street bridge, where now is located the Log Cabin lunch room. Later they had their market in the John Gartner Bakery store building, and while here, in the holiday season, a live turkey made an escape by flying through one of the plate glass windows.

Levi Morse suffered severe fire of his store building in the great Stewart Bros. mill fire. But never less, Levi was plucky, he re-buil store building now known as M Block which he still owned at death, and now occupied by the tional Tes Company.

342
TECK
ST. CHARLES

AGENDA ITEM EXECUTIVE SUMMARY		Agenda Item number: *4g
Title: Historic Preservation Commission approve Historic Landmark Desig "O'Connor House"		
Presenter:	Rachel Hitzemann	

Meeting: Planning & Development Committee Date: October 12, 2020

Proposed Cost: N/A Budgeted Amount: N/A Not Budgeted:

Executive Summary (if not budgeted please explain):

Lance Ramella has nominated his property at 316 Cedar St. for Landmark status.

In accordance with the Zoning Ordinance, the Historic Preservation Commission held a public hearing on the landmark nomination on 9/16/2020. The Commission recommended approval of the nomination by a vote of 6-0, based on the criteria listed in the attached resolution.

The house was built in the Greek Revival style in 1855. The building was constructed with Limestone that was acquired from the adjacent quarry to the East.

John O'Connor was the original owner of the home.

If the Landmark designation is approved by City Council, a Certificate of Appropriateness from the Historic Preservation Commission will be required prior to issuance of a permit for construction, alteration, repair, demolition, relocation, or other material change that affects the exterior architectural appearance of the structure.

Attachments (please list):

Historic Commission Resolution, Landmark Nomination

Recommendation/Suggested Action (briefly explain):

Historic Preservation Commission recommendation to approve a Historic Landmark Designation for 316 Cedar St., "O'Connor House".

City of St. Charles, Illinois

Historic Preservation Commission Resolution No. 7-2020

A Resolution Recommending Approval for Landmark Designation (316 Cedar St. – O'Connor House)

WHEREAS, per Section 17.32.060 of the St. Charles Zoning Ordinance, it is the responsibility of the St. Charles Historic Preservation Commission to evaluate applications for Landmark Designation and to make recommendations to the City Council regarding them; and

WHEREAS, the Historic Preservation Commission has reviewed the application for designation of 316 Cedar St. and hereby finds that the Landmark nomination meets one or more of the criteria for Landmark Designation listed in Section 17.32.060.C of the St. Charles Zoning Ordinance based on the historical and architectural significance as described in the following findings:

1. The property has character, interest or value which is part of the development, heritage or cultural character of the community.

Constructed in 1855, this Greek Revival home built in Limestone is a prominent building on Cedar Street. This home is one of the few homes in St. Charles that has maintained and preserved the original limestone material.

Johnson's Statuary, a family owned business, was operated out of this building from 1972-2019.

2. The structure embodies distinguishing characteristics of an architectural style valuable for the study of a period, or use of indigenous materials.

-AND-

The structure embodies elements of design, detailing, materials, or craftsmanship that are of architectural significance.

The limestone for the home was taken from the local quarry pit that was located on the adjacent lot to the East.

The limestone found on the building is part of the original construction. The building maintains a well-preserved foundation, exterior walls and windows beads/sills.

4. Property has a unique location or physical characteristics that make it a familiar visual feature.

Resolution No. 7-2020

Page 2

Located near Lincoln Park, St, Patrick Church and Downtown St. Charles, this building has a prominent façade that is a familiar visual feature on the East side of

Town.

5. The property is suitable for preservation or restoration.

While there have been additions made to the structure, the original building maintains many of it's original details and building materials.

NOW THEREFORE, be it resolved by the St. Charles Historic Preservation Commission to recommend to the City Council that the property known as 316 Cedar St. as legally described in Exhibit "A", be designated as a Landmark, and that it be referred to as the "O'Connor House", with a construction date of circa 1855.

Roll Call Vote:

Ayes: Norris, Kessler, Smunt, Pretz, Mann, Malay,

Nays: None

Absent: Kirsininkas

Abstain: None

Motion Carried.

PASSED, this 16th day of September, 2020.

Chairman

Exhibit "A" Legal Description

LOT 6 IN BLOCK 47 OF THE ORIGINAL TOWN OF ST. CHARLES, ON THE WEST SIDE OF THE FOX RIVER, IN THE CITY OF ST. CHARLES, KANE COUNTY, ILLINOIS

PIN #09-27-357-005

CITY OF ST. CHARLES

TWO EAST MAIN STREET ST. CHARLES, ILLINOIS 60174-1984



COMMUNITY DEVELOPMENT/PLANNING DIVISION

PHONE: (630) 377-4443 FAX: (630) 377-4062

HISTORIC LANDMARK NOMINATION

Instructions:

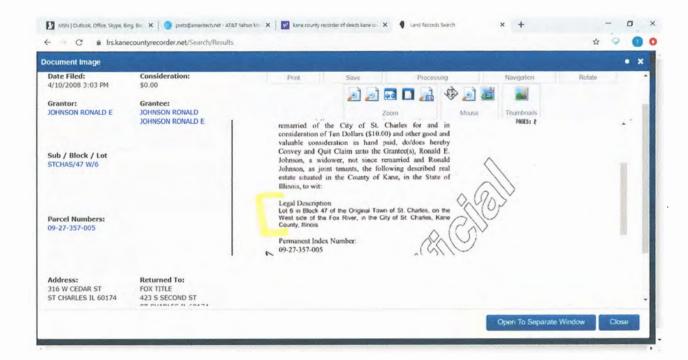
To nominate a property for Historic Landmark Designation, complete this application and submit all required documentation to the Planning Division. Based on a review of the application by City staff and the Historic Preservation Commission, additional detailed information to support this application may be required.

The information you provide must be complete and accurate. If you have a question please call the Planning Division and we will be happy to assist you.

Parcel Number(s):

RECEIVAGE St. Charles, I	L
AUG 1 2 2020	
CDD	ion

1. Toperty		
Information:	0927357005	NAME
	Property Name (Historic or common name of the property):	(REA) WHE
	O'CONNOR HOUSE	(DESIRE) DUNER
2. Applicant:	Name LANCE RAMELLA	Phone 630 544 7826
	Address 3/6 CEDAR STREET	Fax
	57. CHARLES, IL 60174	
3. Record Owner:	Name RicHard MARY RAMELLA	Phone GMAiL, Com
vision in the contract of the	Address FAMILY TRUST	Fax
		Email
or other recorded document	n of Property: The legal description should be obtained from the ment (attach sheets if necessary).	
LOT 6 in	· BLOCK 47 OF THE ORIG	want TOWN
OF ST. C	HARLES, ON THE WEST SIN	DE OF THE FO
RIVER,	IN THE City OF ST. CHAR	CLES, KANE
County	ILLINOIS (SEE ATTACHED)	



I. Classification of Property (Check all that apply):

a) Ownership:	b) Category:X_buildingdistrictsite	c) Integrity:original smoved: dunaltered	late
d) Function or Use:			
Historic/Current / agriculture // x commercial / educational / government / entertainment	/ m / m <u>x / x</u> pr	Current dustrial ilitary useum rivate residence ark	Historic/Current/religious/scientific/transportation/other(specify
e) Architecture:			
Early Republic Federal Early Classical Revival	E ATTACHED &	Late Victo 2nd Got Manlian Second Queen Stick/E	o <u>rian</u> hic Revival hate Empire Ann
Mid-19 th Century X. Greek Revival Gothic Revival Italian Villa National	E ATTAGY)	Stick/E Shingle Roman Renaiss Folk V	e Style esque sance ictorian
Late 19 th /20 th Century Re Beaux Arts Colonial Revival Classical Revival Tudor Revival	evivals		
Late Gothic RevivalDutch Colonial RevivaEnglish CottageItalian RenaissanceFrench RenaissanceSpanish/Mission	ıl	(Amer. AnCraftsmBungaloFoursquPrairie S	ow pare
Regional OriginVernacular (describe)			1
Other (describe)		Ranch	



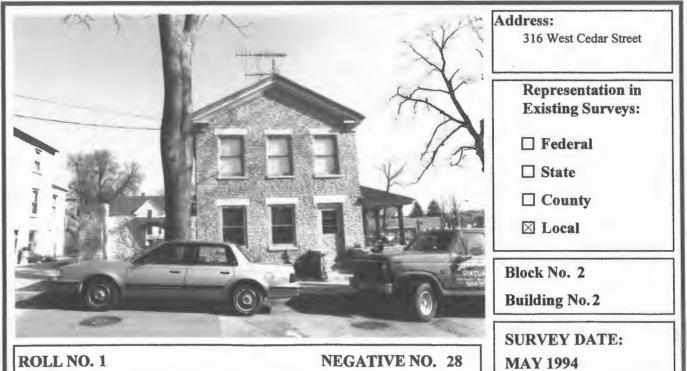
ARCHITECTURAL SURVEY

ST. CHARLES CENTRAL DISTRICT ST. CHARLES, ILLINOIS

DIXON ASSOCIATES / ARCHITECTS

ST. CHARLES HISTORIC PRESERVATION COMMISSION

ARCHITECTURAL IN	TE	GRI	ГҮ	BUILDING CONDITION
	1	2	3	☑ Excellent: Well-maintained
☐ Unaltered				☐ Good: Minor maintenance needed
☑ Minor Alteration	\boxtimes			☐ Fair: Major repairs needed
☐ Major Alteration				☐ Poor: Deteriorated
 ✓ Additions Sensitive to original Insensitive to original 1: first floor; 2: upper floors; 		of/cornic		ARCHITECTURAL DESCRIPTION Style: Greek Revival Date of Construction: ca. 1840's Source: A Field Guide to American Houses Features: Rough limestone exterior with cut limestone window
ARCHITECTURAL SIGN ☐ Significant ☐ Contributing ☐ Non-Contributing	IFIC	CANC	E	beads and sills. East porch and north section added. Board and batten sided garage added.



AFIELD GUIDE TO AMERICAN HOUSES

The guide that enables you to identify, and place in their historic and architectural contexts, the houses you see in your neighborhood or in your travels across America—houses built for American families (rich, poor, and in-between), in city and countryside, from the 17th century to the present



ROMANTIC HOUSES

Greek Revival

1825-1860

IDENTIFYING FEATURES

Gabled or hipped roof of low pitch; cornice line of main roof and porch roofs emphasized with wide band of trim (this represents the classical entablature and is usually divided into two parts: the frieze above and architrave below); most have porches (either entry or full-width) supported by prominent square or rounded columns, typically of Doric style; front door surrounded by narrow sidelights and a rectangular line of transom lights above, door and lights usually incorporated into more elaborate door surround.

PRINCIPAL SUBTYPES

Six principal subtypes can be distinguished on the basis of porch and roof configurations:

ENTRY PORCH LESS THAN FULL HEIGHT, OR ABSENT—About 20 percent of Greek Revival houses have small entry porches which do not extend the full height of the facade. In some examples the entry porch is recessed *into* the facade. About 5 percent lack porches altogether.

FULL-HEIGHT ENTRY PORCH—This subtype has a dominant central porch extending the full height, but less than the full width, of the facade; it thus resembles the Early Classical Revival style from which the Greek Revival sprang. The Greek Revival version can usually be distinguished from its predecessor by the typical band of cornice trim and the rectangular lights, rather than a curving fanlight, over the entrance. As in the earlier style, many Greek Revival examples have a traditional classical pediment above the entry porch. In contrast to the earlier style, however, many Greek examples have flat-roofed entry porches. As in the entry porch less than full height, this type of entry porch also occurs recessed into the facade. About one-fourth of Greek Revival houses are of this subtype; like Early Classical Revival houses, these are most common in the southern states.

FULL-FACADE PORCH—In this configuration, the colonnaded porch occupies the full width and height of the facade. No pediment occurs above the porch, which is covered either by the main roof or, less commonly, by a flat or shed-style extension from it. In a few examples, the full-facade porch also extends around one or both sides of the house. This subtype makes up about one-fourth of Greek Revival houses. Like the preceding type, it is most common in the southern states.

FRONT-GABLED ROOF.—All of the preceding subtypes have side-gabled or hipped roofs. In

this subtype the gable end is turned 90 degrees to make the principal facade. In some high-style examples a full-width, colonnaded porch is present beneath the front gable, giving the house the appearance of a miniature Greek temple with its traditional classical pediment. Smaller entry porches are common on vernacular examples. This subtype is more common in the northeastern and midwestern states.

GABLE FRONT AND WING—In this subtype a front-gabled roof, as in the type just described, has a side wing (less commonly two wings) added; these are typically lower than the dominant front-gabled portion. This subtype rarely occurs outside of the northeastern states and is particularly common in western New York and Ohio.

TOWN HOUSE—A sixth subtype consists of narrow urban houses with Greek Revival detailing. These occur both with and without porches. They are most common in those port cities of the Atlantic and Gulf coasts that were expanding in the decades from 1830 to 1860. These include Boston; New York; Philadelphia; Washington; Richmond, Virginia; Savannah, Georgia; Mobile, Alabama; New Orleans, Louisiana; and Galveston, Texas.

VARIANTS AND DETAILS

The principal areas of elaboration in Greek Revival houses are cornice lines, doorways, porch-support columns, and windows:

CORNICE LINES—The wide band of trim beneath the cornice of both the main roof and the porch roofs is an almost universal feature of Greek Revival houses. Commonly the band is made up of undecorated boards, but complex incised decorations also occur. In gabled houses the trim band may be variously treated along the gabled walls. Post-1850 examples, particularly in the South, often have Italianate brackets added at the cornice line.

DOORWAYS—As in the preceding Georgian, Adam, and Early Classical Revival styles, elaborated door surrounds are a dominant feature of Greek Revival houses. The door itself is either single or paired and is most frequently divided into one, two, or four panels. The door is usually surrounded on sides and top by a narrow band of rectangular panes of glass held in a delicate, decorative frame. Door and glazed surround, in turn, are usually encased in a larger decorative enframement of wood or masonry. Not uncommonly door and glass are recessed behind the front wall, thus creating complex three-dimensional effects; free-standing columns are sometimes added to the inset portion.

COLUMNS—Classical columns for the support of porch roofs are a prominent feature of most Greek Revival houses. In some examples they dominate the entire facade; others retain only smaller entry porch columns. Although many Greek Revival houses have "correct" Greek columns, many also have Roman details; still more have vernacular adaptations with no clear classical precedent. The following guide to Classical Column Identification must therefore be used in combination with other typical features when identifying Greek Revival houses:

Classical columns are distinguished principally by their capitals (tops) and bases. Both Greek and Roman columns share three principal types of capitals which define the three familiar orders of classical architecture: Doric (plain capitals), Ionic (capitals with scroll-like spirals called volutes), and Corinthian (capitals shaped like inverted bells decorated with leaves). All three types are found in Greek Revival houses, as well as in most



SCHOOL SECTION AND

other classically influenced American styles. Greek and Roman examples of these three orders are distinguished by subtle differences in either the capitals or bases.

All columns of classical antiquity were round, as are many Greek Revival columns. Vernacular Greek Revival houses, on the other hand, commonly have square (and occasionally octagonal) columns, which were simple and inexpensive to construct from boards and moldings. Such columns generally lack classical capitals. About 40 percent of columns found on Greek Revival houses are square; the remaining 60 percent include about 40 percent Doric, 15 percent Ionic, and 5 percent Corinthian. Note that the Greek Doric column has no base, while the Roman version does. This distinction frequently will distinguish Greek Revival Doric columns from the Roman Doric columns of the Early Classical Revival. Note, however, that many Greek Revival houses retained Roman columns, particularly in the southern states, so that column type alone is seldom sufficient to identify the style.

Pilasters are also frequent Greek Revival features. They are most commonly used on the corners of frame houses but are occasionally found across the entire facade in lieu of free-standing columns.

WINDOWS—As in the preceding Adam style, Greek Revival window sashes most commonly had six-pane glazing. The rounded, three-part Palladian windows of Adam houses disappeared, to be replaced only occasionally by rectangular, tripartite examples. Small frieze-band windows, set into the wide trim beneath the cornice, are frequent. These are often covered with an iron or wooden grate fashioned into a decorative Greek pattern. Window surrounds were generally far less elaborate than doorways.

OCCURRENCE

Greek Revival was the dominant style of American domestic architecture during the interval from about 1830 to 1850 (to 1860 in the Gulf Coast states) during which its popularity led it to be called the National Style. It occurs in all areas settled by 1860, as noted on the map, and especially flourished in those regions that were being rapidly settled in the decades of the 1830s, '40s, and '50s. The style moved with the settlers from the older states as they crossed into Kentucky, Tennessee, and the Old Northwest Territory (today's Midwest). It followed the southern planters as they moved westward from the Old South into Alabama, Mississippi, and Louisiana. It even arrived on the west coast, sometimes disassembled into packages and shipped by way of Cape Horn! Each of the principal subtypes of the style shows geographic differences in frequency of occurrence, as noted above and in the maps.

Not surprisingly, the largest surviving concentrations of Greek Revival houses are found today in those states with the largest population growth during the period from 1820 to 1860. These are, in descending order of growth: New York, Pennsylvania, Ohio, Illinois, Virginia, Massachusetts, Indiana, Missouri, Tennessee, Alabama, Wisconsin, Georgia, Mississippi, Michigan, Texas, Kentucky, and Louisiana. New York gained about 2½ million persons during the interval while Louisiana gained about ½ million.

COMMENTS

The final years of the 18th century brought an increasing interest in classical buildings to both the United States and western Europe. This was first based on Roman models (see the Early Classical Revival chapter), but archeological investigation in the early 19th





century emphasized Greece as the Mother of Rome which, in turn, shifted interest to Grecian models. Two additional factors enhanced Greek influence in this country. Greece's involvement in a war for independence (1821-30) aroused much sympathy in the newly independent United States; at the same time, the War of 1812 diminished American affection for British influence, including the still dominant Adam style in domestic architecture.

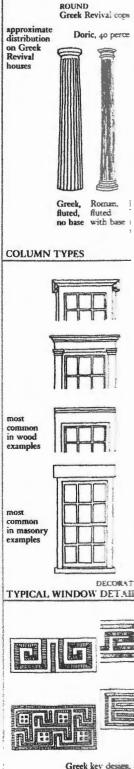
The Greek Revival began and ended in this country with public buildings built in Philadelphia. Among the first examples was the Bank of the United States (1818, William Strickland), and one of the last monuments was the Ridgeway Branch of the Philadelphia Library (1870, Addison and Hutton). Most domestic examples date from the period from 1830 to 1860. Among the earliest was a Greek remodeling of the Custis-Lee house in Arlington, Virginia, completed in 1820. The style was spread by carpenter's guides and pattern books, the most influential of which were written by Asher Benjamin (The Practical House Carpenter; The Builder's Guide) and Minard Lafever (The Modern Builder's Guide; The Beauties of Modern Architecture).

In addition to these guides for local carpenter-builders, there were a growing number of trained architects in America, some educated abroad, who designed high-style buildings in the fashionable Grecian mode. Among the most prominent were Benjamin H. Latrobe and his pupils Robert Mills and William Strickland; Strickland's own pupils Thomas U. Walter and Gideon Shryock; Ithiel Town, Alexander Jackson Davis (early work), John Haviland, Alexander Parris, and Isaiah Rogers.

One of the most familiar stereotypes in American architecture is the full-colonnaded Greek Revival mansion of the southern states. In this century these are sometimes called Southern Colonial houses, a historical inaccuracy since most were built long after American independence. This particular Greek Revival subtype does, however, have a little recognized colonial background, for it sprang, at least in part, from French colonial building practices. Early in their colonial expansions both the French and English appended broad living porches, a rarity in Europe, to houses built in tropical regions. The origins of these large galeries or verandahs are obscure, yet they appear wherever British or French colonists encountered warm climates, including the West Indies, Africa, India, and Australia. In the United States, most were built by the French in subtropical Louisiana. With the waning of French influence after the Louisiana Purchase in 1803, these forms slowly evolved in the Gulf Coast states into the full colonnaded Greek Revival form now sometimes known as Southern Colonial.

The decline of Greek Revival influence was gradual. In the more fashion-conscious urban centers of the Atlantic seaboard it began to be replaced by the Gothic Revival and Italianate movements in the 1840s. In the interior states, and in rural areas everywhere, it remained a dominant style for domestic buildings until the early 1860s.

An important and enduring legacy of the Greek Revival to American domestic architecture is the front-gabled house. Popularized during the ascendance of the Greek Revival style in the early 19th century, this became the predominant form for detached urban houses in cities of the Northeast and Midwest until well into the 20th century. There it occurs in unadorned folk versions, as well as in styled Gothic, Italianate, Queen Anne, and Shingle houses. In rural areas, the form of Greek Revival known as gable front and wing likewise remained a popular form for folk houses until the 1930s.



Greek key desagn

II. Building Materials:

Please mark the appropriate boxes listing the materials that exist on the building.

	Foundation	Walls	Roof	Others]
Wood					
Weatherboard,					
Clapboard					
Shingle					
Log					1
Plywood]
Shake					
Stone					1
Granite		/	1		
Sandstone		•	at)		(
Limestone	X (oese) of) × (oeis		WINDOW BEAUS	+ (olla.
Marble	1,10	7 (00		SIÀ	45
Slate			,	WINDOW BEADS	
Brick					1
Metal					1
Iron					1
Copper					
Bronze		**************************************			
Tin					
Steel	-				
Lead					1
Nickel					1
Cast Iron					
Stucco		*			
Terra Cotta					
Asphalt		and the second s	X		
Asbestos					1
Concrete					
Adobe		and the state of t			
Ceramic Tile		***************************************			
Glass					1
Cloth/Canyas					
Synthetics					
Fiberglass					
Vinyl					1
Aluminum		T			
Rubber	444	***************************************			
Plastic					
Drivit/EIFS					1
Other					

III.		ignificance of Property: lease indicate source of documentation, if available.
	a	Original Owner: John O CONNOR
	b	Architect/ Builder:
	c)) Significant Person(s):
	ď	Significant Dates (i.e., construction dates): /855 (SEE ATTACHE) OWNERSH
	e)	Please indicate which of the following criteria apply to the property:(check all that apply.)
		Y Property has character, interest, or value which is part of the development, heritage, or cultural character of the community, county, or nation.
		Property is the site of a significant local, county, state, or national event.
		Property is identified with a person who significantly contributed to the development of the community, county, state, or nation.
		Structure embodies distinguishing characteristics of an architectural style valuable for the study of a period, type; method of construction, or use of indigenous materials. (**LIMESTONE** Frank P.T. Flom A) JACENT LOT 70 EAST). Property is identified with the work of a master builder designer architect, or landscape.
		Property is identified with the work of a master builder, designer, architect, or landscape architect whose work has influenced the development of the area, the county, the state, or the nation.
		X Structure embodies elements of design, detailing, materials, or craftsmanship that are of architectural significance. (OCIGINAL LINE STONE CONTROL (CONTROL OF WEADS / STRUCTURE embodies design elements that make it structurally or architecturally innovative.
		Y Property has a unique location or physical characteristics that make it a familiar visual feature. (LOCATED NEAR LINCOLN PARK, ST. PATRICK CHILAGH, AND DOWN TOWN STORMS Structure is a particularly fine or unique example of a utilitarian structure with a high level of historical or architectural significance.
		Property is suitable for preservation or restoration.
		Property is included on theIllinois and/orNational Register of Historic Places.
		Property has yielded, or is likely to yield information important to prehistory, history, or other areas of archaeological significance.

316 Cedar Street 09-27-357-005 STCHAS/47/6

All supporting documents are stored at the Kane County Recorder of Deeds office.

1842	PoA	Cicero Millington to Darwin Millington
1849	WD	Darwin/Miranda Millington & Ira/Sarah Minard to Joseph Brown
1850	WD	Joseph Brown to Charles Whaylen
1850	WD	Charles/Mary Whalen to John O'Connor
		(Whaylen and Whalen is spelled two ways but is the same family)
1855	Mtg	Edw. O'Brian to John/Mary O'Connor
1855		House/Structure is built
1860		Map of City of St. Charles with Library of Congress shows structure on lot 6.
1865	WD	John O'Connor to Zimri Willard
1882	WD	Zimri/Louisa Willard to Emma Willard
1899	WD	Emma Willard to Ida Hill
1926	JTWD	Ida (Hill) Jackman to Mary Smelting
1974	JTWD	Mary Smelting to Ronald E. Johnson
2020	WD	Ronald E. Johnson to Richard/Mary Ramella Family Trust



1860 map

IV. Attachments

- 1. <u>Descriptive Statement:</u> Attach a narrative statement describing the property and its historical architectural significance as indicated in Sections I, II, and III above. Describe structural changes, additions, and decorative modifications or material changes and dates of such work if known. State the reasons it should be designated as a Historic property.
- 2. <u>Plat of Survey:</u> Attach a plat of survey showing the boundaries and location of the property. This may be obtained from the County Recorder (630-232-5935) at the Government Center. You may also have one from your house closing.
- 3. Photographs: Attach photographs showing the important structures or features of the property and a photograph as viewed from the public way. Black and white or color prints. A minimum of one photograph of the structure as viewed from the public way is required.

I (we) certify that this application and the documents submitted with it are true and correct to the best of my (our) knowledge and belief.

Record Owner Date

Applicant or Authorized Agent Date

316 Cedar Street Description

Built in 1855 on Lot 6, Block 47 of the Original Town of St. Charles, Kane County, Illinois (see attached), by John O'Connor (see attached), this beautifully restored limestone Greek Revival house (see attached) resides prominently within walking distance of Lincoln Park, St. Patrick's Church, and the city downtown. Rough limestone exterior with cut limestone window beads and sills this is has been known as the Johnson Statuary location in recent years. The east porch, north addition, and board and batten garage are additions to the original structure. The limestone used for the construction originated from a quarry located directly east of this structure known today as the VFW parking lot and currently used as a public parking area.









A	AGEND	A ITEN	M EXECUTIVE SUMMARY	Agen	da Item Number: *4h			
CITY OF ST. CHARLES	Title:	appro	Historic Preservation Commission recommendation to approve a Façade Improvement Grant Agreement for 13 S 2 nd St.					
ILLINOIS • 1834	Presenter:	Rachel Hitzemann						
Meeting: Planning & Development Committee Date: October 12, 2020								
Historic Recommendation: \$2,630		Budgeted Amount: \$30,000 (\$5,047.5 Remaining)		Not Budgeted:				

Executive Summary (if not budgeted please explain):

Program Description

The Façade Improvement Grant program provides assistance to property owners and commercial tenants to rehabilitate and restore the exterior of buildings in the downtown. Grant funding is available for buildings located in Special Service Area 1B (Downtown Revitalization) or in a Historic District or designated Historic Landmark site. Applications are first reviewed by the Historic Preservation Commission for appropriateness of design. The grants are provided as a reimbursement for up to 50% of the funds invested into an exterior rehabilitation project involving new improvements and up to 25% for maintenance work. Up to \$10,000 is available for a 30 ft. length of building façade. There is a limit of \$20,000 of grant funds per property in any 5 year period.

Proposal

Cynthia Hahn and Alba Morgante, owners of HM Hairbar LLC, have requested a Façade Improvement Grant for their building located at 13 S 2nd St. The grant is for painting and brick preparation for painting.

Historic Commission review – 9/16/2020

The Historic Commission reviewed the project and determined the work would be eligible for a 50% reimbursement on eligible items. There was a unanimous vote to recommend approval for the full eligible grant amount of \$2,630; citing the visibility of this building on a heavily trafficked street. They also noted that this building needed restoration.

Grant Amount

Total Cost of Project: \$5,260

The project is eligible to receive up to \$2,630

Attachments (please list):

Historic Commission Resolution, Program Requirements, Façade Improvement Grant Application, Grant Agreement

Recommendation/Suggested Action (briefly explain):

Historic Preservation Commission recommendation to approve a Façade Improvement Grant Agreement for $13 \ S \ 2^{nd} \ St$.

City of St. Charles, Illinois

Historic Preservation Commission Resolution No. 8-2020

A Resolution Recommending Approval of

A Façade Improvement Grant Application

(13 S. 2nd St.)

WHEREAS, it is the responsibility of the St. Charles Historic Preservation Commission to review

applications for the Facade Improvement Grant Program; and

WHEREAS, the Historic Preservation Commission has reviewed the Facade Improvement Grant

Application for 13 S 2nd St. and has found said application to be architecturally appropriate and in

conformance with the Downtown Design Guidelines and the Historic Preservation Ordinance, Chapter 17.32

of the Zoning Ordinance; and

WHEREAS, the Historic Preservation Commission finds said Facade Improvement Grant

Application to be in conformance with the program requirements.

NOW THEREFORE, be it resolved by the St. Charles Historic Preservation Commission to

recommend to the City Council approval of the Facade Improvement Application; because the structure is

prominent on a heavily trafficked street and the building needs extensive restoration and rehabilitation

work.

Roll Call Vote:

Aves: Norris, Smunt, Pretz, Mann, Kirsininkas, Kessler

Nays: None. Abstain: None Absent: Malay

Motion Carried.

PASSED, this 16th day of September, 2020.

FACADE IMPROVEMENT GRANT PROGRAM DESCRIPTION

May 1, 2017

COMMUNITY & ECONOMIC DEVELOPMENT DEPT. /PLANNING DIVISION

CITY OF ST. CHARLES



1. <u>Program Purpose</u>

- The Facade Improvement Program is intended to promote reinvestment and restoration of commercial and residential buildings in the downtown area, with a focus on supporting historic preservation practices.
- The program is intended to assist property owners and commercial tenants to rehabilitate and restore the visible exterior of existing structures.
- Improvements must meet criteria for appropriateness of design.
- Reimbursement grants are provided to property owners or commercial tenants in recognition of the positive impact that individual building improvements can have on the overall appearance, quality and vitality of downtown St. Charles.

2. Application, Review and Approval Process:

- Determine if your property is eligible for either the Commercial or Residential Façade Improvement Grant.
- o Determine if your project is eligible for grant reimbursement.
- O Define the scope of your proposed improvements. This will probably involve consulting with an architect or other appropriate design professional (for projects that do not need an architect, consult with a contractor).
- Contact the City to schedule a preliminary review of the project by the Historic Preservation Commission early in the design process to determine if the project scope and improvements will meet the program requirements. The Historic Preservation Commission will consider the architectural appropriateness of proposed improvements using Design Guidelines and the Historic Preservation Ordinance (Chapter 17.32 of the Municipal Code). Improvements that are not architecturally appropriate, as determined by the City Council upon recommendation of the Historic Preservation Commission, are not eligible for a reimbursement grant. The Design Guidelines apply to all grant projects, regardless of whether they are in the Historic District.
- The grant Program Year runs from May 1 to April 30 of the following year. Grant applications are accepted beginning in March of each year for the Program Year beginning on May 1. (Note: The budget for the Program Year will not be finalized until approved by the City Council each year. This typically occurs in early April.)
- Submit a complete grant application. Attend the following meetings on the dates provided by City staff:
 - The **Historic Preservation Commission** will review and make a recommendation regarding the grant. They meet on the 1st and 3rd Wednesdays of each month at 7:00pm.
 - The Planning & Development Committee of the City Council will review the Historic Commission recommendation at their meeting on the second Monday of the month at 7:00pm.

If recommended for approval, the City Council will then vote on the formal grant agreement at a subsequent meeting The grant agreement will follow the standard form, which is attached. Attendance at this meeting is not necessary unless requested.

The earliest the grant agreement can be approved by the City Council is the third Monday of May. Work initiated prior to City Council approval of the grant agreement is not eligible for reimbursement.

3. Commercial Façade Grant

• Eligible Properties:

Commercial or Multi-Family Residential Buildings (two or more units) located within either:

- Special Service Area #1B
- o Historic District or Landmark Site

Properties in SSA #1B are given first priority at the beginning of the program year.

- Minimum Project Cost: \$2,500
- Grant for Front or Side Facades (visible from street): Maximum grant amount is based upon the frontage of the façade to be renovated, at a maximum of \$10,000 per 30 ft. horizontal length of façade. A facade is defined as a thirty foot wide span along the front or side of a building facing a public street, measured along the building wall generally parallel to the right of way line. For building fronts or sides exceeding thirty feet, a pro rata amount will be applied.
- Grant for Rear Entrance Improvements: Maximum grant amount of \$10,000, available for buildings with an existing or proposed rear entrance that is accessible to the public from a dedicated public street, alley, or other right of way, or from a parking lot or walkway that is owned or leased by the City, or from other property that is encumbered by an easement granting public pedestrian access. The rear entrance to be improved must provide public access to a business or businesses within the building.

• Maximum Grant Limits:

- o Total grant amount during any five-year period is capped at \$20,000.
- o For properties on the National Register of Historic Places or Locally Designated Landmarks, the total grant amount for any five-year period is capped at \$30,000.

• Eligible Improvements:

- o 25% Reimbursement for Routine Maintenance:
 - ✓ Cleaning, patching, caulking of exterior surfaces.
 - ✓ Re-coating of paint on exterior surfaces (without extensive surface preparation)
 - ✓ Re-roofing visible roof surfaces with non-historic material (such as 3-tab or architectural grade asphalt shingles)
 - ✓ Spot masonry repairs or tuckpointing
 - ✓ Like-in-kind replacement of non-historic elements on a building

o 50% Reimbursement for:

For Historic structures, maintenance utilizing Historic Preservation practices:

- ✓ Repair or restoration of historic features
- ✓ Replacement of deteriorated historic features with like materials or appropriate synthetic materials
- ✓ Re-roof or repair of visible roof surfaces with non-standard materials (such as wood shake, slate, or other decorative non-standard materials)
- ✓ Extensive restoration/repair of historic masonry material
- ✓ Painting of exterior surfaces where the surface preparation includes removal of worn/failing paint and intensive surface preparation prior to painting

Building improvements:

- ✓ Exterior building upgrades or enhancements that will improve the historic character of a building
- ✓ Improvement, replacement or installation of storefront systems, doors, windows and trim materials.
- ✓ Removal of architecturally inappropriate features on buildings (including removal of synthetic surface materials)
- ✓ Exterior lighting that illuminates a façade

- o 100% Reimbursement for Architectural Services (Up to \$4,000)
 - Where architectural services are required, the owner or tenant should retain an architect to prepare a conceptual design and cost estimate for work proposed. If the project is approved by the City, the architect may provide bidding and construction plans and documents, as well as construction supervision. Only those architectural services directly related to the approved facade improvement will be reimbursed.
- o Ineligible Improvements:
 - Signs and Awnings, unless in connection with other eligible improvements.
 - Any interior improvement or finishes
 - Any improvements to internal building systems, including HVAC, plumbing, electrical (except for wiring for exterior lighting)
 - Any site improvements, including sidewalks, parking lots and landscaping.
- Improvements not specifically listed as eligible or ineligible are subject to review as to eligibility by the Historic Preservation Commission as an advisory body and approval or disapproval by City Council.

5. Terms and Conditions applicable to all grants:

- o **Grant applications will be considered in the order they are received.** In the event that the total amount of the potential reimbursement grants exceeds the amount budgeted for the program year, the applications will be carried over for consideration during the following program year.
- O Not more than one grant shall be approved for a building in any program year, and a grant shall not be approved if a grant was made for the same portion of the building within the previous five years. For the Residential Grant Program, within the 5 program years following approval of a grant, a grant for the same property will not be considered until September of each program year.
- O The maximum amount of the reimbursement grant for a specific property will be set forth in a Facade Improvement Agreement between the City and the property owner or tenant. If the actual costs exceed the original final estimates submitted with the application and used to determine the final total amount of reimbursement within the Agreement, the property owner or tenant will be responsible for the full amount of the excess. The City cannot reimburse more than the total amount specified in the Agreement.
- O Reimbursement grants are subject to Federal and State taxes, and are reported to the Internal Revenue Service on Form 1099. You are required to provide your taxpayer ID number or social security number as part of the Façade Improvement Agreement. Property owners and tenants should consult their tax advisor for tax liability information.
- The following items are not considered "improvements" and therefore they are not eligible for reimbursement:
 - Building Permit fees and related costs.
 - Extermination of insects, rodents, vermin and other pests.
 - Title reports and legal fees.
 - Acquisition of land or buildings.
 - Financing costs.
 - Sweat equity.
 - Working capital for businesses.
- Work that has been initiated prior to the approval of the Facade Improvement Agreement by the City Council is NOT eligible for grant reimbursement.
- O All improvements must be completed prior to the end of the program year on April 30. If the work is not complete by the end of the program year, the City's remaining obligation to reimburse the owner or tenant for the project terminates. The City may, its sole discretion, grant a single one-year extension due to unforeseen circumstances that have prevented the completion of the project.
- o The property owner and tenant shall be responsible for maintaining the facade improvements without alteration for five (5) years. A restrictive covenant limiting alterations may be required by the City Council at the time of approval of the Facade Improvement Agreement.
- Any project changes must be approved by the City. Major changes or elimination of improvements
 must be approved by the City Council. Minor revisions must be approved by the Historic Preservation
 Commission.
- This is a reimbursement program -- you must pay your architect, contractors and suppliers before you receive payment from the City.

FAÇADE IMPROVEMENT GRANT APPLICATION



COMMUNITY & ECONOMIC DEVELOPMENT DEPT. /PLANNING DIVISION

CITY OF ST. CHARLES

<u>Grant</u>	Type (select one):	
Ø	Commercial Residential	St. Charles, IL
Proper	ty Information:	SEP 1 1 2020
Buildin	g or establishment for which the reimbursement grant is requested:	CDD
Addres	13 S. 2 ¹⁸ STREET	Planning Divisio
Propert	y Identification Number: 09 - 27 - 364 - 013	
	HM HAIR BAR - CYNTHIA ALBA	MORGANTE
Project	Description:	
Ex	TERIOR BRICK REPAIR AND	EXTERIOR
PAI	NT OF BRICK	
_	NT OF BOOKS AND TRIM ON WI	
· •		
Total C	Cost Estimate: \$ 5,260,00	
Submit	tal Checklist:	
×	\$50 Application Fee	
図	Detailed Scope of Work: Must identify all improvements, construction methods, buil Costs must be broken down and itemized by task. In general, this scope of work should contractor(s) who will be completing the project.	
×	Documentation on Existing Conditions: Reports or photographs to demonstrate need	1 for improvements.
	W-9 Form: Filled out and signed by the grant applicant, with a Federal Tax ID Numb. Number for an individual)	er (or a Social Security

Applicant Contact Information:

Phone Number: 630-903-5801

Email Address: HMHAIRBAR 2029 @ GMAIL.COM

Statement of Understanding:

- I agree to comply with the guidelines and procedures of the Façade Improvement Grant Program. I have read and understand the "Terms and Conditions".
- I understand that I must submit detailed cost documentation, copies of bids, contracts, invoices, receipts, and contractor's final waivers of lien upon completion of the approved improvements.
- I understand that work done before a Façade Improvement Agreement is approved by the City Council is not eligible for a grant.
- I understand the Façade Improvement reimbursement grants are subject to taxation and that the City is required to report the amount and recipient of said grants to the IRS

Signature: Date: 9-8-2025

Owner Authorization (if applicable):

If the applicant is other than the owner, you must have the owner complete the following certificate:

I certify that I am the owner of the property at 13 5 2 15, and that I authorize the applicant to apply for a reimbursement grant under the St. Charles Facade Improvement Program and undertake the approved improvements.

Signature: Date: 9-10-7D2D

Attachments:

ATTACHMENT 1: QUOTE FROM CONTRACTOR FOR LABOR EMAIL WITH ESTIMATE FOR MATERIAL

ATTACHMENT 2: PICTURE OF PEOPLETY WITH COLOR SCHEME PLAN

ATTACHMENT 3: PICTURE REFERENCE OF SIMILAR LOOK

EAGLE ROCK RENOVATIONS LLC.

PROPOSAL

Attention: Cynthia Hahn

13 South 2nd St. St Charles II, 60174

Date: 10/26/18

630-418-3468 rebeca.L@hotmail.com

130 S La Londe Ave. Addison, II 60101

Description	Cost	
The job consists of restoring bricks and cracks on the front of the local. (later side right) Afterwards 2 lateral sides will be painted in white and the frontal side in black and white.	\$	3,800.00

If you have any questions or concerns, please do not hesitate to contact me. Thank you!

Salvador Lozano





Salvador Lozano (paint info)

3 messages

Rebeca Lozano <rebeca.l@hotmail.com>

Sat, Sep 5, 9:25 PM

To: hmhairbar2029@gmail.com <hmhairbar2029@gmail.com>

Hello Cynthia,

I have the cost of approximately how much the paint will be.

Sherwin Williams: we will need 3 cans of 5 gallons each of primer..... aprox. \$540.00

We will need the same amount of white paint....\$480.00

3 gallons of black paint.... aprox.\$140.00

Total: \$1,160.00 (more or less). this is about the price they give me with the discount I get with Serwin Williams.

Benjamin Moore: this one will be a little more expensive but better quality

Will need the same amounts as Sherwin Williams.

\$610.00 (primer)

\$690.00 (white paint)

\$160.00 (black paint)

Total: \$1,460.00

I cannot give you the add for the 3 doors because I need to know if they are wood or metal and I need to see what I need to do for the paint job on them. I will be by the local sometime to check it out.

Let me know your decision, I will contact you to come to an agreement and to let you know if I can and when I can start the job.

Thank you.

Enjoy your weekend!

Salvador Lozano

Cham hairbar <hmhairbar2029@gmail.com>

To: joshuah.hahn@gmail.com <joshuah.hahn@gmail.com>

[Quoted text hidden]

Cham hairbar <hmhairbar2029@gmail.com>

Draft To: Rebeca Lozano <rebeca.l@hotmail.com>

Hi r

[Quoted text hidden]

Sat, Sep 5, 9:28 PM

Tue, Sep 8, 9:26 PM

24 S 2nd St



Image capture: Jul 2019 © 2020 Google

St. Charles, Illinois



Street View



- 1) PAINT WHITE PAINT SIDES AND FRONT 2) PAINT BLACK ACCENT W/ BLACK TRAM. AND BLACK CROWN





ARCHITECTURAL SURVEY

ST. CHARLES CENTRAL DISTRICT ST. CHARLES, ILLINOIS

ST. CHARLES HISTORIC PRESERVATION COMMISSION

DIXON ASSOCIATES / ARCHITECTS

ARCHITECTURAL INTEGRITY			ΓY	BUILDING CONDITION
	1	2	3	☐ Excellent: Well-maintained
☐ Unaltered				☐ Good: Minor maintenance needed
☐ Minor Alteration				☐ Fair: Major repairs needed
☐ Major Alteration	\boxtimes			☐ Poor: Deteriorated
□ Additions Sensitive to original Insensitive to original 1: first floor; 2: upper floors ARCHITECTURAL SIGN □ Significant □ Contributing □ Non-Contributing	; 3: roc	•		ARCHITECTURAL DESCRIPTION Style: Italianate Date of Construction: 1880's Source: Features: Two story brick structure with distinctive metal cornice at parapet. Recessed entries at first floor.



ROLL NO. 3

NEGATIVE NO. 21

Address

13/15 South 2nd Street

Representation in Existing Surveys:

- ☐ Federal
- ☐ State
- ☐ County
- ☐ Local

Block No. 10

Building No.9

SURVEY DATE:

MAY 1994

CITY OF ST. CHARLES FACADE IMPROVEMENT AGREEMENT

Program Year: May 1, 2020 to April 30, 2021

THIS AGREEMENT, entered into this 19th day of October, 2020, between the City of St. Charles, Illinois (hereinafter referred to as "CITY") and the following designated OWNER/LESSEE, to wit:

Owner/Lessee's Name: Cynthia Hahn/HM Hairbar LLC

Tax ID# or Social Security #

For the following property:

Address of Property: 13 S 2nd St.

PIN Number: 09-27-364-013

WITNESSETH:

WHEREAS, the CITY has established a Facade Improvement Program adopted by City Ordinance No. 2017-M-7; and

WHEREAS, CITY has agreed to participate, subject to its sole discretion, in reimbursing Owners/Lessees for the cost of eligible exterior improvements to buildings through the Façade Improvement Program; and

WHEREAS, the OWNER/LESSEE desires to participate in the Facade Improvement Program pursuant to the terms and provisions of this Agreement.

NOW, THEREFORE, in consideration of the mutual covenants and agreements obtained herein, the CITY and the OWNER/LESSEE do hereby agree as follows:

SECTION 1:

- A. With respect to Commercial Façade Grant improvements, the CITY shall reimburse OWNER/LESSEE for the cost of improvements to the OWNER/LESSEE's property at the rate of up to twenty five (25%) of the cost of Routine Maintenance Improvements, up to fifty percent (50%) of the cost of Historic Preservation Improvements and other Building Improvements, and up to one hundred percent (100%) of the cost of fees for Architectural Services pertaining to such improvements, provided that the total reimbursement for eligible improvements and architectural services shall not exceed the amount shown in Exhibit I, "Total Reimbursement Amounts", attached hereto.
- B. With respect to Residential Façade Grant improvements, the CITY shall reimburse OWNER/LESSEE for the cost of improvements to the OWNER/LESSEE's property at the rate of up to fifty percent (50%) of the cost of Historic Preservation Improvements, and up to one hundred percent (100%) of the cost of fees for Architectural Services pertaining to such improvements, provided that the total reimbursement for eligible improvements and architectural services shall not exceed the amount shown in Exhibit I, "Total Reimbursement Amounts", attached hereto.

The actual total reimbursement amounts per this Agreement shall not exceed the amounts shown in Exhibit I. The improvement costs which are eligible for City reimbursement include all labor, materials, equipment and other contract items necessary for the proper execution and completion of the work as shown on the plans, design drawings, specifications and estimates approved by the City. Such plans, design drawings, specifications and estimates are attached hereto as Exhibit II.

SECTION 2: No improvement work shall be undertaken until its design has been submitted to and approved by the City Council. Following approval, the OWNER/LESSEE shall contract for the work and shall commence and complete all such work within the Program Year, ending April 30.

SECTION 3: The Director of Community Development shall periodically review the progress of the contractor's work on the facade improvement pursuant to this Agreement. Such inspections shall not replace any required building permit inspection. All work which is not in conformance with the approved plans, design drawings and specifications shall be immediately remedied by the OWNER/LESSEE and deficient or improper work shall be replaced and made to comply with the approved plans, design drawings and specifications and the terms of this Agreement.

SECTION 4: Upon completion of the improvements and upon their final inspection and approval by the Director of Community Development, the OWNER/LESSEE shall submit to the CITY a properly executed and notarized contractor statement showing the full cost of the work as well as each separate component amount due to the contractor and each and every subcontractor involved in furnishing labor, materials or equipment in the work. In addition, the OWNER/LESSEE shall submit to the CITY proof of payment of the contract cost pursuant to the contractor's statement and final lien waivers from all contractors and subcontractors. The OWNER/LESSEE shall also submit to the CITY a copy of the architect's statement of fees for professional services for preparation of plans and specifications. The CITY shall, within fifteen (15) days of receipt of the contractor's statement, proof of payment and lien waivers, and the architect's statement, issue a check to the OWNER/LESSEE as reimbursement, subject to the limitations set forth in Exhibit "I".

In the alternative, at its sole discretion, CITY may reimburse OWNER/LESSEE in two payments. The first reimbursement may be made only 1) upon completion of work representing 50% or more of the maximum reimbursement specified in Exhibit I hereof; 2) upon receipt by CITY of the architect's invoices, contractor's statements, invoices, proof of payment and notarized final lien waivers for the completed work; and 3) upon a determination by the Director of Community Development that the remainder of the work is expected to be delayed for thirty days or more following completion of the initial

work due to weather, availability of materials, or other circumstances beyond the control of the OWNER/LESSEE. The second, final reimbursement payment shall be made by CITY only upon submittal of all necessary documents as described herein.

SECTION 5: If the OWNER/LESSEE or his contractor fails to complete the improvement work provided for herein in conformity with the approved plans, design drawings and specifications and the terms of this Agreement, or if the improvements are not completed by the end of the Program Year on April 30, this Agreement shall terminate and the financial obligation on the part of the CITY shall cease and become null and void. The CITY may, at its sole discretion, grant a single one-year extension to the end of the following program year due to unforeseen circumstances that have prevented the completion of the project.

SECTION 6: Upon completion of the improvement work pursuant to this Agreement and for a period of five (5) years thereafter, the OWNER/LESSEE shall be responsible for properly maintaining such improvements in finished form and without change or alteration thereto, as provided in this Agreement, and for the said period of five (5) years following completion of the construction thereof, the OWNER/LESSEE shall not enter into any Agreement or contract or take any other steps to alter, change or remove such improvements, or the approved design thereof, nor shall OWNER/LESSEE undertake any other changes, by contract or otherwise, to the improvements provided for in this Agreement unless such changes are first submitted to the Director of Community Development, and any additional review body designated by the Director, for approval. Such approval shall not be unreasonably withheld if the proposed changes do not substantially alter the original design concept of the improvements as specified in the plans, design drawings and specifications approved pursuant to this Agreement. If requested by the CITY, OWNER/LESSEE agrees to execute and record a restrictive covenant regarding the maintenance of improvements completed per this agreement.

SECTION 7: The OWNER/LESSEE releases the CITY from, and covenants and agrees that the CITY shall not be liable for, and covenants and agrees to indemnify and hold harmless the CITY and its officials, officers, employees and agents from and against, any and all losses, claims, damages, liabilities or expenses, of every conceivable kind, character and nature whatsoever arising out of, resulting from or in any way connected with directly or indirectly with the facade improvement(s), including but not limited to actions arising from the Prevailing Wage Act (820 ILCS 30/0.01 et seq.) The OWNER/LESSEE further covenants and agrees to pay for or reimburse the CITY and its officials, officers, employees and agents for any and all costs, reasonable attorneys' fees, liabilities or expenses incurred in connection with investigating, defending against or otherwise in connection with any such losses, claims, damages, liabilities, or causes of action. The CITY shall have the right to select legal counsel and to approve any settlement in connection with such losses, claims, damages, liabilities, or causes of action. The provisions of this section shall survive the completion of said facade improvement(s).

SECTION 8: Nothing herein is intended to limit, restrict or prohibit the OWNER/LESSEE from undertaking any other work in or about the subject premises which is unrelated to the facade improvement provided for in this Agreement.

SECTION 9: This Agreement shall be binding upon the CITY and upon the OWNER/LESSEE and its successors, to said property for a period of five (5) years from and after the date of completion and approval of the facade improvement provided for herein. It shall be the responsibility of the OWNER/LESSEE to inform subsequent OWNER(s)/LESSEE(s) of the provisions of this Agreement.

IN WITNESS THEREOF, the parties hereto have executed this Agreement on the date first appearing above.

OWNER/LESSEE	CITY OF ST. CHARLES
	Mayor
	ATTEST:
	City Clerk

EXHIBIT "I"

Total Reimbursement Amounts

Commercial Façade Grants:

	Total Estimated Cost	Reimbursement Percentage	Total Maximum Grant Amount
Routine Maintenance Improvements	\$	25%	
Historic Preservation Improvements	\$	50%	\$
Building Improvements	\$ 5,260	50%	\$ 2,630
Architectural Services	\$	100% (not to exceed \$4000)	\$
TOTAL	\$5,260	-	\$ 2,630

Residential Façade Grants:

	Total Estimated Cost	Reimbursement Percentage	Total Maximum Grant Amount
Historic Preservation Improvements	\$	50%	\$
Architectural Services	\$	100% (not to exceed \$2000)	\$
TOTAL		-	

EXHIBIT "II"

Plans, Design drawings, Specifications and Estimates

Attachments:
Scope of work
Proposal for work – dated 10/16/18
Paint Info- dated 9/5/2020